

TABLE 2.07.B-1: LAND USE REGULATIONS – FISH HATCHERY SUBDISTRICT		
Timber Stand Improvement	MUP	
Tree Farms	MUP	
Early Successional Stage Vegetation Management	A	
Nonstructural Fish Habitat Management	A	
Nonstructural Wildlife Habitat Management	A	
Structural Fish Habitat Management	MUP	
Structural Wildlife Habitat Management	MUP	
Farm/Ranch Accessory Structures	MUP	
Fire Detection and Suppression	A	
Fuels Treatment	MUP	
Insect and Disease Suppression	A	
Prescribed Fire Management	A	
Sensitive Plant Management	A	
Uncommon Plant Community Management	A	
Erosion Control	A	
SEZ Restoration	A	
Runoff Control	A	

4. **Development Standards.** Table 2.07.B-2 prescribes the development standards for the Fish Hatchery Subdistrict.

TABLE 2.07.B-2: DEVELOPMENT STANDARDS – FISH HATCHERY SUBDISTRICT	
Maximum Density	
<i>Residential</i>	Single Family Dwelling: 1 du/parcel
<i>Recreation</i>	Developed Campgrounds: 8 sites per acre
Building Height	TRPA Code of Ordinances Chapter 37
Minimum Setbacks (measured from property line unless otherwise noted)	See also 17.54.130, 17.54.140, and 17.54.150
<i>Front</i>	50 ft
<i>Side</i>	30 ft
<i>Rear</i>	30 ft
Maximum Community Noise Equivalent Level	55 CNEL

5. **Shorezone.** Within the specified shorezone tolerance district, the following primary uses may be permitted by TRPA in the backshore, nearshore, and fore-shore. Accessory structures shall be regulated pursuant to the regulations applicable to the primary use upon which they are dependent in accordance with the TRPA Code of Ordinances. The following structures may be permitted by TRPA in the shorezone as an Allowed (A) or Special (S) use only if they are accessory to an existing, allowed use located on the same or adjoining littoral parcel.

TABLE 2.07.B-3: SHOREZONE – FISH HATCHERY SUBDISTRICT	
<i>Tolerance District</i>	<i>1</i>
Primary Uses	
Beach Recreation	A
Safety and Navigation Facilities	A
Boat Launching Facilities	S
Water Oriented Outdoor Recreation Concessions	S
Accessory Structures	
Buoys	A
Piers	A
Fences	S
Boat Ramps	A
Floating Docks and Platforms	A
Shoreline Protective Structures	S
Water Intake Lines	A

- C. **Homewood / Tahoe Ski Bowl Master Plan Subdistrict.** The Homewood / Tahoe Ski Bowl Subdistrict is located within the West Shore Subarea. Development within the Homewood / Tahoe Ski Bowl Subdistrict is subject to the adopted Homewood Master Plan (December 14, 2011).

- D. **Lower Truckee Subdistrict.** The Lower Truckee Subdistrict is located within the Greater Tahoe City Subarea. This area should be managed for recreational uses that are compatible with the special scenic and resource values of the Subdistrict. Existing developed facilities that contribute to scenic degradation should be relocated to other suitable areas outside the Subdistrict.
1. ***Special Designations.*** None.
 2. ***Special Policies.***
 - a. Encourage existing commercial uses in this area to relocate to more appropriate areas. Incentives through TDRs to any receivable subdistrict should be provided to encourage commercial uses to relocate. Commercial uses that are allowed to remain for safety reasons should be required to show that there is no safe feasible alternative site and should be required to do visual buffering or landscaping as conditions for any permits for additions, modifications, or alterations.
 - b. Provide suitable parking facilities for recreational users of the river.
 - c. Optimize recreation and travel use of the river corridor to that which maintains its attractiveness and environmental stability.
 - d. Provide opportunities for low to moderate resource management in the plateau area that is located above the Truckee River canyon area and is not visible from Highway 89.
 - e. Public recreation opportunities on Lake Tahoe and the Truckee River should be encouraged. Prior to any expansion, the total number of rafts operating at one time on the Truckee River should be established by a comprehensive environmental analysis. This analysis should include, but not be limited to, determination of overall recreation needs, attractiveness of the facilities, environmental constraints and impacts, parking and traffic constraints, and various water flow limitations. Parking for commercial rafting should be provided by the businesses and in locations that do not further congest the "wye" area.
 - f. Consistent with the Truckee River Corridor Special Planning Area provisions (Section 2.09.B.3), the County shall consider a plan that recognizes existing industrial and commercial uses. The County shall consider rezoning of such industrial and commercial sites, and establishing development standards that focus on environmental redevelopment and / or restoration of those sites.
 3. ***Permissible Uses.*** The following primary uses may be permitted within all or a portion of the Subdistrict. The list indicates if the use is Allowed (A), subject to an Administrative Review Permit (C), or must be considered under the provisions for a Conditional Use Permit (CUP) or Minor Use Permit (MUP). Existing uses not listed shall be considered nonconforming uses within this Subdistrict. The establishment of new uses not listed shall be prohibited.

TABLE 2.07.D-1: LAND USE REGULATIONS – LOWER TRUCKEE SUBDISTRICT		
Allowable Land Uses	Land Use Permit	Add'l Regs.
Residential		
Single-Family Dwelling	A	
Summer Homes	MUP	
Commercial		
Fuel and Ice Dealers	CUP	
Public Service		
Pipelines and Power Transmission	CUP	
Regional Local Public Health and Safety Facilities	MUP	
Public Utility Centers	MUP	
Transportation Routes	CUP	
Transit Stations and Terminals	CUP	
Recreation		
Day Use Areas	A	
Riding and Hiking Trails	A	
Undeveloped Campgrounds	A	
Outdoor Recreation Concessions	MUP	
Rural Sports	CUP	
Visitor Centers	MUP	
Snowmobile Courses	CUP	
Resource Management		
Reforestation	A	
Regeneration Harvest	MUP	
Sanitation Salvage Cut	A	
Selection Cut	A	
Special Cut	A	
Thinning	A	
Tree Farms	MUP	
Timber Stand Improvement	MUP	
Early Successional Stage Vegetation Management	A	
Nonstructural Fish Habitat Management	A	
Nonstructural Wildlife Habitat Management	A	
Structural Fish Habitat Management	A	
Structural Wildlife Habitat Management	A	

TABLE 2.07.D-1: LAND USE REGULATIONS – LOWER TRUCKEE SUBDISTRICT		
Farm/Ranch Accessory Structures	MUP	
Grazing	MUP	
Range Pasture Management	MUP	
Range Improvement	MUP	
Fire Detection and Suppression	A	
Fuels Treatment	A	
Insect and Disease Suppression	A	
Prescribed Fire Management	A	
Sensitive Plant Management	A	
Uncommon Plant Community Management	A	
Erosion Control	A	
SEZ Restoration	A	
Runoff Control	A	

4. **Development Standards.** Table 2.07.D-2 prescribes the development standards for the Lower Truckee Subdistrict.

TABLE 2.07.D-2: DEVELOPMENT STANDARDS – LOWER TRUCKEE SUBDISTRICT	
Maximum Density	
<i>Residential</i>	Single Family Dwelling: 1 du/parcel Summer House: 1 du/parcel or lease site
Building Height	TRPA Code of Ordinances Chapter 37
Minimum Setbacks (measured from property line unless otherwise noted)	See also 17.54.130, 17.54.140, and 17.54.150
<i>Front</i>	50 ft
<i>Side</i>	30 ft
<i>Rear</i>	30 ft
Maximum Community Noise Equivalent Level	50 CNEL
<i>Highway 89 Corridor</i>	55 CNEL

- E. **North Star Subdistrict.** The North Star Subdistrict is located within the North Tahoe West Subarea. This area is best suited for low intensive resource management, although some opportunity for ski trail expansion should be provided.
1. **Special Designation.** None.
 2. **Special Policies.**
 - a. Coordinate with the USFS and operators of the North Star ski facility to assess the feasibility of, and demand for, expanding the North Star operations into this planning area.
 - b. Ski expansion within the Basin should be limited to lifts and runs. Access should be from base facilities outside the Basin.
 - c. Other accessory uses to ski areas, such as warming huts and eating and drinking establishments, should be serviced from outside the Basin. Also, such facilities, if constructed, should be screened from views originating from within the Basin.
 - d. Base facilities for cross country ski and snowmobile courses should be located outside the Basin.
 3. **Permissible Uses.** The following primary uses may be permitted within all or a portion of the Subdistrict. The list indicates if the use is Allowed (A), subject to an Administrative Review Permit (C), or must be considered under the provisions for a Conditional Use Permit (CUP) or Minor Use Permit (MUP). Existing uses not listed shall be considered nonconforming uses within this Subdistrict. The establishment of new uses not listed shall be prohibited.

