

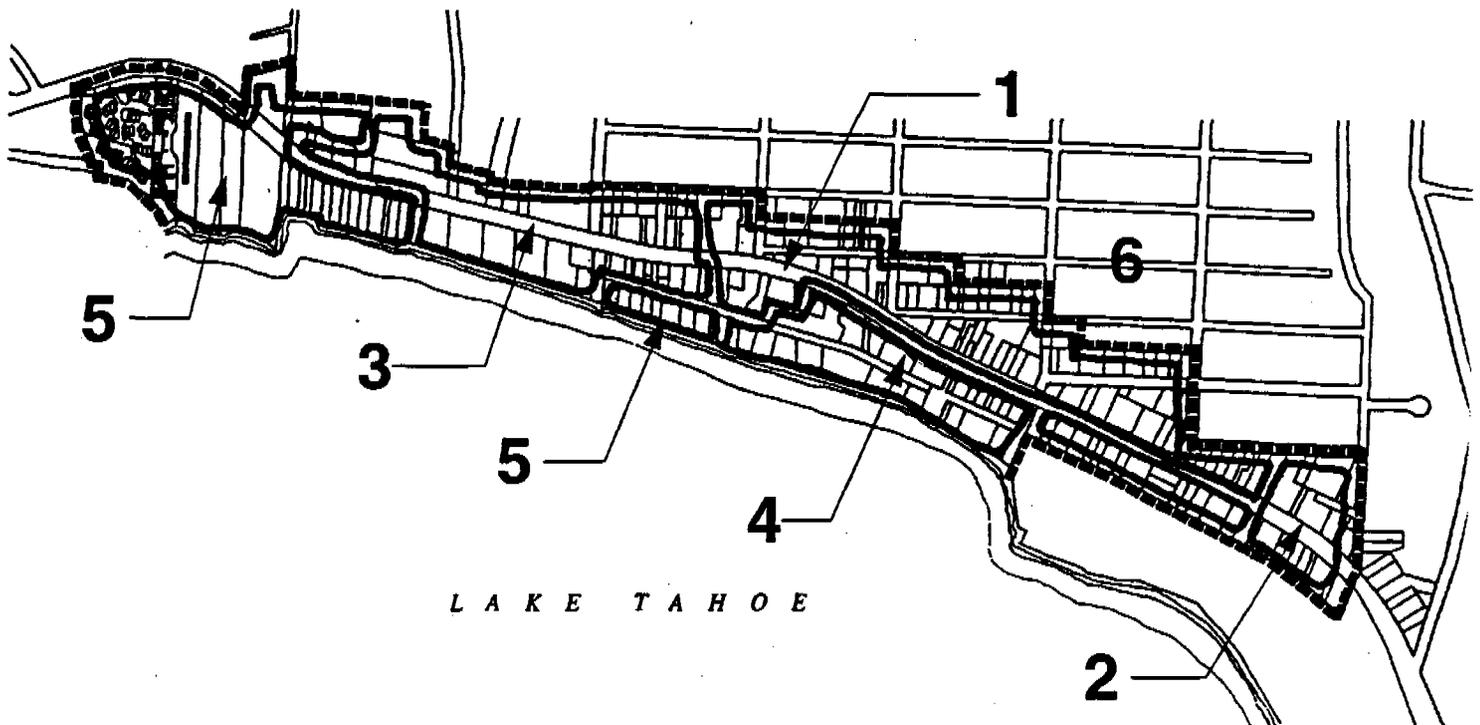
CHAPTER 19 KINGS BEACH COMMUNITY PLAN

This chapter presents design standards and guidelines specific to the Kings Beach Commercial Community Plan Area. It is based on a set of goals and design and implementation principles taken from Chapter I of the Community Plan.

The overall design concept for Kings Beach is based on building off the existing small town main street and the existing recreation amenities. The spine or main street of the this commercial area is State Route 28. The focus of Kings Beach is the State Beach and the downtown commercial district. On the east and west sides are transition or entry areas into the downtown area.

The Kings Beach Community Plan is divided into the following design areas:

1. Downtown Commercial Area
2. East Entry Area
3. West Entry Area
4. State Park Area
5. Beach Street Tourist/Residential Area
6. Residential Area



Design Concepts

Downtown Commercial Area: The size and scale of the commercial district and its improvements reflect that of a pedestrian oriented area.

All the streets are to be designed to maintain a pedestrian feeling of space, e.g. building to street ratios of no less than 1 foot of building height to 3 feet street width, low level lighting, sidewalks, pedestrian oriented signs, attractions and parking within walking distances, etc. The buildings on the street frontages use a door-window-door-window facade presentation to promote pedestrian shopping.

The key attractions upon which the district builds are the North Tahoe Conference Center and its motel district, the Kings Beach State Park and its recreation amenities, small attractors such as the Post Office, and the existing mass of resident/tourist commercial uses. The concept is to build on these anchors and to create an additional attraction of an arts and crafts colony all within walking distance.

The key facilities and improvements are noted in the Community Plan. Typical street sections are also provided as guidance for review of public and private projects in the area.

East and West Entry Areas: The design concept for the east and west ends of the commercial area is to provide a sense of entry. The design of the area is that of a boulevard. Boulevard design relies on landscaping, medians, and setbacks.

Standards

The standards presented in this chapter supplement the standards presented in previous chapters. If there is a conflict with the earlier chapters, the standards of this chapter shall apply. These standards may be adjusted to preserve natural features or to reduce conflicts with other standards. Set-back standards on S.R. 28 may only be reduced upon making the required findings of the TRPA Code for scenic corridors.

