

Chapter **7**

Background Data:
*Auburn Municipal Airport
and Environs*

Background Data: Auburn Municipal Airport and Environs

INTRODUCTION

Auburn Municipal Airport is a 253-acre general aviation facility serving the City of Auburn and surrounding areas in the counties of Placer, El Dorado, Nevada, and Sacramento. The airport is owned and operated by the City of Auburn.

The City of Auburn is situated in the foothills of the Sierra Nevada Mountains approximately 35 miles northeast of Sacramento. Auburn Municipal Airport is situated 3.5 miles north of the city center in a noncontiguous incorporated island surrounded by unincorporated lands of Placer County.

AIRPORT MASTER PLAN AND AIRPORT LAYOUT PLAN STATUS

The Auburn City Council adopted a master plan for Auburn Municipal Airport in July 2007. Since publication of the master plan, minor amendments have been made to the Airport Layout Plan (ALP) drawing to reflect recent construction projects none of which have compatibility planning implications. The current ALP was approved by the Federal Aviation Administration (FAA) in August 2012. The information contained on the 2012 ALP, together with supplemental information provided in the 2007 master plan and by airport personnel form the foundation for this *Auburn Municipal Airport Land Use Compatibility Plan (ALUCP)*.

Airfield Configuration

The airport consists of a single 3,700-foot east/west runway designated Runway 7-25. Both runway ends have runway protection zones (RPZs) that are wider than required based upon prior plans and occasional use by large aircraft. The RPZs lie entirely on airport property. The airport has one GPS-based nonprecision instrument approach procedure which provides a straight-in approach to Runway 7 and a circle-to-land approach to Runway 25. The airport's building area and aircraft parking aprons are located south of the airfield.

Minor improvements are planned for the airport including a future straight-in nonprecision approach to Runway 25 and additional aircraft hangar spaces southeast of the airfield. Long-term plans (beyond 20-

years) include a new full-length parallel taxiway north of the runway which will serve future aviation development.

The 2007 master plan includes a substantial analysis of extending the existing runway further to the west, east or combination of both. The intention was to address the future needs of the Airport by extending the current 3,700 foot long runway to an optimal 4,280 feet. The City Council ultimately did not include that extension in the 2007 master plan as it did not seem economically feasible at the time. The City is initiating a master plan update in 2014 to reassess the feasibility of extending the existing runway to accommodate slightly larger planes and meet the future aviation needs for the region. However, until such time as the contemplated runway extension is represented in a master plan adopted for the airport by the City, this feature is not incorporated into the ALUC's ALUCP for the Airport.

Aircraft Activity and Forecasts

The adopted master plan contains the most recent information regarding current and forecast airport activity. Airport personnel indicate that the current (2012) airport activity level is approximately 70,000 annual operations. This activity level is consistent with the base year level presented in the 2007 Master Plan for 2004/2005. The master plan's 20-year forecast of 104,000 annual operations continues to be representative of the growth potential at Auburn Municipal Airport. The master plan forecast serves as the basis of this *ALUCP*. Exhibit 4D contains additional details regarding existing and forecast airport activity.

Aircraft Traffic Patterns

For fixed-wing aircraft, Runways 7 and 25 both have a standard left-hand pattern, thus creating traffic patterns both north and south of the runway. However, because over 90% of aircraft operations are on Runway 25 (east to west), the predominant traffic pattern is the one on the south.

In lieu of a dedicated heliport, helicopters approach and depart using the runway. The predominant traffic pattern for helicopters is south of the airport, making right-hand turns to land on Runway 7 or left turns to Runway 25.

For noise abatement purposes, aircraft departing to the east on Runway 25 are requested, if speed and altitude permit, to make a 20° left turn at the end of the runway to avoid the convalescent home and mobile park.

The airport has one instrument approach procedure, RNAV (GPS) RWY 7. This approach allows for a straight in approach to Runway 7 or a circling approach to either Runway 7 or 25. The straight in procedure to Runway 7 has three categories and associated visibility minimums: 1) visibility minimums of 1 ¼ mile with a decision height of 315 feet above touchdown zone elevation, or 2) 1 ¾ mile visibility minimums with a decision height of 435 feet above touchdown zone elevation, or 3) Visibility minimums of 1-mile with a decision height of 567 feet above touchdown zone elevation. The circling approach has visibility minimums as low as 1-mile and a decision height of 601 feet above airport elevation. Aircraft utilizing the circling approach may circle the airport any distance desired so long as the runway can be seen at a distance no closer than 1 mile from the runway. The circling approach is not permitted at night.

SURROUNDING LAND USES

Auburn Municipal Airport is situated within the city limits, but in a noncontiguous segment some 1.5 miles beyond the northern edge of the remainder of the city. The city's sphere of influence encompasses all of the intervening land plus additional areas on all sides of the airport. At present, though, the airport environs fall primarily within unincorporated Placer County jurisdiction. No large-scale annexation is anticipated within the foreseeable future.

Surrounding lands contain a mixture of residential, industrial, and commercial uses and open space. The most intensive development is to the west along Grass Valley Highway (State Highway 49), three-quarters of a mile west of the runway end. Various commercial uses, hospital and a mobile home park lie along the highway corridor. Major uses to the south include a reservoir and a golf course. Areas to the north and east consist mostly of rural residential uses, convalescent home and some undeveloped land. An aqueduct, owned and operated by Pacific Gas and Electric Company, traverses the eastern side of the airport property. Schools, places of worship, and other public facilities also exist within the airport environs. Planned land uses reflect existing land use patterns.

EXHIBITS

The following exhibits illustrate the compatibility factors and background information which serve as the basis for this *ALUCP*.

Exhibit 7A: Airport Features Summary—Presents information pertaining to the airport configuration, operational characteristics, and applicable planning documents.

Exhibit 7B: 2012 Airport Layout Plan—The FAA-approved ALP depicting the airport configuration and airport building areas.

Exhibit 7C: Airport Activity Summary—Presents existing and forecast activity levels for the airport provided in the 2007 Master Plan and brought forward for *ALUCP* purposes.