TABLE 2.07.E-1: LAND USE REGULATIONS – NORTH STAR SUBDISTRICT		
Allowable Land Uses	Land Use Permit	Add'l Regs.
Public Service		
Transmission and Receiving Facilities	CUP	Placer County Code Section 17.56.060.F
Pipelines and Power Transmission Lines	CUP	
Recreation		
Cross Country Skiing Courses	MUP	
Riding and Hiking Trails	A	
Downhill Skiing Facilities	CUP	
Snowmobile Courses	CUP	
Resource Management		
Reforestation	A	
Regeneration Harvest	A	
Sanitation Salvage Cut	A	
Selection Cut	A	
Special Cut	A	

TABLE 2.07.E-1: LAND USE REGULATIONS – NORTH STAR SUBDISTRICT		
Thinning	A	
Timber Stand Improvement	A	
Tree Farms	MUP	
Early Successional Stage Vegetation Management	A	
Nonstructural Fish Habitat Management	A	
Nonstructural Wildlife Habitat Management	A	
Structural Fish Habitat Management	A	
Structural Wildlife Habitat Management	A	
Farm/Ranch Accessory Structures	MUP	
Fire Detection and Suppression	A	
Fuels Treatment	A	
Insect and Disease Suppression	A	
Prescribed Fire Management	A	
Sensitive Plant Management	A	
Uncommon Plant Community Management	A	
Erosion Control	A	
Runoff Control	A	
SEZ Restoration	A	

4. ***Development Standards.*** Table 2.07.E-2 prescribes the development standards for the North Star Subdistrict.

TABLE 2.07.E-2: DEVELOPMENT STANDARDS – NORTH STAR SUBDISTRICT	
Maximum Density	N/A
Building Height	TRPA Code of Ordinances Chapter 37
Additional Recreation Development	Winter Day Use: 1,000 PAOT
Minimum Setbacks (measured from property line unless otherwise noted)	See also 17.54.130, 17.54.140, and 17.54.150
<i>Front</i>	50 ft
<i>Side</i>	30 ft
<i>Rear</i>	30 ft
Maximum Community Noise Equivalent Level	55 CNEL

F. **North Tahoe High School Subdistrict.** The North Tahoe High School Subdistrict is located within the Greater Tahoe City Subarea. This area should continue to provide developed recreational facilities for the local residents.

1. **Special Designations.** (See Section 3.14)
 - a. TDR Receiving Area for:
 - i. Existing Developments
2. **Special Policies.**
 - a. Continue to provide winter recreational opportunities for cross country skiers.
 - b. A secondary access to the high school should be considered.
 - c. Off-road vehicle use should be discouraged. New off-road routes allowing motor vehicle routes are prohibited except for emergency services.
 - d. Consider this subdistrict as a preferred site for relocating recreation now located in stream environment zones or other unsuitable areas.
 - e. This subdistrict should accommodate the connection of the North Tahoe PUD Bike Trail.
 - f. Special Area #1 has been created as a receiving area for public service facilities. The area may only be developed in such a way as to be visibly screened from adjacent neighborhoods and State Route 28 and not exceed established CNEL.
3. **Permissible Uses.** The following primary uses may be permitted within all or a portion of the Subdistrict. The list indicates if the use is Allowed (A), subject to an Administrative Review Permit (C), or must be considered under the provisions for a Conditional Use Permit (CUP) or Minor Use Permit (MUP). Existing uses not listed shall be considered nonconforming uses within this Subdistrict. The establishment of new uses not listed shall be prohibited.

TABLE 2.07.F-1: LAND USE REGULATIONS – NORTH TAHOE HIGH SCHOOL SUBDISTRICT		
Allowable Land Uses	Land Use Permit	Add'l Regs.
Residential		
Single-Family Dwelling	MUP	
Public Service		
Religious Assembly	MUP	
Cultural Facilities	MUP	
Pipelines and Power Transmission	CUP	
Public Utility Centers	MUP	
Transmission and Receiving Facilities	CUP	Placer County Code Section 17.56.060.F
Transportation Routes	CUP	
Cemeteries	MUP	

TABLE 2.07.F-1: LAND USE REGULATIONS – NORTH TAHOE HIGH SCHOOL SUBDISTRICT		
Membership Organizations	MUP	
Schools – Kindergarten through Secondary	MUP	
Transit Stations and Terminals	CUP	
Publicly Owned Assembly and Entertainment	CUP	
Regional Public Health and Safety	MUP	Limited to Special Area #1
Government Offices	MUP	Limited to Special Area #1
Recreation		
Cross Country Skiing Courses	MUP	
Day Use Areas	A	
Riding and Hiking Trails	MUP	
Undeveloped Campgrounds	A	
Participant Sports	MUP	
Developed Campgrounds	A	
Outdoor Recreation Concessions	MUP	
Rural Sports	CUP	
Group Facilities	MUP	
Snowmobile Courses	CUP	
Recreation Centers	CUP	
Resource Management		
Reforestation	A	
Sanitation Salvage Cut	A	
Management Selection Cut	MUP	
Special Cut	MUP	
Thinning	A	
Early Successional Stage Vegetation Management	A	
Nonstructural Fish Habitat Management	A	
Nonstructural Wildlife Habitat Management	A	
Structural Fish Habitat Management	MUP	
Structural Wildlife Habitat Management	MUP	
Fire Detection and Suppression	A	
Fuels Treatment	MUP	
Insect and Disease Suppression	A	

TABLE 2.07.F-1: LAND USE REGULATIONS – NORTH TAHOE HIGH SCHOOL SUBDISTRICT		
Uncommon Plant Community Management	A	
Erosion Control	A	
Runoff Control	A	
SEZ Restoration	A	

4. **Development Standards.** Table 2.07.F-2 prescribes the development standards for the North Tahoe High School Subdistrict.

TABLE 2.07.F-2: DEVELOPMENT STANDARDS – NORTH TAHOE HIGH SCHOOL SUBDISTRICT	
Maximum Density	
<i>Residential</i>	Single Family Dwelling: 1 du/parcel
<i>Recreation</i>	Developed Campgrounds: 8 sites per acre Group Facilities: 25 persons per acre
Building Height	TRPA Code of Ordinances Chapter 37
Minimum Setbacks (measured from property line unless otherwise noted)	See also 17.54.130, 17.54.140, and 17.54.150
<i>Front</i>	50 ft
<i>Side</i>	30 ft
<i>Rear</i>	30 ft
Maximum Community Noise Equivalent Level	55 CNEL

G. **North Tahoe Recreation Area Subdistrict.** The North Tahoe Recreation Area Subdistrict is located within the North Tahoe West Subarea. This Subdistrict should continue to provide recreation opportunities consistent with the needs of the area.

1. **Special Designation.** None.
2. **Special Policies.**
 - a. New or additional recreational development should be consistent with a long-term management plan for the entire Subdistrict.
 - b. Placer County should consider establishing a new access route to the park to enhance safety and minimize motor vehicle impacts in residential areas.
3. **Permissible Uses.** The following primary uses may be permitted within all or a portion of the Subdistrict. The list indicates if the use is Allowed (A), subject to an Administrative Review Permit (C), or must be considered under the provisions for a Conditional Use Permit (CUP) or Minor Use Permit (MUP). Existing uses not listed shall be considered nonconforming uses within this Subdistrict. The establishment of new uses not listed shall be prohibited.

TABLE 2.07.G-1: LAND USE REGULATIONS – NORTH TAHOE RECREATION AREA SUBDISTRICT		
Allowable Land Uses	Land Use Permit	Add'l Regs.
Public Service		
Religious Assembly	MUP	
Cultural Facilities	MUP	
Pipelines and Power Transmission Lines	CUP	
Publicly Owned Assembly and Entertainment Facilities	CUP	
Local Public Safety Facilities	MUP	
Public Utility Centers	MUP	
Transmission and Receiving Facilities	CUP	Placer County Code Section 17.56.060.F
Transportation Routes	CUP	
Government Offices	MUP	
Membership Organizations	MUP	
Schools – Pre- through Secondary	MUP	
Transit Stations and Terminals	CUP	
Recreation		
Cross Country Skiing Courses	CUP	
Day Use Areas	A	
Recreation Center	CUP	
Riding and Hiking Trails	A	

TABLE 2.07.G-1: LAND USE REGULATIONS – NORTH TAHOE RECREATION AREA SUBDISTRICT		
Undeveloped Campgrounds	A	
Participant Sports	A	
Developed Campgrounds	A	
Off-Road Vehicle Course	CUP	
Outdoor Recreation Concessions	A	
Rural Sports	A	
Group Facilities	MUP	
Snowmobile Courses	CUP	
Resource Management		
Reforestation	A	
Sanitation Salvage Cut	A	
Selection Cut	A	
Special Cut	A	
Thinning	A	
Timber Stand Improvement	A	
Tree Farms	MUP	
Early Successional Stage Vegetation Management	A	
Nonstructural Fish Habitat Management	A	
Nonstructural Wildlife Habitat Management	A	
Structural Fish Habitat Management	A	
Structural Wildlife Habitat Management	A	
Fire Detection and Suppression	A	
Fuels Treatment	A	
Insect And Disease Suppression	A	
Prescribed Fire Management	A	
Sensitive Plant Management	A	
Uncommon Plant Community Management	A	
Erosion Control	A	
Runoff Control	A	
SEZ Restoration	A	

4. **Development Standards.** Table 2.07.G-2 prescribes the development standards for the North Tahoe Recreation Area Subdistrict.