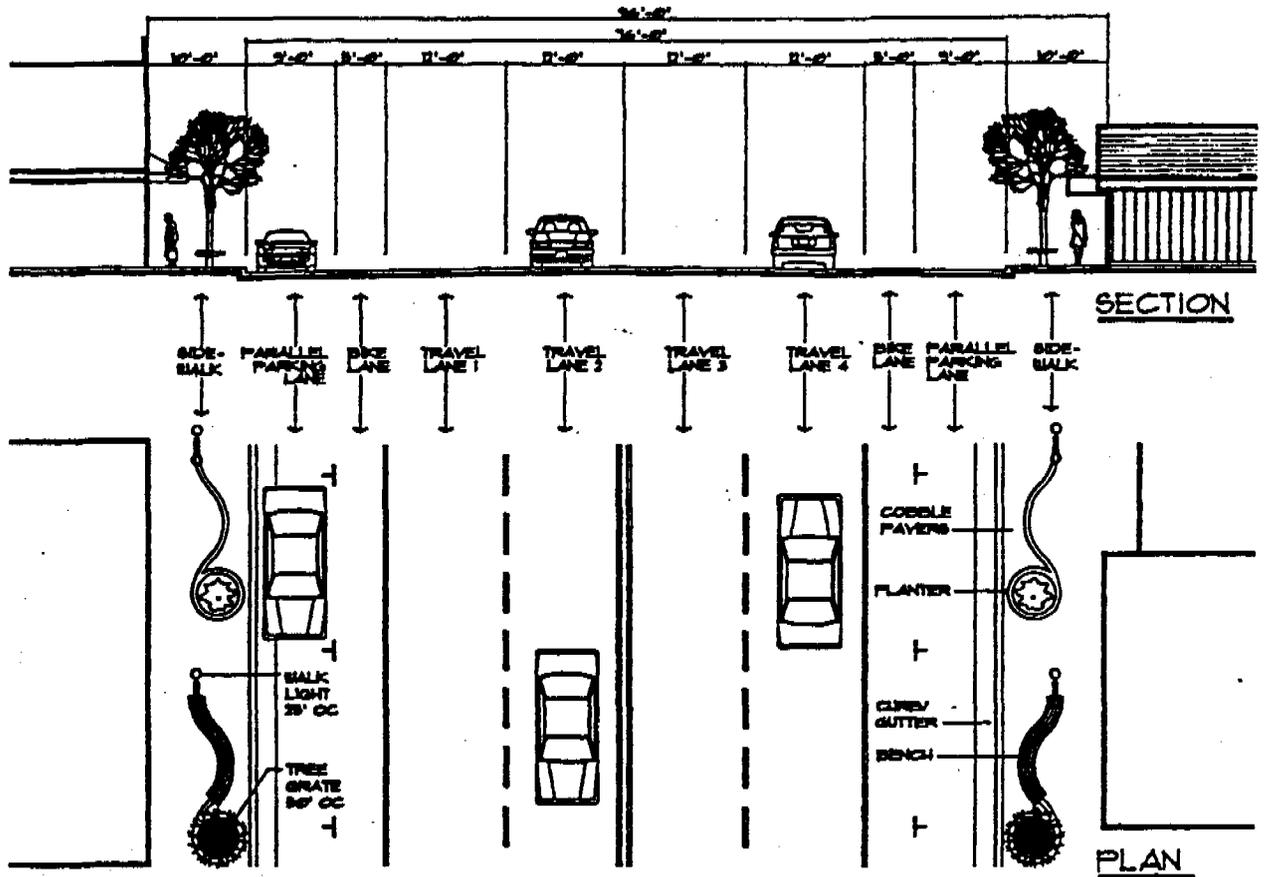
Projects subject to the requirements of this chapter shall be conditioned to meet the following standards where applicable. The specifications for improvements, e.g. street lights, pavers, etc. shall be established by Placer County or approved areawide improvement plans.

- A. *Projects fronting State Route 28 Downtown shall provide the following improvements for the roadway frontage (See Guidelines for recommended design details.):*
- (1) Ten feet wide concrete and paver sidewalk.*
 - (2) Eight inch vertical concrete curbs or as required by Caltrans.*
 - (3) Street trees with grates planted 50 feet on center.*
 - (4) Pedestrian street lights 12' high 50 feet on center.*
 - (5) Ten feet building setback form the property line.*
- B. *Projects fronting State Route 28 Entry Areas and Other Areas shall provide the following improvements for the roadway frontage (See Guidelines for recommended design details):*
- (1) Six to eight feet wide paver sidewalk.*