Exhibits 7D and 7E: Compatibility Factors—Depicts the extents of the four compatibility factors upon which the compatibility zones for Auburn Municipal Airport were derived. The four compatibility factors are defined by:

- › *Noise* – Future noise contours reflecting a forecasted aircraft activity level of 104,000 annual operations.
- › *Overflight* – Primary traffic patterns reflecting where aircraft and helicopters operating at Auburn Municipal Airport routinely fly.
- › *Safety* – Generic safety zones for a short general aviation runway as provided in the *California Airport Land Use Planning Handbook* (October 2011).
- › *Airspace Protection* – FAA notification and obstruction surfaces as defined by Federal Aviation Regulation (FAR) Part 77, *Safe, Efficient Use and Preservation of the Navigable Airspace*.
- › *Compatibility Zones* – Policy zones developed for this *ALUCP* are based on the above four factors. Airport-specific considerations used to develop these zones are summarized in Chapter 4.

Exhibit 7F: Airport Environs Information—Summarizes information about current and planned land uses in the environs of the Auburn Municipal Airport. Airport land use compatibility policies contained in the County’s and City’s general plans are also summarized.

Exhibit 7G: General Plan Land Use Designations—Shows planned land use designations as reflected in the 2013 and 1993 adopted general plan land use diagrams for Placer County and the City of Auburn, respectively. Planned land use designations for the unincorporated areas within the city’s sphere of influence are consistent with the county’s designations shown on the map.

Exhibit 7H: Aerial—An aerial photo of the airport environs.

GENERAL INFORMATION

- *Airport Ownership:* City of Auburn
- *Property Size*
 - › Fee title: 253 acres
 - › Avigation easement: None
- *Airport Classification:* General Aviation
- *Airport Elevation:* 1,539 ft. MSL (surveyed)

BUILDING AREA

- *Location*
 - › South side of runway
- *Aircraft Parking Capacity*
 - › 138 tiedown spaces on apron
 - › 80 hangar spaces
 - › 3 helicopter spaces
- *Services*
 - › Self-serve general aviation and jet fuel available 24 hours per day
 - › Aircraft repairs; avionics sales and services
 - › Aircraft rental; flight instruction; pilot supplies
 - › Scenic flights; rental cars; restaurant

RUNWAY/TAXIWAY DESIGN**Runway 7/25**

- *Airport Reference Code:* B-I (small)
- *Critical Aircraft:* Cessna 414 Chancellor
- *Dimensions:* 3,700 ft. long, 75 ft. wide
- *Runway OFA Width:* 250 ft.
- *Pavement Strength* (main landing gear configuration)
 - › 30,000 lbs. (single wheel)
- *Effective Gradient:* 1.24% (rising to east)
- *Runway Lighting:* Medium-intensity runway edge lighting (pilot controlled); runway edge lights (REILS)
- *Runway Markings:* Nonprecision
- *Primary Taxiways:* Full-length parallel taxiway on south

APPROACH PROTECTION

- *Runway Protection Zones (RPZs)*
 - › Runway 7: 500 ft. inner width, 700 ft. outer width, 1,000 ft. long; all on airport property
 - › Runway 25: 500 ft. inner width, 700 ft. outer width, 1,000 ft. long; all on airport property
 - › RPZs wider than required dimensions based upon prior plans and occasional use by large aircraft
- *Approach Obstacles*
 - › Runway 7: 4 ft. hill, 342 ft. from runway end, 35:1 slope to clear
 - › Runway 25: 10 ft. hill, 425 ft. from runway end, 90 ft. off centerline, 22:1 slope to clear

Source: Data Compiled by Mead & Hunt November 2012

TRAFFIC PATTERNS AND APPROACH PROCEDURES

- *Airplane Traffic Patterns*
 - › Runway 7/25: Left traffic
 - › Pattern Altitude: 1,000 ft. AGL
- *Helicopter Traffic Patterns*
 - › Runway 7/25: Traffic pattern south of airport (east of Highway 49 and north of Bell Road); right turns to Runway 7 and left turns to Runway 25
 - › Pattern Altitude: 580 ft. AGL
- *Ultralight Activity:* On and in airport environs
- *FAR Part 77 Category*
 - › Runway 7: Nonprecision [A(NP)]
 - › Runway 25: Visual [A(V)]
- *Instrument Approaches*
 - › Runway 7 RNAV (GPS): Straight-in nonprecision approach (1 mile visibility; 567 ft. AGL minimum descent height); circling (1 mile visibility, 601 ft. AGL minimum descent height)
- *Visual Navigational Aids*
 - › Airport: Rotating beacon
 - › Runway 7: 2-light PAPI on left
 - › Runway 25: 2-light PAPI on left
- *Noise Abatement Procedures*
 - › Runway 25: Departures make 20° left turn at runway end to avoid overflight of convalescent hospital and mobile home park

AIRPORT PLANNING DOCUMENTS

- *Airport Master Plan*
 - › Adopted by Auburn City Council July 2007
- *Airport Layout Plan*
 - › Approved by FAA August 2012
 - › Accepted by Caltrans Division of Aeronautics for basis of this ALUCP (approval pending)