TABLE 2.07.G-2: DEVELOPMENT STANDARDS – NORTH TAHOE RECREATION AREA SUBDISTRICT	
Maximum Density	
<i>Recreation</i>	Developed Campgrounds: 8 sites per acre Group Facilities: 25 persons per acre
Building Height	TRPA Code of Ordinances Chapter 37
Additional Developed Outdoor Recreation	Overnight Uses: 200 persons at one time
Minimum Setbacks (measured from property line unless otherwise noted)	See also 17.54.130, 17.54.140, and 17.54.150
<i>Front</i>	50 ft
<i>Side</i>	30 ft
<i>Rear</i>	30 ft
Maximum Community Noise Equivalent Level	55 CNEL

H. **Snow Creek Subdistrict.** The Snow Creek Subdistrict is located within the North Tahoe West Subarea. The planning of this Subdistrict should focus on providing outdoor recreation opportunities.

1. **Special Designation.** None.
2. **Special Policies.**
 - a. Owners of legal lots and parcels of record are encouraged to utilize TRPA development transfer incentives to transfer their development rights out of this subdistrict.
3. **Permissible Uses.** The following primary uses may be permitted within all or a portion of the Subdistrict. The list indicates if the use is Allowed (A), subject to an Administrative Review Permit (C), or must be considered under the provisions for a Conditional Use Permit (CUP) or Minor Use Permit (MUP). Existing uses not listed shall be considered nonconforming uses within this Subdistrict. The establishment of new uses not listed shall be prohibited.

TABLE 2.07.H-1: LAND USE REGULATIONS – SNOW CREEK SUBDISTRICT		
Allowable Land Uses	Land Use Permit	Add'l Regs.
Residential		
Single-Family Dwelling	CUP	
Public Service		
Pipelines and Power Transmissions	CUP	
Transmission and Receiving Facilities	CUP	Placer County Code Section 17.56.060.F
Local Public Health and Safety Facilities	MUP	
Public Utility Centers	MUP	
Transportation Routes	CUP	
Government Offices	MUP	
Transit Stations and Terminals	CUP	
Recreation		
Cross Country Skiing Courses	MUP	
Day Use Areas	A	
Riding and Hiking Trails	A	
Undeveloped Campgrounds	MUP	
Participant Sports	A	
Developed Campgrounds	CUP	
Outdoor Recreation Concessions	A	
Recreational Vehicle Park	CUP	
Rural Sports	CUP	

TABLE 2.07.H-1: LAND USE REGULATIONS – SNOW CREEK SUBDISTRICT		
Group Facilities	MUP	
Snowmobile Courses	CUP	
Beach Recreation	A	
Resource Management		
Reforestation	A	
Regeneration Harvest	MUP	
Sanitation Salvage Cut	A	
Selection Cut	MUP	
Special Cut	MUP	
Thinning	A	
Timber Stand Improvement	MUP	
Tree Farms	MUP	
Early Successional Stage Vegetation Management	A	
Nonstructural Fish Habitat Management	A	
Nonstructural Wildlife Habitat Management	A	
Structural Fish Habitat Management	MUP	
Structural Wildlife Habitat Management	MUP	
Farm/Ranch Accessory Structures	MUP	
Grazing	MUP	
Range Pasture Management	MUP	
Range Improvement	MUP	
Fire Detection and Suppression	A	
Fuels Treatment	MUP	
Insect and Disease Suppression	A	
Prescribed Fire Management	A	
Sensitive Plant Management	A	
Uncommon Plant Community Management	A	
Erosion Control	A	
Runoff Control	A	
SEZ Restoration	A	

4. **Development Standards.** Table 2.07.H-2 prescribes the development standards for the Snow Creek Subdistrict.

TABLE 2.07.H-2: DEVELOPMENT STANDARDS – SNOW CREEK SUBDISTRICT	
Maximum Density	
<i>Residential</i>	Single Family Dwelling: 1 du/parcel
<i>Recreation</i>	Developed Campgrounds: 8 sites per acre Group Facilities: 25 persons per acre
Minimum Setbacks (measured from property line unless otherwise noted)	See also 17.54.130, 17.54.140, and 17.54.150
<i>Front</i>	50 ft
<i>Side</i>	30 ft
<i>Rear</i>	30 ft
Maximum Community Noise Equivalent Level	50 CNEL 55 CNEL for the Highway 28 Corridor

5. **Shorezone.** Within the specified shorezone tolerance district, the following primary uses may be permitted by TRPA in the backshore, nearshore, and foreshore. Accessory structures shall be regulated pursuant to the regulations applicable to the primary use upon which they are dependent in accordance with the TRPA Code of Ordinances. The following structures may be permitted by TRPA in the shorezone as an Allowed (A) or Special (S) use only if they are accessory to an existing, allowed use located on the same or adjoining littoral parcel.

TABLE 2.07.H-3: SHOREZONE – SNOW CREEK SUBDISTRICT	
<i>Tolerance District</i>	6
Primary Uses	
Beach Recreation	A
Water Oriented Outdoor Recreation Concessions	S
Safety and Navigation Facilities	A
Accessory Structures	
Buoys	A
Piers (Multiple Use Only)	A
Fences	S
Floating Docks and Platforms	A
Shoreline Protective Structures	S
Water Intake Lines	A

- I. **Tahoe City Golf Course Subdistrict.** The Tahoe City Golf Course Subdistrict is located within the Greater Tahoe City Subarea. This Subdistrict should continue to serve as a recreation/public service area, maintaining the existing character.
1. **Special Designation.** None.
 2. **Special Policies.** The following special policies apply to the Tahoe City Golf Course Subdistrict.
 - a. The Tahoe City Golf Course Subdistrict should continue as a recreational and restoration and public service area.
 3. **Permissible Uses.** The following primary uses may be permitted within all or a portion of the Subdistrict. The list indicates if the use is Allowed (A), subject to an Administrative Review Permit (C), or must be considered under the provisions for a Conditional Use Permit (CUP) or Minor Use Permit (MUP). Existing uses not listed shall be considered nonconforming uses within this Subdistrict. The establishment of new uses not listed shall be considered nonconforming uses.

TABLE 2.07.I-1: LAND USE REGULATIONS – TAHOE CITY GOLF COURSE SUBDISTRICT		
Allowable Land Uses	Land Use Permit	Add'l Regs.
Commercial		
Amusements and Recreation Services	CUP	
Public Service		
Local Public Health and Safety Facilities	CUP	
Public Utility Centers	CUP	
Pipelines and Power Transmission Facilities	CUP	
Transit Stations and Terminals	CUP	
Transportation Routes	CUP	
Transmission and Receiving Facilities	CUP	Placer County Code, Section 17.56.060.F
Recreation		
Day Use Areas	A	
Golf Courses	A	
Participant Sport Facilities	MUP	
Cross Country Skiing Courses	A	
Outdoor Recreation Concessions	A	
Riding and Hiking Trails	A	
Snow Mobile Courses	CUP	
Resource Management		
Reforestation	A	
Sanitation Salvage Cut	A	
Special Cut	A	

TABLE 2.07.I-1: LAND USE REGULATIONS – TAHOE CITY GOLF COURSE SUBDISTRICT		
Selection Cut	MUP	
Thinning	A	
Early Successional Stage Vegetation Management	A	
Nonstructural Fish Habitat Management	A	
Nonstructural Wildlife Habitat Management	A	
Structural Fish Habitat Management	A	
Structural Wildlife Habitat Management	A	
Fire Detection and Suppression	A	
Fuels Treatment/Management	A	
Insect and Disease Suppression	A	
Sensitive Plant Management	A	
Uncommon Plant Community Management	A	
Erosion Control	A	
SEZ Restoration	A	
Runoff Control	A	

4. ***Development Standards.*** Table 2.07I-2 prescribes the development standards for the Tahoe City Golf Course Subdistrict.

TABLE 2.07.I-2: DEVELOPMENT STANDARDS–TAHOE CITY GOLF COURSE SUBDISTRICT	
Minimum Setbacks (measured from property line unless otherwise noted)	See also 17.54.130, 17.54.140, and 17.54.150
<i>Front</i>	50 ft
<i>Side</i>	30 ft
<i>Rear</i>	30 ft
Maximum Community Noise Equivalent Level	55 CNEL

- J. Upper Ward Valley Subdistrict.** The Upper Ward Valley Subdistrict is located partially within the Greater Tahoe City Subarea and partially within the West Shore Subarea. This subdistrict should be managed for a variety of dispersed and developed recreational opportunities consistent with the need to protect natural environmental qualities and to limit increased vehicle miles of travel. The boundaries of this Subdistrict do not coincide with the scale of any future ski area development. Instead, the Subdistrict boundary serves as a planning guide for expansion of a ski area site within the larger Area Plan.
1. **Special Designation.** None.
 2. **Special Policies.**
 - a. Expansion of downhill ski opportunities may include such facilities as warming huts, first aid, food service, and minor lift maintenance facilities, but should prohibit such base facilities as lodges, and parking lots. In-basin access to skiers via Ward Valley Road shall be limited according to the Ski Area Master Plan, which may recommend access to the ski area for local residents of the Alpine Peaks Subdistrict and to individuals arriving via approved mass transportation.
 - b. Paige Meadows camping opportunities and winter trailhead should be expanded.
 - c. Paige Meadows should remain closed to snowmobile use.
 - d. A high priority should be given to the maintenance of Ward Creek as a fishery.
 3. **Permissible Uses.** The following primary uses may be permitted within all or a portion of the Subdistrict. The list indicates if the use is Allowed (A), subject to an Administrative Review Permit (C), or must be considered under the provisions for a Conditional Use Permit (CUP) or Minor Use Permit (MUP). Existing uses not listed shall be considered nonconforming uses within this Subdistrict. The establishment of new uses not listed shall be prohibited.