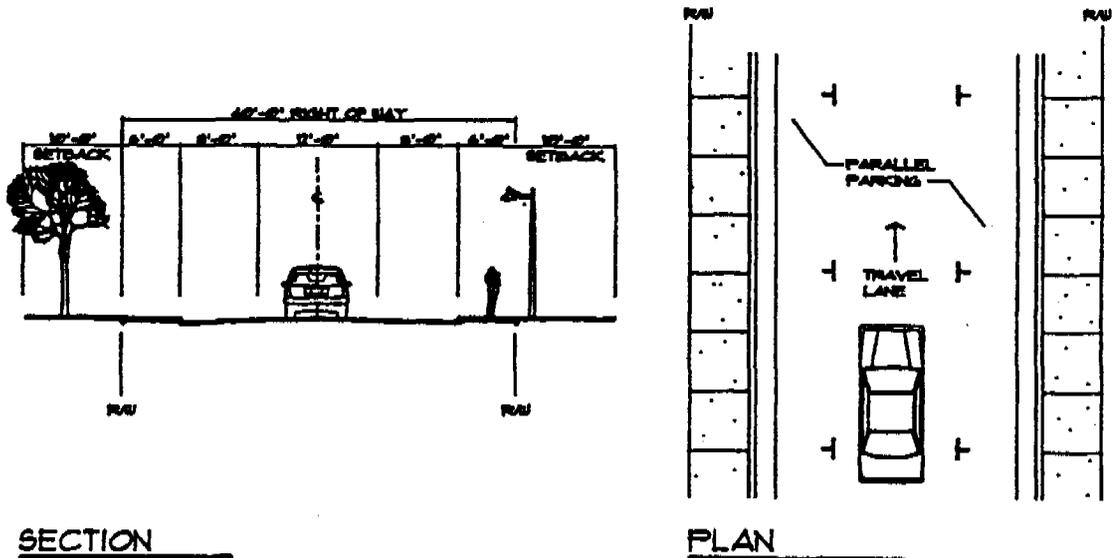
- (2) *Six inch vertical concrete curbs or as required by Caltrans.*
 - (3) *Street trees planted 50 feet on center or pockets of shrubs planted 25 feet on center or a combination of both subject to an approved landscape plan*
 - (4) *Pedestrian street lights 12' high 50 feet on center or low level lights 25' on center.*
- C. *Projects fronting other public right-of-ways within the Community Plan shall provide on the frontage (See Guidelines for recommended design details.):*
- (1) *Six feet wide minimum concrete sidewalks.*
 - (2) *Pedestrian street lights 12' high 50 feet on center.*
 - (3) *Landscaping in setback area.*
 - (4) *Six inch vertical; concrete curbs or as required by Placer County.*
- E. *Construction of new buildings or expansion of existing buildings shall not exceed the minimum buildings setback requirement from the property lines as follows:*
- (1) *Front - as specified in the typical section or 20' if not specified.*
 - (2) *Side - no side setback on adjoining commercial uses and 10' on properties adjoining noncommercial uses.*
 - (3) *rear - as specified in the typical section or 20' if not specified.*
- F. *Projects with existing coverage in excess of 75% of their project area, shall be required to provide an increase in landscaping equal to 5% of the project area. The landscaping requirement shall be met within the project area or, if not feasible, off-site in a related area. This condition may be waived by the Design Review Committee, if the project is part of an assessment district which is providing the required increase in landscaping or the landscaping requirement has been met by a previous approval.*
- G. *Projects located between the designated scenic corridors and Lake Tahoe or the Truckee River shall not cause a reduction of the views of Lake Tahoe from the corridors. TRPA may consider as an alternative, off-site improvements if it is determined there is a net increase in the lake views within the scenic unit.*

Guidelines

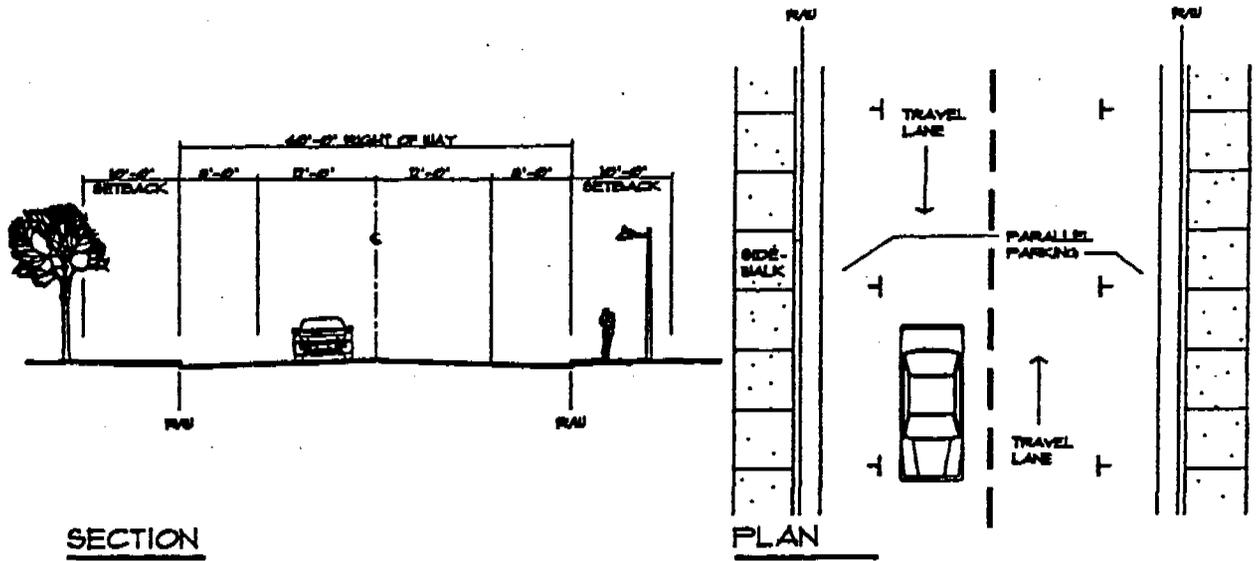
- A. **The following figures and text display the design standards which are to be applied to the Kings Beach Community Plan. Specific standards may be established by Placer County Department of Public Works or approved areawide improvement plans.**
- (1) **State Route 28 Typical Cross Section for the Downtown Commerical Area:** Projects in/or fronting S.R. 28 in the Downtown Area should consider these basic elements: Four travel lanes, 10' building setback from the property line, bike lanes, 10-15' sidewalks, parallel parking, lighting and landscaping.