PROPOSED FACILITY IMPROVEMENTS

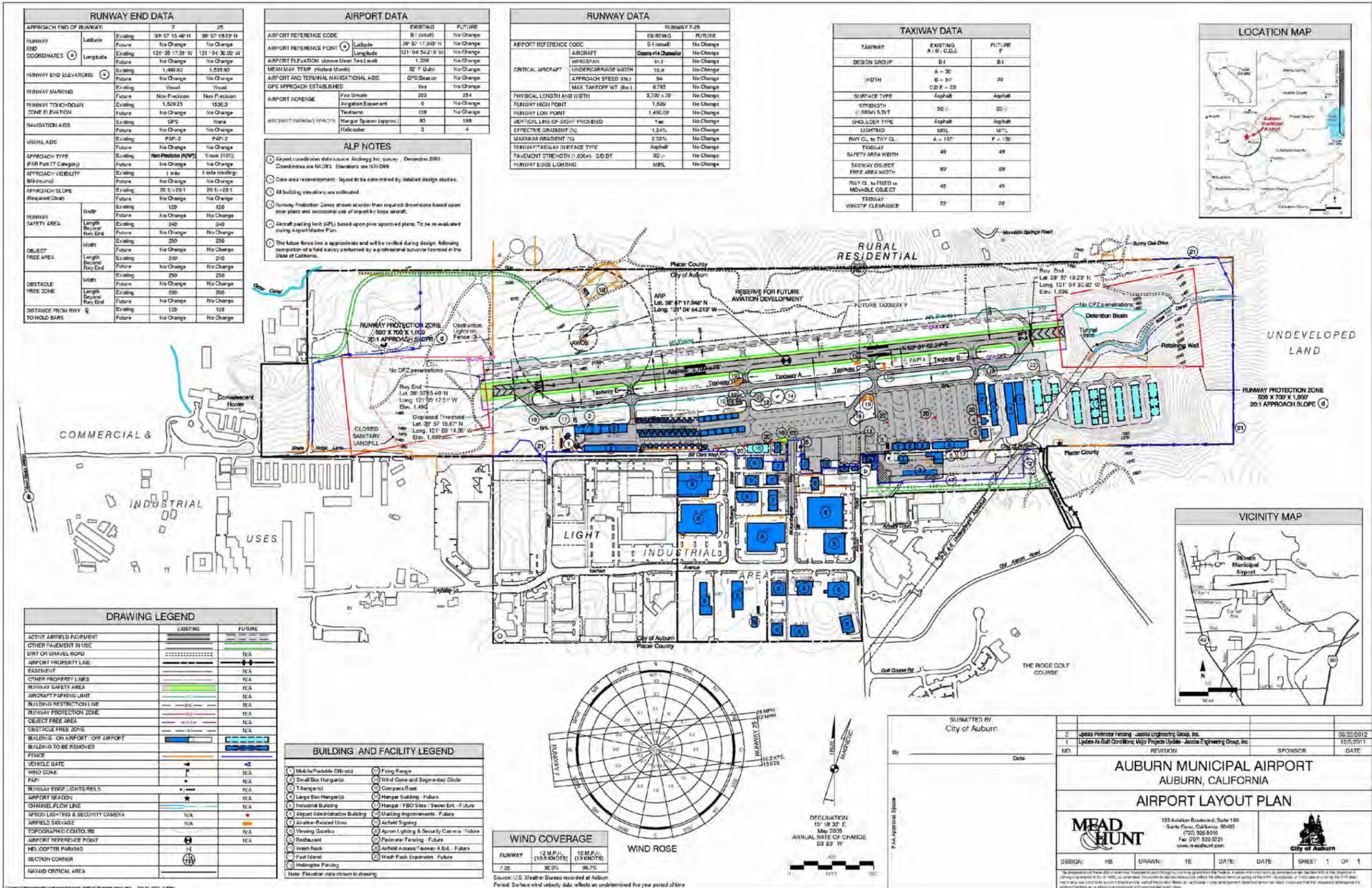
- *Airfield*
 - › Construct full-length parallel taxiway on north side of runway to serve future aviation development
 - › Establish nonprecision approach to Runway 25
- *Building Area*
 - › Construct additional hangars and helicopter spaces
 - › Acquire 1.0 acre to connect southeast hangar area to Old Airport Road

Exhibit 7A

Airport Features Summary

Auburn Municipal Airport

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RUNWAY END DATA			
APPROACH END OF RUNWAY		7	25
RUNWAY END COORDINATES (A)	Latitude	Existing 38° 57' 15.46" N	38° 57' 15.23" N
	Longitude	Existing 121° 04' 30.02" W	121° 04' 30.02" W
RUNWAY END ELEVATIONS (A)	Existing	1,490.00	1,235.90
	Future	No Change	No Change
RUNWAY MARKING	Existing	Visual	Visual
	Future	Non Precision	Non Precision
RUNWAY TOUCH-DOWN ZONE ELEVATION	Existing	1,529.25	1,536.0
	Future	No Change	No Change
NAVIGATION AIDS	Existing	GPS	None
	Future	No Change	No Change
VISUAL AIDS	Existing	PAPI-2	PAPI-2
	Future	No Change	No Change
APPROACH TYPE (FAA Part 77 Category)	Existing	Non Precision (NPV)	Visual (V)
	Future	No Change	No Change
APPROACH VISIBILITY (Minimum)	Existing	1 mile	1 mile (V)
	Future	No Change	No Change
APPROACH SLOPE (Required/Obst)	Existing	20:1 > 20:1	20:1 > 20:1
	Future	No Change	No Change
RUNWAY SAFETY AREA	Width	Existing 240	240
	Future	No Change	No Change
OBJECT FREE AREA	Width	Existing 250	250
	Future	No Change	No Change
OBSTACLE FREE ZONE	Width	Existing 250	250
	Future	No Change	No Change
DISTANCE FROM RWY TO HOLD BARS	Existing	125	125
	Future	No Change	No Change

AIRPORT DATA			
	EXISTING	FUTURE	
AIRPORT REFERENCE CODE	B1 (small)	No Change	
AIRPORT REFERENCE POINT (A)	Latitude 38° 57' 17.340" N	No Change	
	Longitude 121° 04' 54.213" W	No Change	
AIRPORT ELEVATION (Above Mean Sea Level)	1,326	No Change	
MEAN MAX TEMP. (Hottest Month)	52° F (July)	No Change	
AIRPORT AND TERMINAL NAVIGATIONAL AIDS	GPS/Deacon	No Change	
GPS APPROACH ESTABLISHED	Yes	No Change	
AIRPORT ACREAGE	Fee Simple	203	204
	Aviation Easement	0	No Change
AIRPORT MARKING SPACIS	Thresholds	128	No Change
	Heliporter	3	4

RUNWAY DATA			
	EXISTING	FUTURE	
AIRPORT REFERENCE CODE	B1 (small)	No Change	
AIRCRAFT	Class A14 (small)	No Change	
CRITICAL AIRCRAFT	WINGSPAN	11.3	No Change
	UNDERCARRIAGE WIDTH	10.8	No Change
APPROACH SPEED (kt)	94	No Change	
MAX TAKEOFF WT. (lbs.)	6,783	No Change	
PHYSICAL LENGTH AND WIDTH	3,700 x 75'	No Change	
RUNWAY HIGH POINT	1,539	No Change	
RUNWAY LOW POINT	1,450.00	No Change	
VERTICAL LINE OF SIGHT PROVIDED	Yes	No Change	
EFFECTIVE GRADIENT (%)	1.24%	No Change	
MAXIMUM GRADIENT (%)	2.50%	No Change	
RUNWAY/TAXIWAY SURFACE TYPE	Asphalt	No Change	
PAVEMENT STRENGTH (1,000psi) - C.D.D.T	30	No Change	
RUNWAY EDGE LIGHTING	MRL	No Change	