TABLE 2.07.J-1: LAND USE REGULATIONS – UPPER WARD VALLEY SUBDISTRICT		
Allowable Land Uses	Land Use Permit	Add'l Regs.
Residential		
Single-Family Dwelling	A	
Public Service		
Local Public Health and Safety Facilities	MUP	
Public Utility Centers	MUP	
Transportation Routes	CUP	
Transit Stations and Terminals	CUP	
Pipelines and Power Transmissions	CUP	
Recreation		
Cross Country Skiing Courses	MUP	
Day Use Areas	A	

TABLE 2.07.J-1: LAND USE REGULATIONS – UPPER WARD VALLEY SUBDISTRICT		
Riding and Hiking Trails	A	
Undeveloped Campgrounds	A	
Developed Campgrounds	CUP	
Outdoor Recreation Concessions	MUP	
Downhill Skiing Facilities	CUP	
Snowmobile Courses	CUP	
Resource Management		
Reforestation	A	
Regeneration Harvest	A	
Sanitation Salvage Cut	A	
Selection Cut	A	
Special Cut	A	
Thinning	A	
Timber Stand Improvement	A	
Tree Farms	MUP	
Early Successional Stage Vegetation Management	A	
Nonstructural Fish Habitat Management	A	
Nonstructural Wildlife Habitat Management	A	
Structural Fish Habitat Management	A	
Structural Wildlife Habitat Management	A	
Farm/Ranch Accessory Structures	MUP	
Grazing	MUP	
Range Pasture Management	MUP	
Range Improvement	MUP	
Fire Detection and Suppression	A	
Fuels Treatment	A	
Insect and Disease Suppression	A	
Prescribed Fire Management	A	
Sensitive Plant Management	A	
Uncommon Plant Community Management	A	
Erosion Control	A	
Runoff Control	A	
SEZ Restoration	A	

4. **Development Standards.** Table 2.07.J-2 prescribes the development standards for the Upper Ward Valley Subdistrict.

TABLE 2.07.J-2: DEVELOPMENT STANDARDS – UPPER WARD VALLEY SUBDISTRICT	
Maximum Density	
<i>Residential</i>	Single Family Dwelling: 1 du/parcel
<i>Recreation</i>	Developed Campgrounds: 8 sites per acre
Additional Developed Outdoor Recreation	Winter Day Use: 4,000 PAOT Overnight Uses: 280 PAOT
Minimum Setbacks (measured from property line unless otherwise noted)	See also 17.54.130, 17.54.140, and 17.54.150
<i>Front</i>	50 ft
<i>Side</i>	30 ft
<i>Rear</i>	30 ft
Maximum Community Noise Equivalent Level	50 CNEL

2.08 **Tourist Planned Development**

A. **Granlibakken Subdistrict.** The Granlibakken Subdistrict is located partially within the Greater Tahoe City Subarea and partially within the West Shore Subarea. This area should continue as a multi-use tourist-oriented planned unit development.

1. **Special Designations.** (See Section 3.14)
 - a. TDR Receiving Area for:
 - i. Existing Development
 - ii. Multi-Residential Units
 - b. Multi-Residential Incentive Program Area
2. **Special Policies.**
 - a. Development of this area per the CTRPA approved litigation settlement shall be allowed.
 - b. Consolidation of undeveloped lots should be encouraged.
3. **Permissible Uses.** The following primary uses may be permitted within all or a portion of the Subdistrict. The list indicates if the use is Allowed (A), subject to an Administrative Review Permit (C), or must be considered under the provisions for a Conditional Use Permit (CUP) or Minor Use Permit (MUP). Existing uses not listed shall be considered nonconforming uses within this Subdistrict. The establishment of new uses not listed shall be prohibited.

TABLE 2.08.A-1: LAND USE REGULATIONS – GRANLIBAKKEN SUBDISTRICT		
Allowable Land Uses	Land Use Permit	Add'l Regs.
Residential		
Employee Housing	MUP	
Multiple Family Dwelling	A	
Single-Family Dwelling	MUP	
Tourist Accommodation		
Hotel, Motel and Other Transient Dwelling Units	A	
Timeshare (Hotel/Motel Design)	CUP	
Timeshare (Residential Design)	CUP	
Commercial		
Eating and Drinking Places	CUP	
Amusements and Recreation Services	CUP	
Privately Owned Assembly and Entertainment	CUP	
Secondary Storage	CUP	
Public Service		
Religious Assembly	MUP	
Cultural Facilities	MUP	

TABLE 2.08.A-1: LAND USE REGULATIONS – GRANLIBAKKEN SUBDISTRICT		
Day Care Centers	MUP	
Local Assembly and Entertainment	MUP	
Local Post Office	A	
Local Public Health and Safety Facilities	MUP	
Pipelines and Power Transmission	CUP	
Transmission and Receiving Facilities	CUP	Placer County Code Section 17.56.060.F
Transit Stations and Terminals	CUP	
Transportation Routes	CUP	
Recreation		
Day Use Areas	A	
Participant Sports Facilities	A	
Sport Assembly	MUP	
Cross Country Skiing Courses	MUP	
Group Facilities	MUP	
Outdoor Recreation Concessions	A	
Riding and Hiking Trails	MUP	
Rural Sports	MUP	
Snowmobile Courses	CUP	
Resource Management		
Reforestation	A	
Sanitation Salvage Cut	A	
Thinning	A	
Early Successional Stage Vegetation Management	A	
Nonstructural Fish Habitat Management	A	
Nonstructural Wildlife Habitat Management	A	
Structural Fish Habitat Management	A	
Structural Wildlife Habitat Management	A	
Fire Detection and Suppression	A	
Fuels Treatment	A	
Insect and Disease Suppression	A	
Sensitive Plant Management	A	
Uncommon Plant Community Management	A	

TABLE 2.08.A-1: LAND USE REGULATIONS – GRANLIBAKKEN SUBDISTRICT		
Erosion Control	A	
Runoff Control	A	
SEZ Restoration	A	

4. **Development Standards.** Table 2.08.A-2 prescribes the development standards for the Granlibakken Subdistrict.

TABLE 2.08.A-2: DEVELOPMENT STANDARDS – GRANLIBAKKEN SUBDISTRICT	
Maximum Density	
<i>Residential</i>	Single Family Dwelling: 1 du/parcel Multiple Family Dwelling: 15 units per acre Employee Housing: As per the limitations above
<i>Tourist Accommodation</i>	Hotel, Motel and Other Transient Units: <i>With less than 10% of units with kitchens – 40 units per acre</i> <i>With 10% or more units with kitchens – 15 units per acre</i> Timeshare: As per the limitations set forth in this table
<i>Recreation</i>	Group Facilities: 25 people per acre
Building Height	TRPA Code of Ordinances Chapter 37
Minimum Setbacks (measured from property line unless otherwise noted)	See also 17.54.130, 17.54.140, and 17.54.150
<i>Front</i>	50 ft
<i>Side</i>	30 ft
<i>Rear</i>	30 ft
Maximum Community Noise Equivalent Level	55 CNEL

2.09 Overlay Districts

- A. **Town Center Overlay District.** The purpose of Town Center Overlay District is to promote environmental redevelopment and provide capacity for transfers of development in accordance with the Regional Plan.

Except as provided in the supplemental limitations in this Section 2.09, all property within the Tahoe City, Kings Beach, and North Stateline Town Center Overlay Districts shall be eligible for all programs in the Regional Plan and Code for Town Centers, including but not limited to: eligibility as receiving areas for transfers of development rights and existing development in accordance with Chapter 51, Transfers of Development, of the TRPA Code of Ordinances; land coverage provisions for Centers in accordance with Chapter 30, Land Coverage, of the TRPA Code of Ordinances; and development standards for Town Centers in accordance with Chapter 13, Area Plans, of the TRPA Code of Ordinances.

1. *Core Areas.*

- a. **Building Height.** Maximum building height within Town Center Core areas is four stories and 56 feet, subject to the findings listed below.
- i. Three- or four-story buildings in Town Centers shall meet findings listed in Section 37.7.16 of the TRPA Code of Ordinances; and
 - ii. Four-story buildings in Town Centers located between Lake Tahoe and State Highways 28 or 89 shall maintain 35 percent of the site as open view corridors to Lake Tahoe, or if existing development does not comply, increase the width of open view corridors by 10 percent or more.