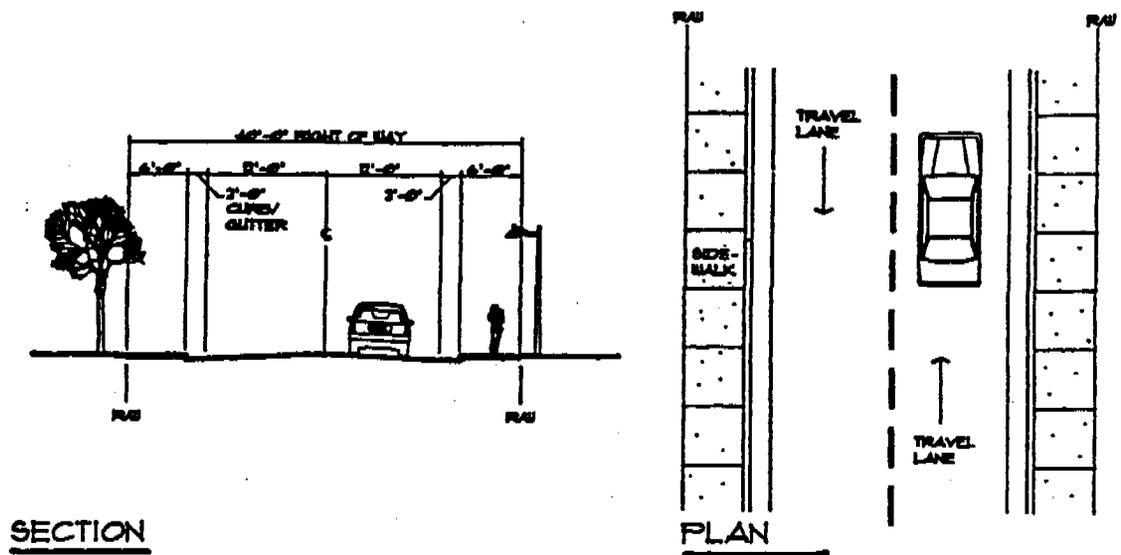
(2) **Brook Street Typical Cross Section:** Projects in/or fronting Brook Street between Bear Street and Coon Street should consider these basic elements: One way/one travel lane, 10' building setbacks from the property line, 6' sidewalks, parallel parking, lighting and landscaping.



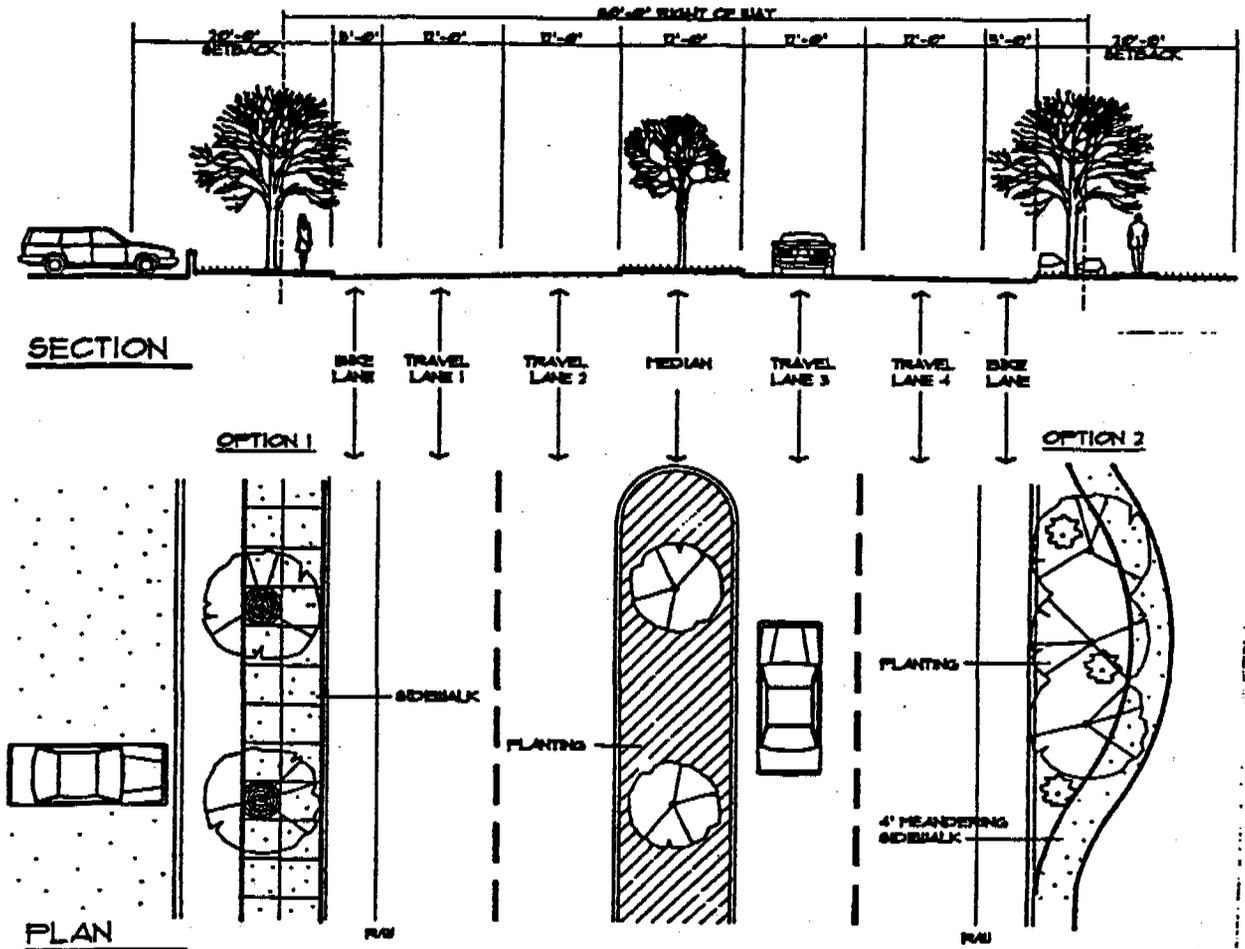
- (3) **Other Streets Typical Cross Section:** Projects in/or fronting other streets in the Downtown Area should consider these basic elements: Two travel lanes, 10' building setbacks from the property line, 6' sidewalks, parallel parking, lighting and landscaping.