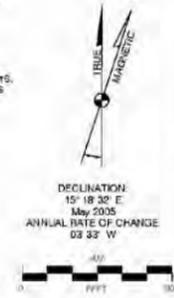
TAXIWAY DATA		
TAXIWAY	EXISTING A/B/C/D/E	FUTURE F
DESIGN GROUP	B1	B1
WIDTH	A = 30'	
	B = 50'	30'
	C/D/E = 35'	
SURFACE TYPE	Asphalt	Asphalt
	STRENGTH (1,000psi) S.D.T	30
SHOULDER TYPE	Asphalt	Asphalt
LIGHTING	MRL	MRL
RWY CL. TO RWY CL.	A = 150'	F = 150'
TAXIWAY SAFETY AREA WIDTH	49	49
TAXIWAY OBJECT FREE AREA WIDTH	60	50
RWY CL. TO FIXED OR MOVABLE OBJECT	45	45
TAXIWAY WING TIP CLEARANCE	20	20



DRAWING LEGEND		
	EXISTING	FUTURE
ACTIVE AIRFIELD PAVEMENT	—	—
OTHER PAVEMENT IN USE	—	—
DIRT OR GRAVEL ROAD	—	—
AIRPORT PROPERTY LINE	—	—
EASEMENT	—	—
OTHER PROPERTY LINES	—	—
RUNWAY SAFETY AREA	—	—
AIRCRAFT PARKING LIMIT	—	—
BUILDING RESTRICTION LINE	—	—
RUNWAY PROTECTION ZONE	—	—
OBJECT FREE AREA	—	—
OBSTACLE FREE ZONE	—	—
BUILDING ON AIRPORT / OFF AIRPORT	—	—
BUILDING TO BE REMOVED	—	—
FENCE	—	—
VEHICLE GATE	—	—
WIND CONE	—	—
PAPI	—	—
RUNWAY EDGE LIGHTS/RELS	—	—
AIRPORT SEASON	—	—
CHARMEL FLOW LINE	—	—
APRON LIGHTING & SECURITY CAMERA	—	—
AIRFIELD SERVICE	—	—
TOPOGRAPHIC CONTOURS	—	—
AIRPORT REFERENCE POINT	—	—
HELICOPTER PARKING	—	—
SECTION CORNER	—	—
NAVAID CRITICAL AREA	—	—

BUILDING AND FACILITY LEGEND	
Multiple Portable Offices	Filing Storage
Small Box Hangar(s)	Wind Cone and Segmented Circle
Thangars(s)	Compass Rose
Large Box Hangar(s)	Hangar Building - Future
Industrial Building	Hangar / FBO Sites / Sewer Ent. - Future
Airport Administration Building	Marking Improvements - Future
Aviation-Related Uses	Airfield Signage
Viewing Gallery	Apron Lighting & Security Camera - Future
Restaurant	Perimeter Fencing - Future
Wash Rack	Airfield Access/wayway A Ent. - Future
Fuel Island	Wash Rack Expansion - Future
Helicopter Parking	

WIND COVERAGE		
RUNWAY	12 M.P.H. (10.5 KNOTS)	15 M.P.H. (13 KNOTS)
7-25	92.0%	96.7%



SUBMITTED BY
 City of Auburn

By _____ Date _____

Scale: 1" = 100'