2. *Transition Areas.*

- a. **Building Height.** Maximum building height within Town Center Transition areas is three stories and 46 feet, subject to the findings listed below.
- i. Three- or four-story buildings in Town Centers shall meet findings listed in Section 37.7.16 of the TRPA Code of Ordinances.
- b. **Sidewalks.** Prior to or concurrent with development of projects utilizing Town Center standards, planned sidewalks or multi-use trails shall be installed along the project's Highway 89, Highway 28 and Highway 267 street frontages, as applicable, and extending off-site to the existing Town Center sidewalk/trail network. Any projects proposed without sidewalk or multi-use trail connections to Town Center Core areas shall be ineligible for Town Center Overlay District standards.

3. ***Non-Contiguous Project Area.*** Projects within the Town Center Overlay District may utilize a non-contiguous project area with TRPA approval. To utilize a non-contiguous project area, all project components shall be located on developed mixed-use land within the Town Center Overlay District and all applicable development standards shall apply. Projects using a non-contiguous project area shall not increase the density of land coverage in any portions of the project

area that are between SR28 or SR 89 and Lake Tahoe, beyond the limits that would apply to those portions of the project area without the use of a non-contiguous project area. All non-contiguous project areas shall comply with the setbacks within Town Centers.

B. Special Planning Area Overlay Districts

1. ***Tahoe City Western Entry Special Planning Area (TCWE-SPA).*** The purpose of the Tahoe City Western Entry Special Planning Area Overlay District is to promote SEZ restoration along the Truckee River in conjunction with any development utilizing Town Center redevelopment incentives. Property owners shall address the requirements listed below to be eligible for Town Center Overlay District standards.
 - a. **Restoration.** Projects with Truckee River frontage shall remove coverage and restore SEZs along the river frontage extending no less than 30 feet from the high water mark. Existing buildings in this area shall be removed or mitigated with additional restoration in other areas. Verified coverage may be relocated to other areas.
 - b. **Public Access.** Projects with Truckee River frontage shall provide public access and amenities along the river frontage, concurrent with development, extending no less than 30 feet from the high water mark. River-front amenities shall include, but not be limited to, a multi-use public trail extension or relocation connecting with the existing Truckee River Multi-Use Trail. Public trail facilities shall qualify as air quality mitigation in accordance with Section 65.2.4.C, Required Offsets, of the TRPA Code of Ordinances.
2. ***Tahoe City River District Special Planning Area (TCGC-SPA).*** The purpose of the Tahoe City River District Special Planning Area Overlay District is to implement project design requirements that support the conversion of the old SR 89/Fanny Bridge roadway segment from a State Highway to a recreation-oriented County roadway. The design of all projects shall demonstrate compatibility with the long term operational plans for the roadway segment and shall support the evolution of the area into an active, popular location with safety enhancements that encourage primary access by bicycling, walking and transit.
3. ***Tahoe City Golf Course Special Planning Area (TCGC-SPA).*** The purpose of the Tahoe City Golf Course Special Planning Area Overlay District is to promote redevelopment in the Tahoe City Town Center, shared use projects, and accelerated SEZ restoration. Property owners shall address the requirements listed below to be part of a Town Center project site or be eligible for Town Center Overlay District standards.
 - a. **Restoration.** All or part of the special planning area may be included in a Town Center Project Area only if an equal or greater area of disturbed SEZ land is restored prior to or concurrent with development. Fifty percent of the qualifying restoration areas must be within 0.5 mile of the project. The other 50 percent may be located elsewhere in the same hydrologically related area (HRA), as defined by TRPA.

- b. Sidewalks. Continuous public sidewalks or paved multi-use trails between redevelopment sites and existing sidewalks in the Town Center shall be provided.
4. ***Truckee River Corridor Special Planning Area (TRC-SPA).*** The purpose of the Truckee River Corridor Special Planning Area Overlay District is to improve the planning framework for two pockets of industrial and commercial use sites along Highway 89 between Tahoe City and Alpine Meadows. Following adoption of the Area Plan, the County shall develop an updated plan for these sites that recognizes existing industrial and commercial uses and establishes zoning designations and development standards that focus on environmental redevelopment, restoration, and consistency with the Regional Plan Goals and Policies. The plan shall be processed as an Area Plan amendment.
5. ***Kings Beach Entry Special Planning Area (KBE-SPA).*** The purpose of the Kings Beach Entry Special Planning Area Overlay District is to promote integrated development addressing land use, design, circulation, recreation, public services, and natural resources. Coordinated preparation of a Special Plan Area is encouraged and if developed shall be processed as an Area Plan Amendment. At a minimum, property owners shall address the requirements listed below to be eligible for Town Center Overlay District standards.
 - a. Sidewalks. Concurrent with or prior to development, continuous sidewalks or paved multi-use trails will be provided between redevelopment sites and existing sidewalks in Kings Beach.
 - b. Restoration. Projects shall relocate all development on the site from the 100 year floodplain that includes Griff Creek and shall restore the stream environment. To the extent feasible, projects shall extend coverage removal and restoration activities to other SEZ lands.
 - c. Scenic Enhancements. Projects on developed sites shall enhance scenic conditions with relocated or remodeled buildings, landscaping, streetscape improvements, and other site improvements.
6. ***North Stateline Special Planning Area (NS-SPA).*** The purpose of the North Stateline Special Planning Area Overlay District is to maintain the validity of existing plans and development approvals, while encouraging the development of an integrated Town Center plan in coordination with property owners. Coordinated preparation of a Special Plan is encouraged and if developed shall be processed as an Area Plan amendment. At a minimum, property owners shall address the requirements listed below to be eligible for Town Center Overlay District standards. Prior to approval of a Special Plan, provisions in the Regional Plan and this Area Plan that apply to Town Centers shall not be used.
 - a. Town Center Plan. A Special Plan shall be prepared and processed as an Area Plan amendment meeting the requirements for Town Centers in Chapter 13, Area Plans, of the TRPA Code of Ordinances

2.10 Gateway Areas

Gateway areas shown on the official zoning map shall be consistent with the Design Guidelines for Gateways (see Subsection 3.09.F).

Chapter 3 Area-Wide Standards and Guidelines

The standards and guidelines outlined in this Chapter apply to the entire Plan area. The district standards in Chapter 2 supplement these standards for each subdistrict or overlay district.

3.01 Permissible Uses

Permissible Uses are defined in Chapter 21 of the TRPA Code of Ordinances. Permissible uses for each zoning subdistrict are outlined in Chapter 2 of these Area Plan Regulations. Additional provisions for secondary residences are outlined in Subsections A and B below.

- A. **Additional Secondary Residences Allowed.** In addition to secondary residences permitted as an accessory use by Section 21.3.2 of the TRPA Code of Ordinances, secondary residences are also permitted as accessory to a single family residence if the parcel is deed restricted to prohibit the secondary residence to be converted to a tourist use or utilized as a vacation rental, and that is deed restricted for affordability as determined by the Placer County Housing Specialist and in accordance with current California Department of Housing and Community Development requirements. A secondary residence shall be considered a Residential Unit subject to the residential allocation and transfer provisions of the TRPA Code of Ordinances. Consistent with the TRPA four-year Area Plan recertification process, the secondary residential unit program shall be evaluated for efficacy and necessary adjustments.
- B. **Requirements for Secondary Residences**
1. ***Application Contents and Requirements.*** An Administrative Review Permit approval is required for a secondary residence within the Lake Tahoe Basin. The following materials shall be submitted with permit applications for secondary residences:
 - a. **Existing Residence.** On a lot with an existing single-family residence, include floor plans and elevations of the second unit and a representative photograph of the primary residence.
 - b. **Undeveloped Parcels.** Applications for secondary residences on undeveloped parcels shall include elevations and floor plans for both the main and second units.
 2. ***Timing of Permit and Construction.*** A permit for a secondary residence may be issued and the unit constructed either simultaneously with or subsequent to the primary residence to be constructed on the site.
 3. ***Design Standards for Secondary Residences.***
 - a. **Maximum Floor Area.** The maximum floor area allowed for a secondary residence, whether attached to the primary unit or detached, shall be based on the area of the lot as follows, provided that an attached unit shall not increase the floor area of an existing primary residence by more than 30 percent. [NOTE: "Floor area" as used in this section means the living area of a residence, exclusive of any garage or carport, which is measured from the outside surfaces of exterior walls or walls between living areas and a garage.]

TABLE 3.01.A: MAXIMUM SECONDARY RESIDENCE SIZE	
<i>Lot Area of Site</i>	<i>Maximum Secondary Residence Floor Area</i>
2.29 acres or less	840 sq. ft.
2.3 to 4.99 acres	1,000 sq. ft.
5 acres or more	1,200 sq. ft.