- (4) **No Parking Option (local streets with no permitted parking):** Two 12' travel lanes, 10' building setbacks from the property line, 6' sidewalks, lighting and landscaping.

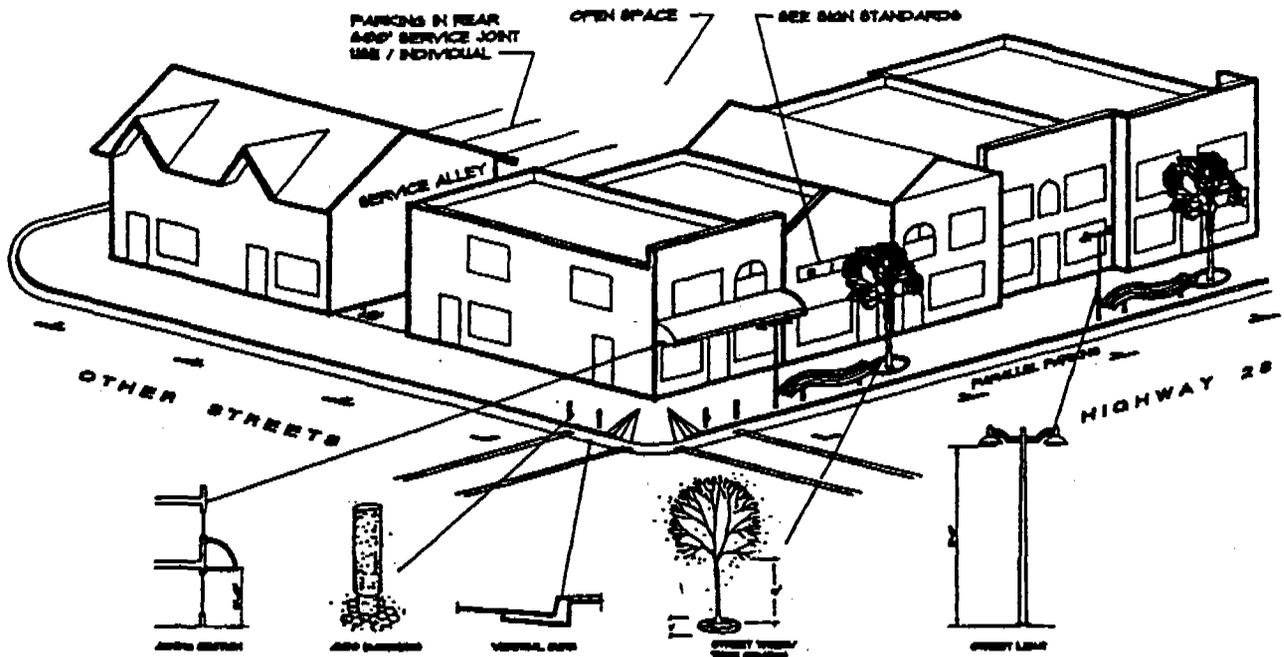


- (5) **State Route 28 Typical Cross Section for the Entry Area:** Projects fronting S.R. 28 in the entry areas or other areas should consider these basic elements: Four travel lanes, median divider or center turn lane, 20' building setbacks from the property line, limited access, no parking, 6' sidewalk (option 1 or 2), lighting and landscaping.



All intersection "improvements" which include additional turn lanes must be evaluated at the time of project approval for their specific scenic effects. Mitigation measures which may be required at that time include: use of planted medians at the approaches to turn pockets, use of dark colored signal poles, and lane width reduction.

- B. **Typical Improvements in the Downtown Commercial Area:** The following illustrates application of the standards to typical area in the Downtown Commercial area.



- C. **Architectural Guidelines:** The following should be considered in the design of buildings within the Kings Beach Community Plan:

(1) **Architectural Theme**

Small town in the Sierras
 Old Tahoe style - friendly rustic atmosphere
 Rustic mountain village
 Natural colors of a mountain setting
 Blend with mountains, tall pines, and Lake
 Pedestrian - inviting, come in
 Main Street - Tahoe style
 Informal, friendly, folksy, charming
 Colony of the arts - quirky, eclectic, unusual

(2) **Styles**

Balconies, dormers, bay windows, facades
 Roof pitches 4:12 thru 12:12
 Reference to existing old Tahoe styles for clip-gambrel roofs, shed roofs

dormers, log details, upper floor balconies, and covered walkways
Window details, shutters, awnings, window boxes, and planters
Non-reflective materials, natural color:

(3) Encouraged Materials

Wood siding - horizontal vertical, and diagonal
Wood shingles and shake
Log siding and details
Brick as building or as accent
River rock as accent
Stucco as accent panel or detail above windows
Green poles as columns, railings, and posts
Roofing - shake, shingle, composition, and colored metal
Windows - wood, metal in dark anodized or white vinyl with wood trim