2	Update Preliminary Legend - Jacobs Engineering Group, Inc.	03/22/2012
1	Update As-Built Conditions, Major Project Update - Jacobs Engineering Group, Inc.	12/25/2011
180	REVISION	SPONSOR
AUBURN MUNICIPAL AIRPORT AUBURN, CALIFORNIA AIRPORT LAYOUT PLAN		
DESIGN: KB	DRAWN: TE	DATE: DATE
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BASED AIRCRAFT ^a			RUNWAY USE DISTRIBUTION ^a		
	Current	Future		Current	Future
<i>Aircraft Type</i>			<i>All Aircraft (including helicopters)</i>		
Single-Engine	197	261	Takeoffs		
Multi-Engine	10	20	Day		
Business jet	0	8	Runway 7	10%	no
Helicopter	3	5	Runway 25	90%	change
Total	210	290	Evening		
<hr/>			Runway 7	5%	no
AIRCRAFT OPERATIONS ^a			Runway 25	95%	change
	Current	Future	Night		
<i>Total</i>			Runway 7	5%	no
Annual	70,000	104,000	Runway 25	95%	change
Average Day	191	285	Landings		
<i>Distribution by Aircraft Type</i>			Day		
Single-Engine	88%	82%	Runway 7	10%	no
Multi-Engine	7%	11%	Runway 25	90%	change
Turboprop	3%	4%	Evening		
Business Jet	<1%	<1%	Runway 7	50%	no
Helicopter	1%	3%	Runway 25	50%	change
<i>Distribution by Type of Operation</i>			Night		
Local (incl. touch-and-goes)	25%	no	Runway 7	50%	no
Itinerant	75%	change	Runway 25	50%	change
<hr/>			Touch-and-go		
TIME OF DAY DISTRIBUTION ^a			Day		
	Current	Future	Runway 7	10%	no
<i>All Aircraft (including helicopters)</i>			Runway 25	90%	change
Day (7 am to 7pm)	90%	no	<hr/>		
Evening (7 pm to 10 pm)	8%	change	FLIGHT TRACK USAGE ^a		
Night (10 pm to 7 am)	2%			Current	Future
<hr/>			<i>All Aircraft (including helicopters)</i>		
			Takeoffs		
			Runway 7, Straight Out	100%	no change
			Runway 25, 20° Left Turn	100%	no change
			Landings		
			Runway 7, Straight In	100%	no change
			Runway 25, Left Turn	100%	no change
			Touch-and-go		
			Runway 7, Left Turn	100%	no change
			Runway 25, Left Turn	100%	no change
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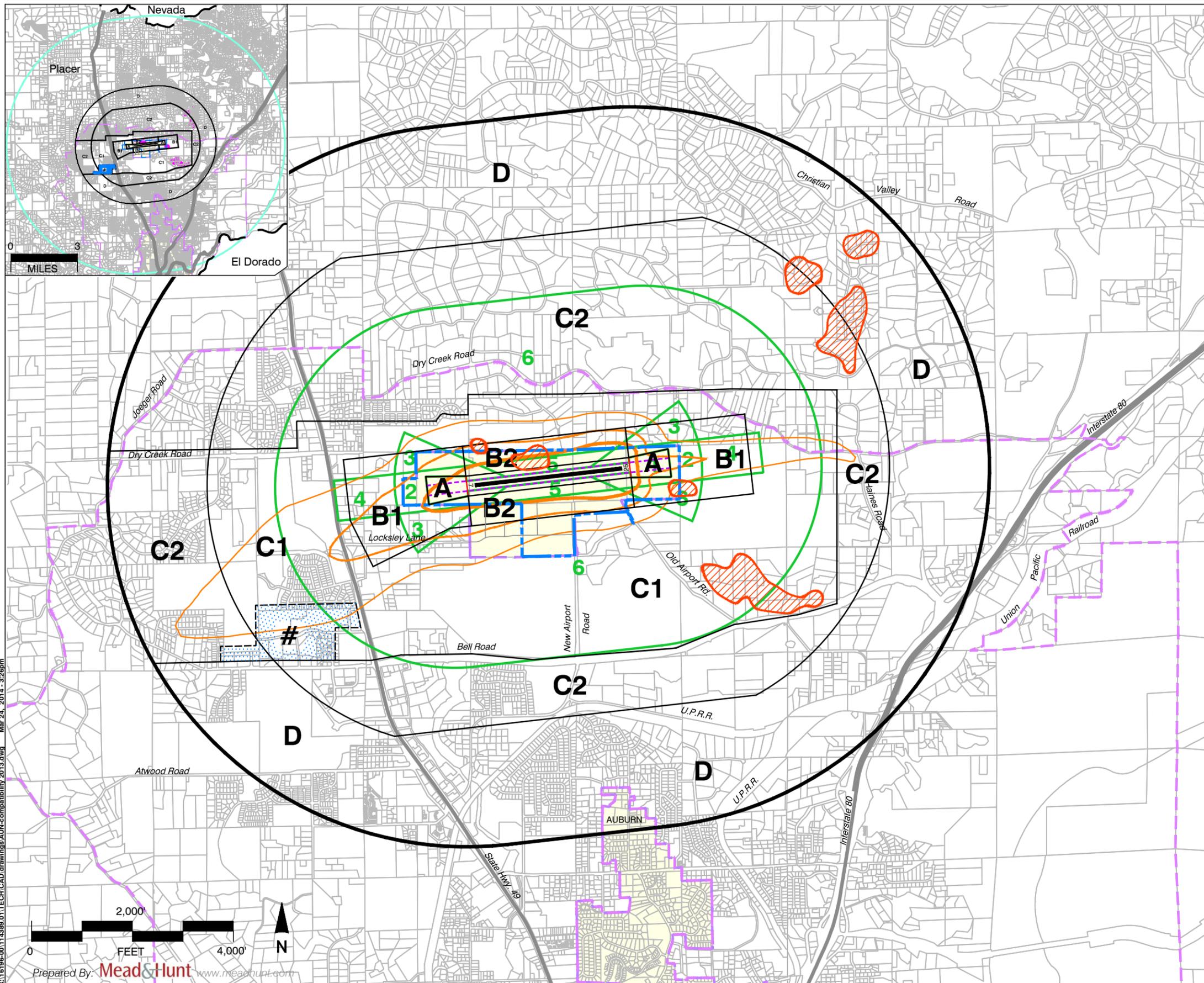
NOTES

^a Source: Current (2012) and future (2033) aircraft activity data brought forward from the Auburn Municipal Airport Master Plan (2007) and verified by airport management for use in this ALUCP.

Exhibit 7C

Airport Activity Data Summary
Auburn Municipal Airport

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Legend

Boundary Lines

- Placer County Limits (outside map view)
- Auburn City Limits
- Auburn Sphere of Influence
- Airport Property Line
- Existing Runway 7-25 (3,700 ft.)
- Airport Influence Area (Adopted 2014)
- Compatibility Policy Zones (Adopted 2014)

Height Review Overlay Zone

See Special Conditions Policy 4.2.3.

Runway Factors¹

- Runway Protection Zone (RPZ)
- Runway Object Free Area (ROFA)

Noise Factors

- 65 dB CNEL
- 60 dB CNEL
- 55 dB CNEL

} 104,000 Annual Operations¹

Safety Factors

- Generic Safety Zones (Short General Aviation Runway)²
 - Zone 1, Runway Protection Zone
 - Zone 2, Inner Approach/Departure Zone
 - Zone 3, Inner Turning Zone
 - Zone 4, Outer Approach/Departure Zone
 - Zone 5, Sideline Zone
 - Zone 6, Traffic Pattern Zone

- Notes:**
1. Source: Auburn Municipal Airport Master Plan, adopted July 2007.
 2. Source: California Airport Land Use Planning Handbook published October 2011. Zone 1 modified to reflect RPZs.
 3. Source: Federal Aviation Regulation (FAR) Part 77, Safe, Efficient Use and Preservation of Navigable Airspace (January 2011).