- b. Allowable Garage Area. Detached secondary residences may be allowed an attached garage or carport with a maximum size of 576 square feet in addition to maximum permitted living area. Such space must be clearly designed for the storage of an automobile(s).
 - c. Appearance of Secondary Residence. The secondary residence shall be architecturally compatible with the primary residence. For attached units, the appearance of the building shall remain that of a single-family residence.
- 4. **Occupancy.**
 - a. Either the primary or secondary residence on the site shall be occupied at least 10 months per year.
 - b. Short-term rental of a secondary residence or its bedrooms to overnight guests for fewer than 30 consecutive days is prohibited.
- 5. **General Development Requirements.** Construction associated with any secondary residence shall conform to the height, setback, lot coverage, site plan review, fees, charges, and other requirements generally applicable to residential construction within the zoning subdistrict where the subject property is located.
- 6. **Building Code and Floor Area Calculation Requirements.** Detached secondary residences or additions to existing primary residences shall comply with appropriate building code requirements, minimum parcel size requirements, maximum unit floor area limits for the secondary residence, parking standards and building setback standards. Floor area calculations shall be measured from exterior walls consistent with Uniform Building Code standards.
- 7. **Deed Restriction.** Prior to issuance of a building permit for a secondary residence, the owner shall record a deed restriction which addresses the restrictions on such units contained herein. The declaration shall run with the land and be binding upon the applicant and successor property owners.

3.02 Temporary Uses, Structures, and Activities

Temporary uses, structures, and activities are outlined in Chapter 22 of the TRPA Code of Ordinances (also see Placer County Zoning Ordinance Section 17.56).

3.03 Land Coverage

Land coverage limitations are outlined in Chapter 30 of the TRPA Code of Ordinances.

3.04 Density

Density within the Town Center Overlay District is outlined in Chapter 13, Area Plans, of the TRPA Code of Ordinances. Additional density standards for each zoning subdistrict are outlined in Chapter 2 of these Area Plan Regulations.

Density outside Town Centers is outlined in Chapter 31, Density, of the TRPA Code of Ordinances. Additional density standards for each zoning subdistrict are outlined in Chapter 2 of these Area Plan Regulations.

3.05 Basic Services

Requirements for basic services are outlined in Chapter 32 of the TRPA Code of Ordinances.

3.06 Streetscape and Roadway Design Standards

The Tahoe Basin roadway network primarily consists of State Highways and County roadways. Other roadways in the basin are operated by the U.S. Forest Service, California State Parks, and California Tahoe Conservancy. There are also private roadways which are owned and maintained by private parties.

State Highways within the Plan Area include State Route 28, State Route 267, and State Route 89. State Routes 267 and 89 are the major highways providing access in and out of North Lake Tahoe and State Routes 28 and 89 provide circulation around the western and northern portions of Lake Tahoe within Placer County. Caltrans facilities are designed, operated and maintained by Caltrans in accordance with their current transportation corridor concept reports. However, Caltrans encourages complete street design and is a partner with Placer County where sidewalks are required along a highway.

The majority of roadways in the Plan area are collector and local roadways. Collector roadways collect traffic from local streets towards highways, while local roadways provide direct access to adjacent land uses and collector roadways.

Placer County Department of Public Works and Facilities maintains a Countywide Highway Deficiency Manual to plan for ultimate right-of-way and pavement widths, as well as sidewalk and bicycle lane improvements, for specific County maintained roadways. Table 3.06.A of this section provides planned design characteristics for specific streetscape and roadways to guide future development improvements. Typical street cross sections are provided in this section for public and private development projects in the Plan Area. In addition, the figures below should be referred to for specific pedestrian streetscape improvements planned within the Kings Beach and Tahoe City Town Centers.

<i>Road Name</i>	<i>Segment</i>	<i>ROW Width</i>	<i>Paved Width</i>	<i>CGS ²</i>	<i>Traffic Index (TI)</i>
Bear Street	Highway 28 - Rainbow Avenue	50	32	YES	6.5
Bear Street	Rainbow Avenue - Speckled Avenue	50	22	YES	6.5
Beaver Street	Highway 28 - Town Center Limit	50	22	YES (W)	6.5
Beaver Street	Town Center Limit - End	40	22	NO	6.5
Brockway Vista Avenue	Secline Street - Deer Street	40	32	YES	6
Brockway Vista Avenue	Coon Street - Chipmunk Street	40	22	NO	6
Brook Avenue	Bear Street - Coon Street	40	22	YES	7
Brook Avenue	Coon Street - Fox Street	40	22	YES(S)	7
Brook Avenue	Fox Street - End	40	22	NO	7
Carnelian Bay Avenue	Highway 28 - Highway 267	60	32	NO	7
Cedarwood Drive	Village Road - End	60	32	NO	6
Chamonix Road	Courchevel Road - End	60	32	NO	6
Chipmunk Street	Brockway Vista Avenue - Highway 28	50	32	NO	7.5
Chipmunk Street	Highway 28 - Minnow Avenue	50	32	YES	7.5
Chipmunk Street	Minnow Avenue - Salmon Avenue	50	22	YES(W)	7.5
Chipmunk Street	Steelhead Avenue - Speckled Avenue	50	22	YES	7.5
Coon Street	Brockway Vista - Trout Avenue	50	32	YES	6.5
Coon Street	Trout Avenue - Speckled Avenue	50	22	YES	6.5
Courchevel Road	Ward Creek Boulevard - End	60	32	NO	6
Cutthroat Avenue	Wolf Street - Fox Street	40	22	YES(N)	6.5
Cutthroat Avenue	Fox Street - Beaver Street	40	22	NO	6.5
Deer Street	Highway 28 - Rainbow Avenue	50	32	YES(E)	6
Deer Street	Rainbow Avenue - Steelhead Avenue	50	22	YES(W)	6
Deer Street	Steelhead Avenue - Speckled Avenue	50	22	NO	6
Dolly Varden Avenue	Highway 267 - Fox Street	40	22	YES(S)	6
Dolly Varden Avenue	Fox Street - Chipmunk Street	40	22	NO	6
Fabian Way	Highway 28 - Old Mill Road	60	32	NO	6.5
Fairway Drive	Highway 89 - Grove Street	60	32	YES(E)	6.5
Fox Street	Highway 28 - Brook Avenue	50	32	YES	6.5
Fox Street	Brook Avenue - Trout Avenue	50	22	YES(W)	6.5
Fox Street	Trout Avenue - Steelhead Avenue	50	22	YES(E)	6.5
Fox Street	Steelhead Avenue - Speckled Avenue	50	22	NO	6.5
Golden Avenue	Secline Street - Fox Street	40	22	YES(S)	6

TABLE 3.06.A: FUTURE STREETScape AND ROADWAY DESIGN CHARACTERISTICS ¹					
<i>Road Name</i>	<i>Segment</i>	<i>ROW Width</i>	<i>Paved Width</i>	<i>CGS</i> ²	<i>Traffic Index (TI)</i>
Golden Avenue	Fox Street - End	40	22	NO	6
Grand Avenue	Highway 89 - Alpine Avenue	60	32	NO	6
Granlibakken Road	Highway 89 - Town Center Limit	60	32	YES	6.5
Granlibakken Road	Town Center Limit - End	60	32	NO	6.5
Grove Street	Highway 28 - Fairway Drive	60	32	YES(S)	6.5
Gstaad Road	Courchevel Road - End	60	32	NO	6
Heather Lane	Polaris Road - Cedarwood Drive	60	32	NO	6
Highlands Drive	Country Club Drive - End	60	32	NO	6
Innsbruck Road	Courchevel Road - End	60	32	NO	6
Kitzbuehl Road	Courchevel Road - End	60	32	NO	6
Lake Forest Road	Highway 28 - Highway 28	60	32	NO	6.5
Loch Levon Avenue	Deer Street - Fox Street	40	22	YES(S)	6
Loch Levon Avenue	Fox Street - End	40	22	NO	6
McKinney Drive	Highway 89 - Highway 89	60	32	NO	5.5
McKinney Rubicon	Highway 89 - End	60	32	NO	6.5
Merdan Road	Gstaad Road - End	60	32	NO	6
Minnow Avenue	Fox Street - Chipmunk Street	40	22	YES(S)	6
N. National Avenue	Highway 267 - End	60	32	NO	7
National Avenue	Highway 28 - End	60	32	NO	7
Old County Road	Highway 28 - Forest Road	60	32	NO	6
Old Mill Road	Highway 28 - Polaris Road	60	32	NO	6.5
Pineland Drive	Highway 89 - Twin Peaks Road	60	32	NO	6
Polaris Road	End - End	60	32	NO	6
Rainbow Avenue	Secline Street - Bear Street	40	32	YES(S)	6.5
Rainbow Avenue	Bear Street - Fox Street	40	22	YES(S)	6
Rainbow Avenue	Fox Street - End	40	22	NO	6
Regency Way	N. National Avenue - End	60	32	NO	6
Salmon Avenue	Coon Street - Fox Street	40	32	YES	6.5
Salmon Avenue	Fox Street - Chipmunk Street	40	22	YES(S)	6
Secline Street	Brockway Vista Avenue - Highway 28	50	32	YES(W)	6.5
Secline Street	Highway 28 - Rainbow Avenue	50	32	YES(W)	6.5
Secline Street	Rainbow Avenue - Steelhead Avenue	50	22	YES	6.5
Speckled Avenue	Highway 267 - Fox Street	50	40	YES	7
Speckled Avenue	Fox Street - Chipmunk Street	40	22	NO	6.5