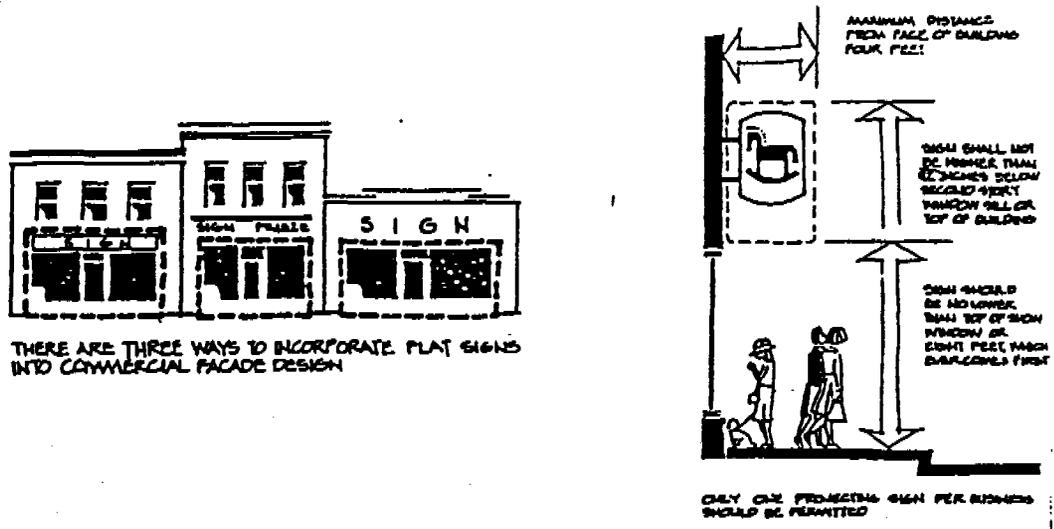
(4) Discouraged Materials

Flat roofs, aluminum or metal (not colored or textured) siding
Glass or glass panel and curtain wall systems
Plastic, interior lighted signs and details
Aluminum or steel windows not anodized or enameled
Ornamental metal railings or details
Concrete block or poured concrete without texture

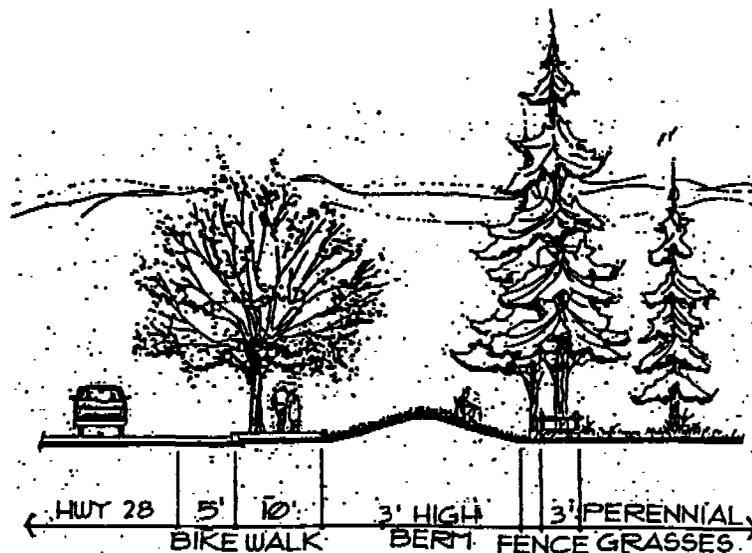
(5) Colors

Base or Body Colors - Browns (dark to light), dark greens, dark blues, gray,
brick red to dark maroon reds
Accent Colors (trim) - black, white, greens, blues, dark reds, rust, mustard,
metallic silver, gold, bronze
Awning Colors - yellow, light green, blues, reds
Roof Colors - browns, tans, greens

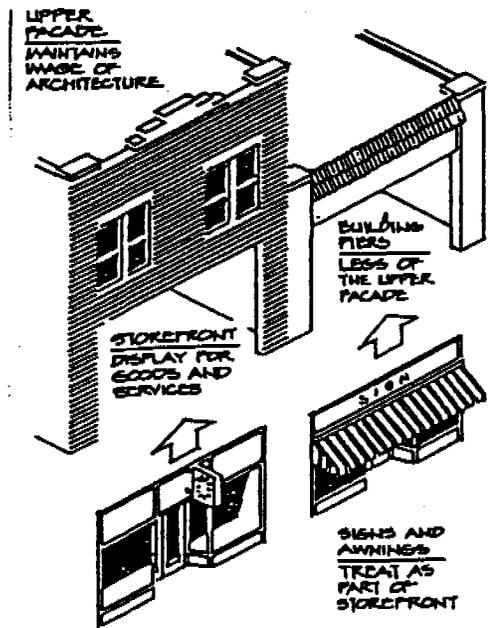
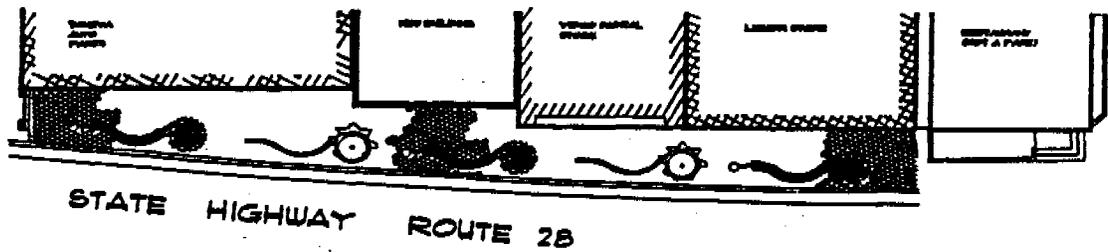
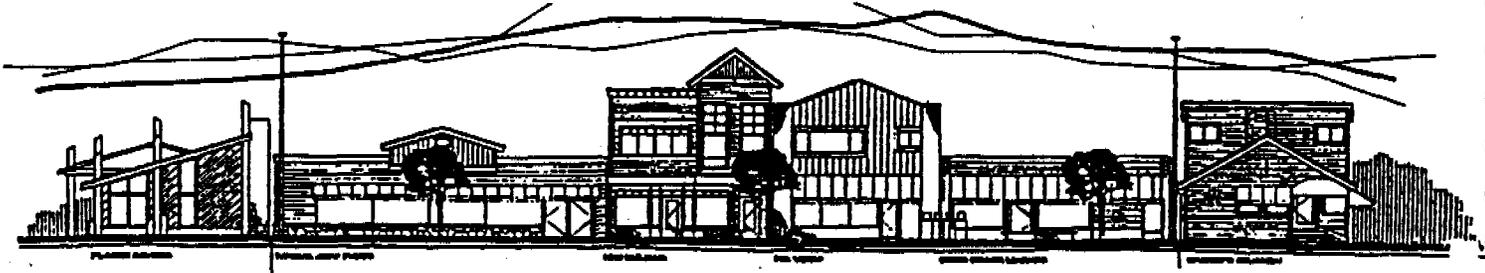
- D. **Signage:** Graphic simplicity and compatibility with building architecture are the basic principles of designing an effective and attractive system of signage.