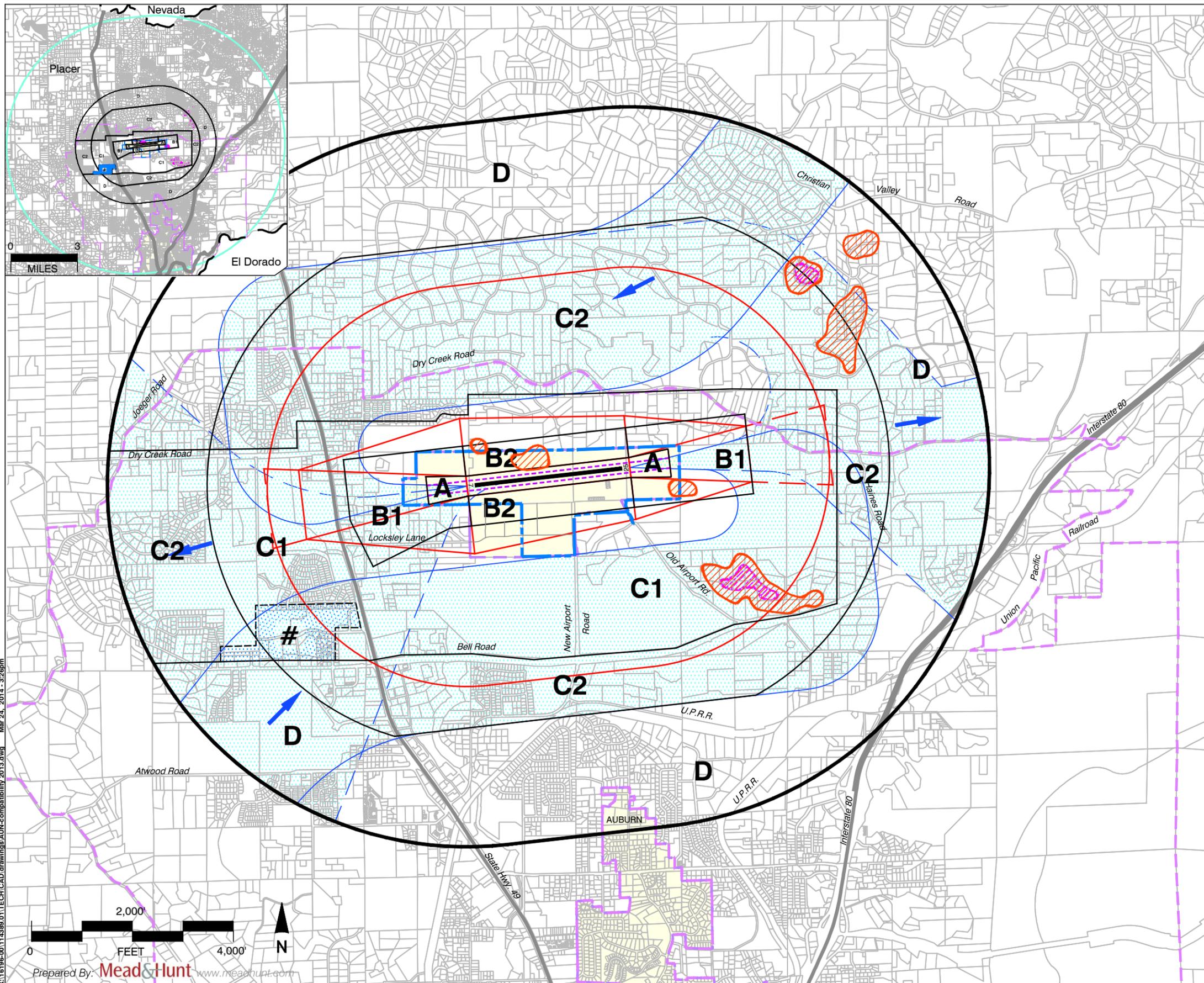
**Auburn Municipal Airport
Land Use Compatibility Plan
(Adopted February 26, 2014)**

**Exhibit 7D
Compatibility Factors Map:
Noise and Safety
Auburn Municipal Airport**

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Prepared By: **Mead & Hunt** www.meachunt.com



Legend

Boundary Lines

- Placer County Limits
- Auburn City Limits
- Auburn Sphere of Influence
- Airport Property Line
- Existing Runway 7-25 (3,700 ft.)
- Airport Influence Area (Adopted 2014)
- Compatibility Policy Zones (Adopted 2014)
- Height Review Overlay Zone
- See Special Conditions Policy 4.2.3.

Runway Factors¹

- Runway Protection Zone (RPZ)
- Runway Object Free Area (ROFA)

Airspace Factors

- FAA Height Notification Boundary² (20,000 ft. radius; 100 to 1 slope; see inset)
- FAA Obstruction Surfaces²
- FAA Obstruction Surface Terrain Penetrations

Overflight Factors

- General Traffic Pattern Envelope/Flight Direction³ (approximately 80% of aircraft overflights estimated to occur within these limits)

- Notes:**
1. Source: Auburn Municipal Airport Master Plan, adopted July 2007.
 2. Source: Federal Aviation Regulation (FAR) Part 77, Safe, Efficient Use and Preservation of Navigable Airspace (January 2011).
 3. Source: Placer County Airport Land Use Compatibility Plan, adopted October 2000 and airport management.

**Auburn Municipal Airport
Land Use Compatibility Plan
(Adopted February 26, 2014)**

**Exhibit 7E
Compatibility Factors Map:
Airspace and Overflight
Auburn Municipal Airport**

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Prepared By: **Mead & Hunt** www.meadhunt.com

AIRPORT SITE

- *Location*
 - › Central Placer County
 - › 3 miles north of central Auburn
- *Topography*
 - › Situated in the foothills of the Sierra Nevada Mountain Range
 - › Gently rolling terrain in airport vicinity with generally higher elevations toward east and northeast

AIRPORT ENVIRONS LAND USE JURISDICTIONS

- *County of Placer*
 - › Runway approaches and traffic pattern over unincorporated Placer County
- *City of Auburn*
 - › Airport property and portion of adjacent industrial land are noncontiguous with incorporated area of city
 - › Nearest portion of city proper approximately 1 mile south
 - › Entire area up to approximately 0.5 mile north of airport in city sphere of influence

EXISTING AIRPORT AREA LAND USES

- *General Character*
 - › Predominantly rural residential to north and east
 - › Urban uses to west and south
- *Runway Approaches*
 - › East (Runway 25): Rural residential; open space
 - › West (Runway 7): Industrial near runway end; convalescent hospital on centerline 0.4 miles from runway end; mobile home park 0.7 miles; ball fields 1.0 mile; school site 1.2 miles; residential subdivision 1.3 miles; hospital 1,1 miles southwest
- *Traffic Pattern*
 - › Southwest: Mixed commercial, office and light industrial along Hwy 46; residential beyond
 - › South: Park/reservoir; golf course; residential, places of worship, schools south of Bell Road
 - › Southeast, North & Northeast: Rural residential

PLANNED AIRPORT AREA LAND USES

- *County of Placer*
 - › Additional industrial development west, northwest, and southeast of runway; commercial to southwest
 - › More low-density residential 1-2 miles west
 - › Continued rural estate (4.6-acre minimum lot size) immediately east; rural residential (2.3-acre minimum lots) farther east and to north
 - › Open space and golf course remain on south
- *City of Auburn*
 - › Additional industrial development within city limits on both sides of runway
 - › Planned land use designations for unincorporated areas within city sphere of influence mostly same as county plans