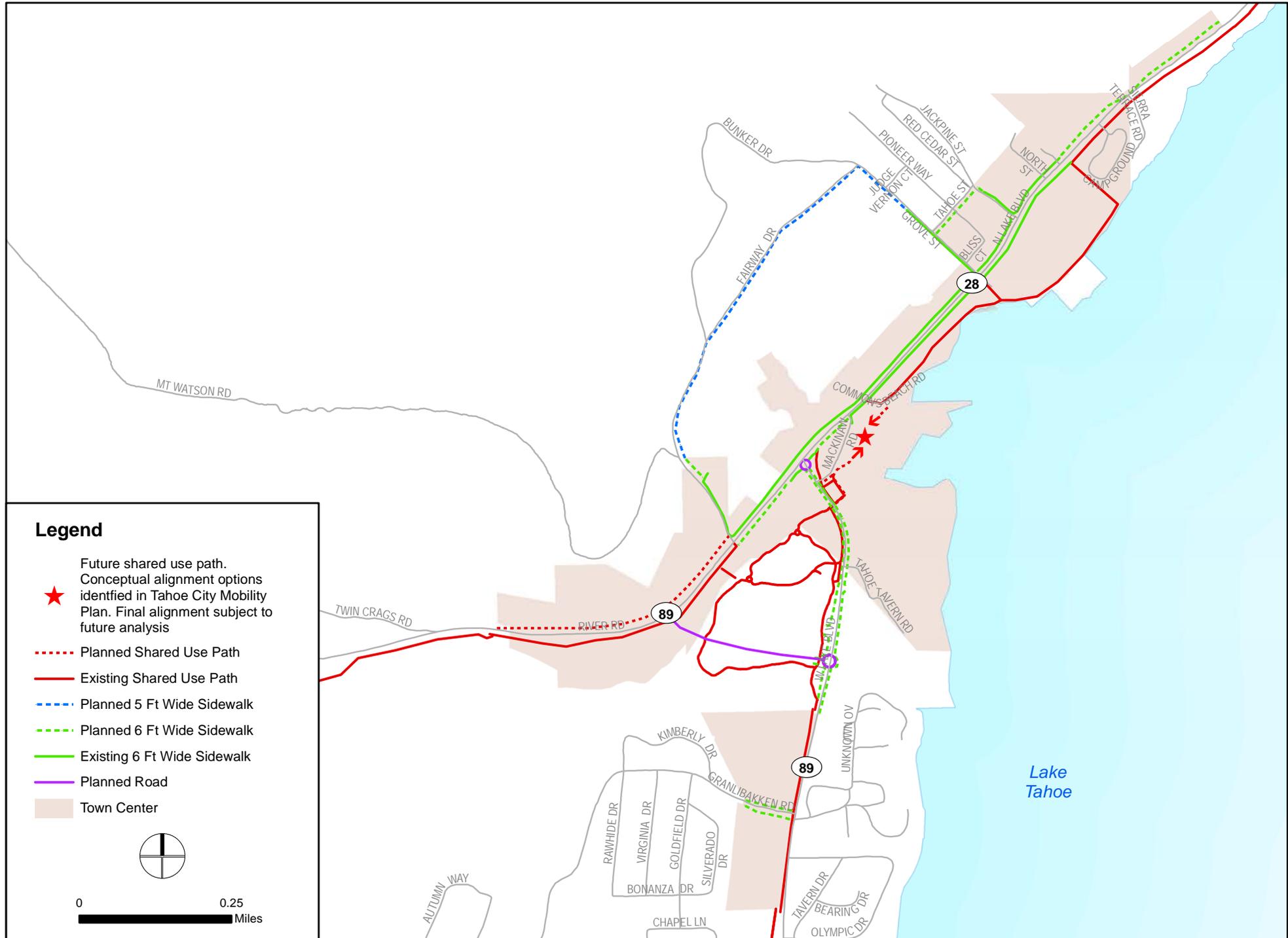
TABLE 3.06.A: FUTURE STREETScape AND ROADWAY DESIGN CHARACTERISTICS ¹

<i>Road Name</i>	<i>Segment</i>	<i>ROW Width</i>	<i>Paved Width</i>	<i>CGS ²</i>	<i>Traffic Index (TI)</i>
Steelhead Avenue	Secline Street - Deer Street	40	32	YES (N)	6.5
Steelhead Avenue	Deer Street - Fox Street	40	22	YES (N)	6
Steelhead Avenue	Fox Street - End	40	22	NO	6
Tahoe Street	Grove Street - Jackpine Street	40	32	YES (S)	6
Trout Avenue	Deer Street - Bear Street	40	32	YES	6.5
Trout Avenue	Bear Street - Fox Street	40	22	YES (S)	6
Trout Avenue	Fox Street - End	40	22	NO	6
Twin Peaks Road	Pineland Drive - Ward Creek Boulevard	60	32	NO	6
Ward Creek Boulevard	Twin Peaks Road - Courchevel Road	60	32	NO	6
Wolf Street	Dolly Varden Avenue - Speckled Avenue	50	22	YES	6.5
Zermatt Road	End - End	60	32	NO	6

Notes:

- (1) The current copy of the Highway Deficiency Manual, which is subject to periodic updates, should be reviewed for changes to the above listed roadways and supersedes this table if changes have taken effect.
- (2) CGS refers to curb, gutter, and sidewalk. "Yes" in the table means curb, gutter, and sidewalk is required on both sides of the road. If sidewalk is only planned on one side of the road, it is indicated in parenthesis, i.e., (N) means curb, gutter, and sidewalk is required along the north side of the road only.

Tahoe City Town Center Pedestrian and Shared Use Path Improvements



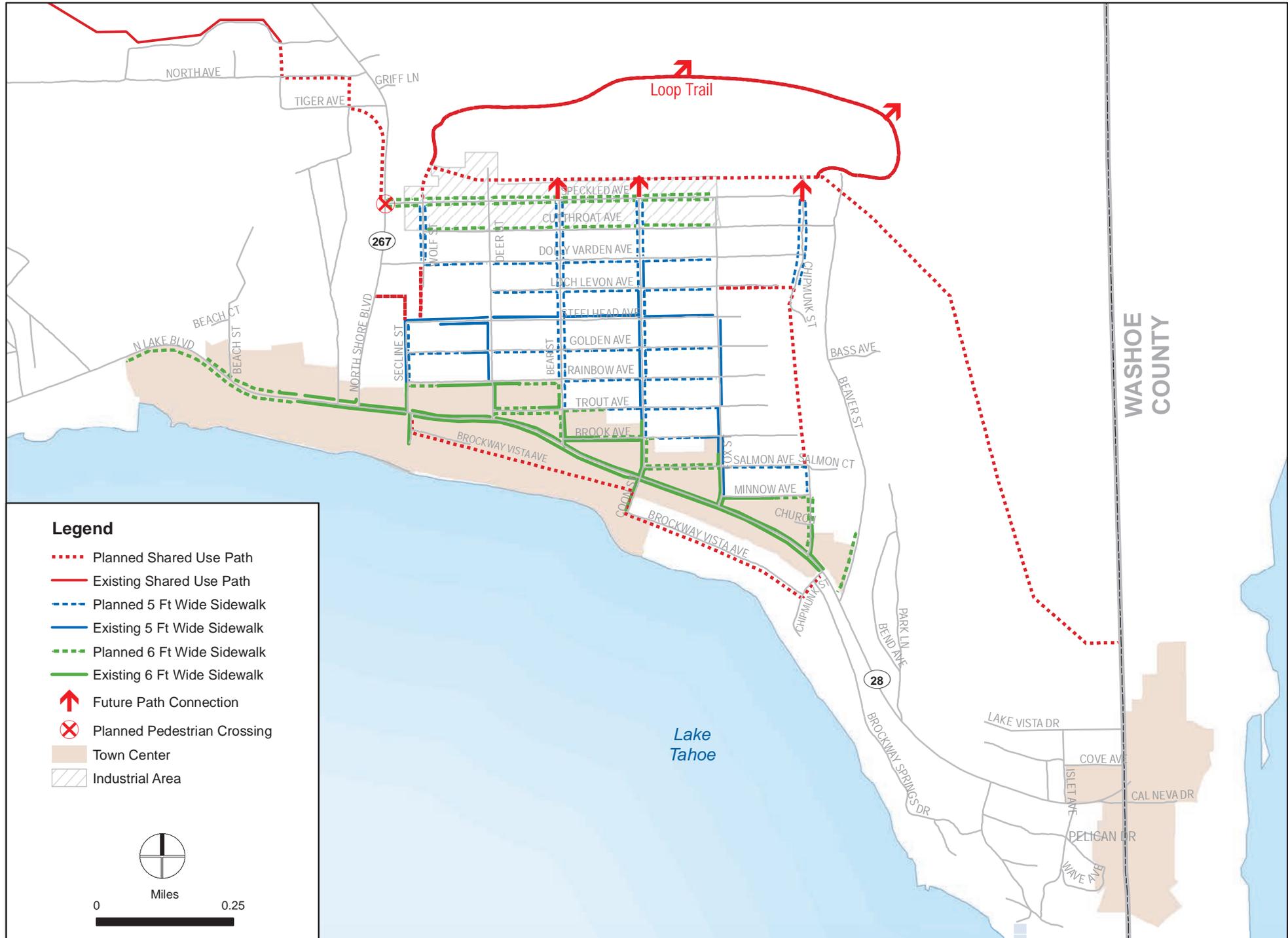
Legend

- 
 Future shared use path.
 Conceptual alignment options
 identified in Tahoe City Mobility
 Plan. Final alignment subject to
 future analysis
- 
 Planned Shared Use Path
- 
 Existing Shared Use Path
- 
 Planned 5 Ft Wide Sidewalk
- 
 Planned 6 Ft Wide Sidewalk
- 
 Existing 6 Ft Wide Sidewalk
- 
 Planned Road
- 
 Town Center