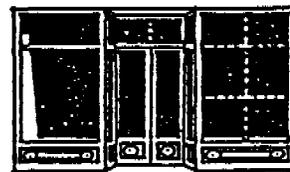
- E. **Parking Lots:** Shared use community parking lots are preferred and should be located within 300 feet of the businesses served. Screened parking lots located in the rear are preferred.
- F. **Transit Stops:** Transit bus stops should be located on each block (300' to 400' walking distance to the businesses served) fronting State Route 28 as recommended by TART. Each stop will provide shelters consistent with the surrounding design theme and shall provide sufficient width of sidewalk (20'-25') to accommodate riders.
- G. **Parking on the State Route 28:** There should be parallel parking in the Downtown Area right-of-way except in front of the State Park where it is discouraged to promote the view.



- H. Store Fronts: The basic retail units (approx. 25' to 50' in width) should be side to side with service delivery and parking in the rear. Buildings fronting State Route 28 should be two story with a variety of integrated facades. Where outdoor art is to be displayed, it should be arranged in such a way as to avoid clutter and to create a "gallery" effect.

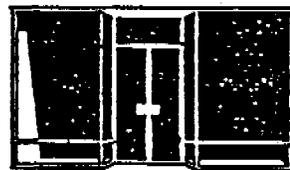


THE TYPICAL COMMERCIAL FACADE IS MADE UP OF TWO PARTS: 1) THE STOREFRONT 2) THE UPPER FACADE



TRADITIONAL STOREFRONT

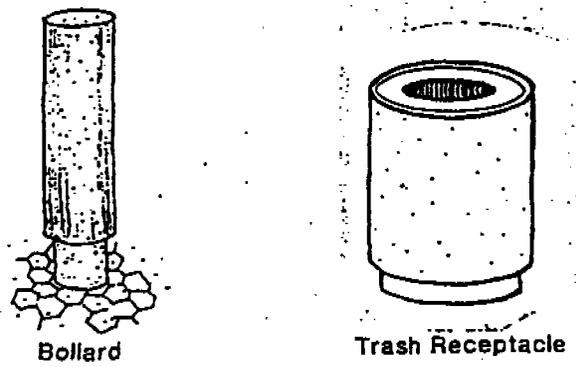
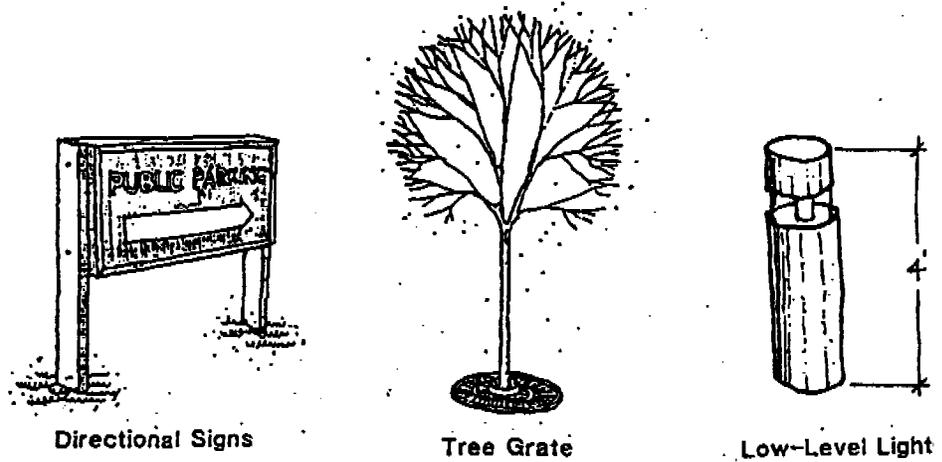
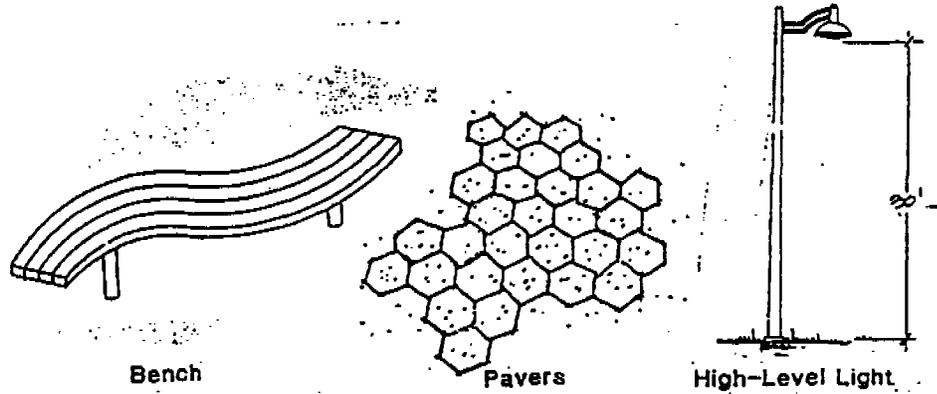
- STREET NUMBER
- TRANSOM WINDOW OVER ENTRANCE
- SHOW WINDOW MULTI-PANE IN OLDER BUILDINGS
- ORNAMENTATION USE OF EXOTIC MATERIALS
- RECESSED ENTRANCE