STATUS OF COMMUNITY PLANS

- *County of Placer*
 - › General Plan Policy Document and General Plan Land Use Diagram approved May 21, 2013
 - › Auburn/Bowman Community Plan adopted June 1994; updated 1999; addresses unincorporated area around airport
- *City of Auburn*
 - › General Plan adopted November 1993
 - › Housing Element adopted December 2008

ESTABLISHED AIRPORT COMPATIBILITY MEASURES**County of Placer**

- *General Plan*
 - › Requires 2,000- ft. buffer between airports and new residential development (Land Use and Circulation, Section 4.B.1.)
 - › County shall work with ALUC to ensure protection of airports from urban encroachment (Transportation 3.F.2.)
 - › Ensure new development around airports does not create safety hazards (Airport Hazards, 8.D.1); Limit land uses in airport safety zones consistent with ALUC plans (Airport Hazards, 8.D.2)
 - › Ensure development within the airport approach and departure zones complies with FAR Part 77 regulations (Airport Hazards, 8.D.3)
 - › All development projects within airport overflight zone to be reviewed for consistency with ALUC plan (Airport Hazards, 8.8)
 - › Prohibits new residential and other noise-sensitive land uses in areas exposed to more than 60 dB CNEL unless mitigated to reduce impacts to outdoor activities; indoor noise level cannot exceed 45 dB CNEL; acoustical analysis required (Noise, 9.A.8)
- *Auburn/Bowman Community Plan*
 - › 1987 Auburn Airport Comprehensive Land Use Plan (CLUP) adopted by reference
 - › Ensure that new development is consistent with CLUP (Community Goal 11)
- *Airport Overflight Combining District (17.52.030)*
 - › Ordinance sets noise, safety, and height compatibility requirements and requires discretionary land use permits applications to be submitted to ALUC for review

City of Auburn

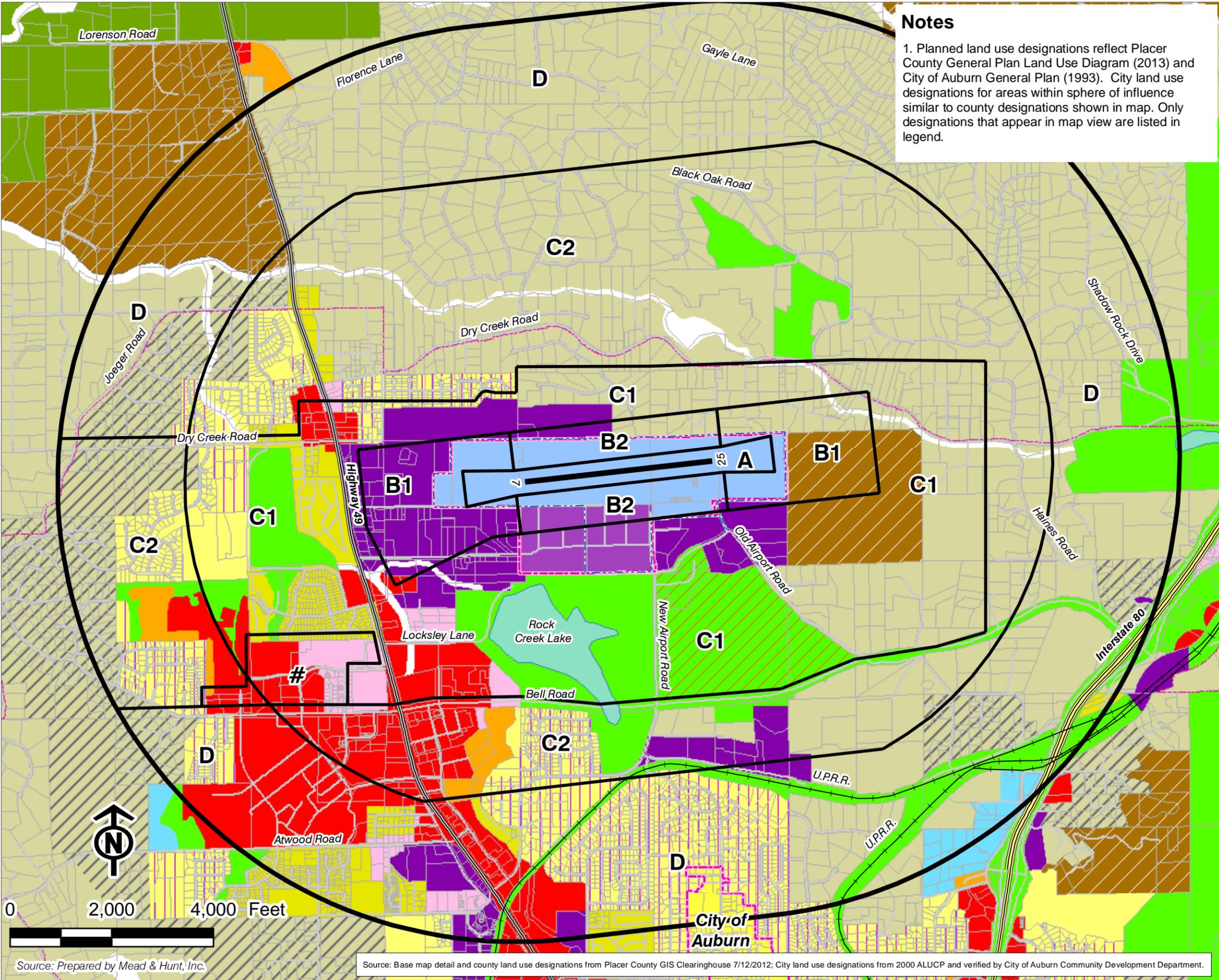
- *General Plan*
 - › 1987 Auburn Airport Comprehensive Land Use Plan adopted by reference
 - › City to continue participation in ALUC
 - › General Plan contains same policy on new noise sensitive development as in county general plan
- *Airport Industrial Design Control District (AI-CD)*
 - › Ordinance and Standards and Regulation in Auburn Airport Industrial Park sets airport-related height limits

Exhibit 7F

Airport Environs Information

Auburn Municipal Airport

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Notes
 1. Planned land use designations reflect Placer County General Plan Land Use Diagram (2013) and City of Auburn General Plan (1993). City land use designations for areas within sphere of influence similar to county designations shown in map. Only designations that appear in map view are listed in legend.