CONTEMPORARY STOREFRONT

- THIN DIMENSION OF WINDOW SASH AND DOOR JAMB
- MAXIMUM AMOUNT OF SHOW WINDOW
- APPLIED METAL FRAMING THROUGHOUT STOREFRONT
- SHOW WINDOW EXTENDS TO SIDEWALK

I. **Street Improvements:** The street amenities established by the N. T. Conference Center should be placed through out the district to provide unity. Placer County shall establish the curb line and flow lines for all frontages consistent with the cross sections.



- J. **Rear Frontage:** Side and rear frontages should provide for attractive facades.

REAR ELEVATION



- K. **Trees:** When planted trees should be 6 to 12 feet tall. Tree wells can be covered with iron gratings or planted with annuals for color. Tree should be varied in type and spacing to enhance and complement the facade, awnings and shape of building, and also complement each other in color and shape. Spacing should be 50' on center starting at the east corner on east/west streets and starting at the north corner on the north/south streets. The recommended trees are those listed below or similar trees.

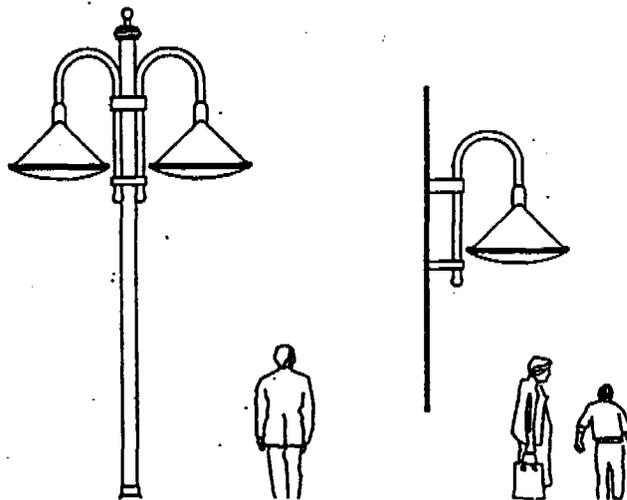
- (1) **Mountain Ash:** Fast growing up to 20-25' then slower growth to forty feet. Drought tolerant, columnar bright yellow Fall foliage. Best for 2-story buildings and can be planted close to street.
- (2) **Scarlet Maple, Norway Maple, and Japanese Maple:** Choose smaller species. Wide spreading shade trees, need some water to get established. Fairly fast growing to 20-30'. Foliage red in Fall. Good for wider sidewalk and bench areas.
- (3) **Flowering Plum:** Moderate growth to 25-35'. Pin flowers in the Spring. Maroon foliage. Need water to become established. Fairly wide branching crown. Good against brick, and be sued in narrow sidewalks.
- (4) **Hawthorne:** Moderate growth to 20-30'. White to pink flowers in the Spring, small red berries in the Summer, red and yellow Fall foliage. Spreading crown, very drought tolerant, soft texture, good against wood or brick or to soften stucco and concrete. Can grow in narrow sidewalks.
- (5) **Native conifers:** Selection of these species should be incorporated into frontage areas where none exist at present.

- L. **Sidewalks:** In the Downtown Commercial Area, the sidewalk should cover the area between the curb and the building set back line. The width may vary from a minimum of 4' to a maximum of 15' of width depending on natural features, existing structures, pedestrian needs, and other design factors.

In the entry areas and other areas the width should be from 4' to 8' and should be placed in the area between the curbline and the 20' setback line. The sidewalk may wander or be straight if setbacks limit the wander.

Surface of the sidewalks in all areas should use brick colored pavers in a brick or hexagon pattern contained in a concrete frame. Curb and gutter will be ramped at cross walks for handicap access.

- M. **Street Lighting:** At intersections and when required by traffic safety street lighting should meet the standards of Caltrans or Placer County. In other areas the street lights 12' high should be spaced 50' on center along the curb. The recommended style of the lights are shown below.



- N. **Community Entrance Signs:** Signs must be monument signs constructed of natural materials (no plastic backlit signs will be acceptable), have no bright paint colors included, be low profile (no taller than 8 feet), be limited in size (no greater than 30 sq. ft.), contain no advertising copy, and be incorporated in a landscaped setting with native trees and shrubs. The location of these signs is critical; they may not be closer to any other freestanding sign than 100 feet, and must be limited to highway access points.