- Legend**
- Placer County Boundary (outside map view)
 - Auburn City Limits
 - Auburn Sphere of Influence
 - Airport Property Line
 - Existing Runway 7-25 (3,700 ft.)
 - Airport Influence Area (Adopted 2014)
 - Compatibility Policy Zones (Adopted 2014)
- Planned Land Use Designations (County)**¹
- Agricultural 10 - 80 Ac. Min.
 - Commercial
 - High Density Residential 10 - 15 DU/Ac.
 - Industrial
 - Low Density Residential 0.4 - 0.9 Ac. Min.
 - Low Medium Density Residential 2 - 5 DU/Ac.
 - Medium Density Residential 5 - 10 DU./Ac.
 - Mixed Use
 - Open Space
 - Open Space / Business Park
 - Professional Office
 - Rural Estate 2.3 - 10 Ac. Min.
 - Rural Estate 4.6 - 10 Ac. Min.
 - Rural Low Density Residential 0.9 - 2.3 Ac. Min.
 - Rural Residential 2.3 - 4.6 Ac. Min.
- Planned Land Use Designations (City)**¹
- Low Density Residential 1.1 - 5.0 DU/Ac.
 - Industrial
 - Public

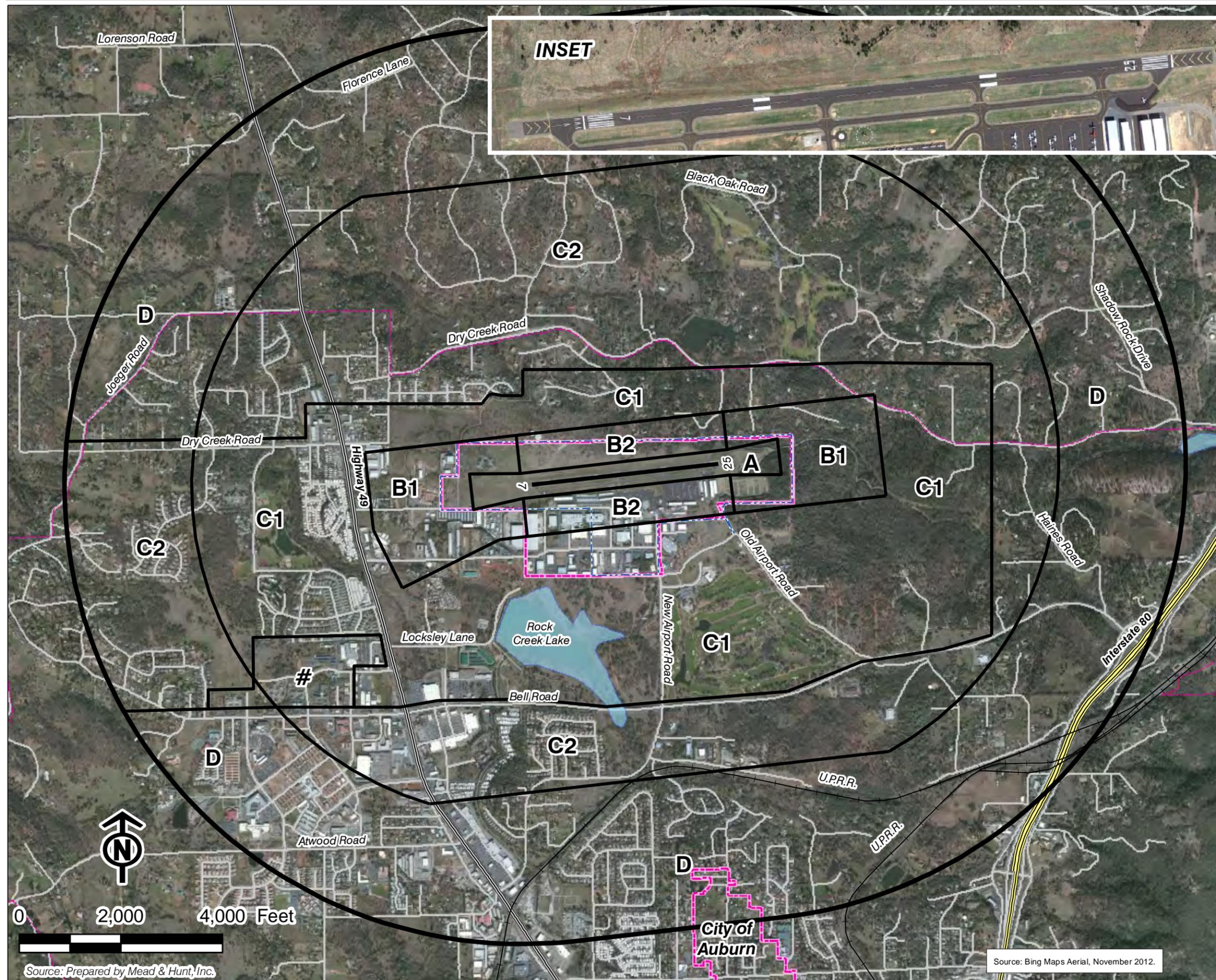
**Auburn Municipal Airport
 Land Use Compatibility Plan**
 (Adopted February 26, 2014)

Exhibit 7G

**General Plan Land Uses
 Auburn Municipal Airport**

Source: Prepared by Mead & Hunt, Inc.

Source: Base map detail and county land use designations from Placer County GIS Clearinghouse 7/12/2012; City land use designations from 2000 ALUCP and verified by City of Auburn Community Development Department.



- Legend**
- Placer County Boundary (outside map view)
 - Auburn City Limits
 - Auburn Sphere of Influence
 - Airport Property Line
 - Existing Runway 7-25 (3,700 ft.)
 - Airport Influence Area (Adopted 2014)
 - Compatibility Policy Zones (Adopted 2014)

**Auburn Municipal Airport
Land Use Compatibility Plan
(Adopted February 26, 2014)**

Exhibit 7H

**Aerial
Auburn Municipal Airport**

Source: Prepared by Mead & Hunt, Inc.

Source: Bing Maps Aerial, November 2012.