0 0.25
Miles

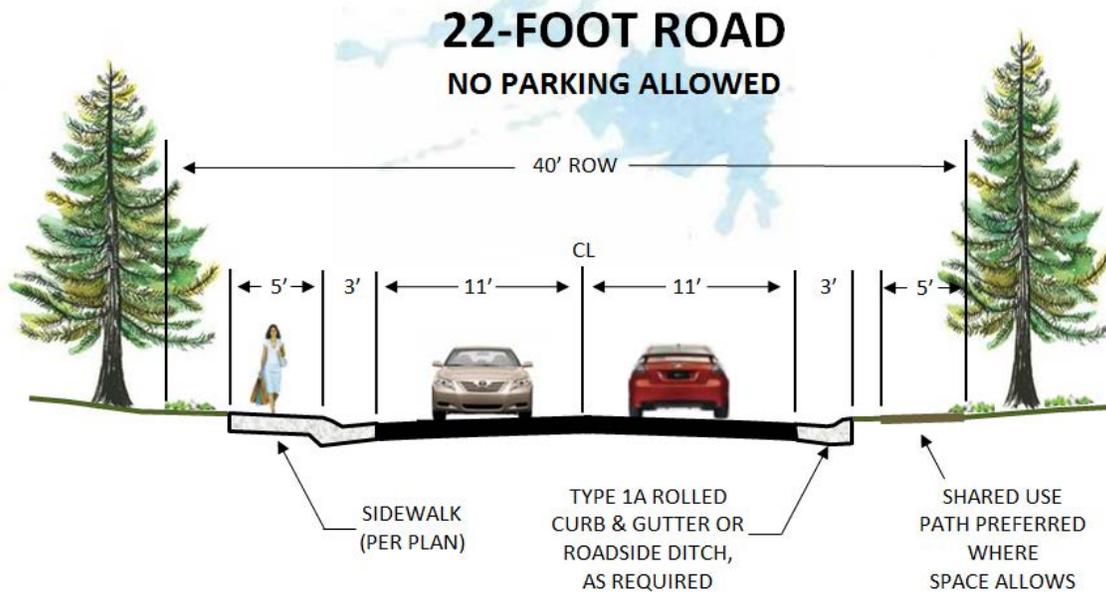
Kings Beach Town Center Pedestrian and Shared Use Path Improvements



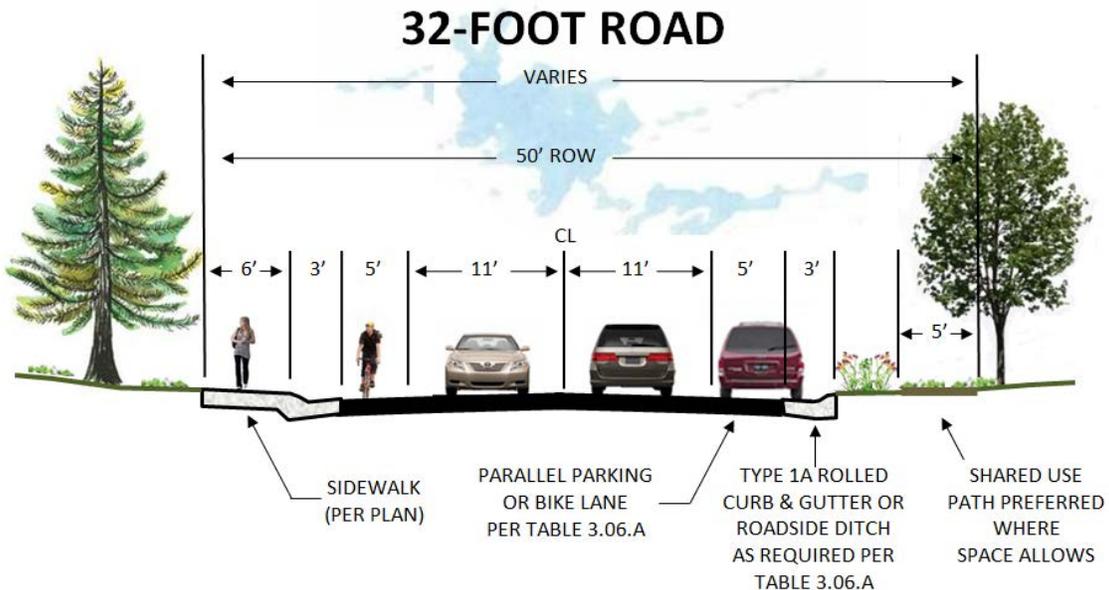
The standards presented in this section supplement the standards presented in previous sections of the Area Plan. If there is a conflict with the earlier sections, the standards of this section shall apply.

Projects subject to the requirements of this section shall be conditioned to meet the following standards where applicable.

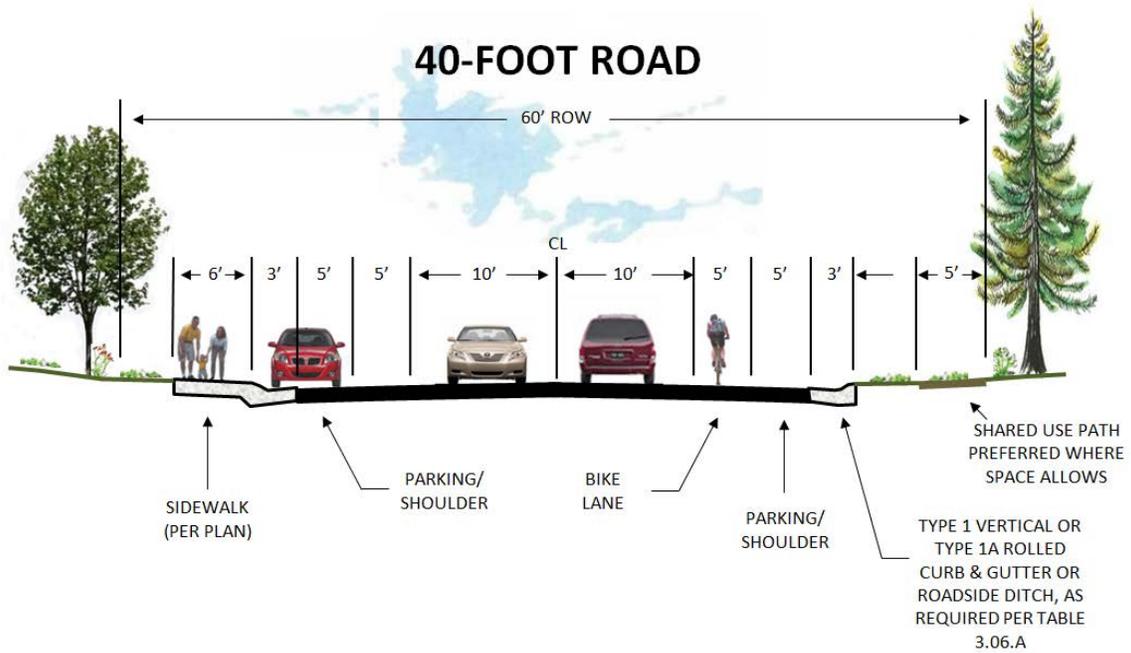
- A. Projects fronting public right-of-ways within the Plan area shall be required to construct frontage improvements as follows:
 - (1) Required ultimate pavement half-section as shown in Table 3.06.A and / or in accordance with the County's Land Development Manual standards and Highway Deficiency Manual, or as otherwise determined by Placer County.
 - (2) Five foot wide or six foot wide concrete sidewalks, as determined by land use. Non-residential uses such as commercial, industrial, recreational, multi-family residential, and uses other than single family residential shall provide six foot wide sidewalks and single-family residential subdivision projects shall provide five foot wide sidewalks, or as otherwise determined by Placer County. Where space allows, detached sidewalks or multi-use paths shall be provided in-lieu of sidewalks attached to the back of curb in order to separate users from the vehicular travel way.
 - (3) Landscaping that also provides stormwater best management practices (such as bioswales) in setback areas.
 - (4) Rolled concrete curb and gutter, or as required by Placer County based on project location. Vertical curb may be specified for straight segments along industrial area roadways. Not all roads will have curb and gutter but may have a different type of road side drainage conveyance such as a stabilized roadside ditch or other County approved drainage feature, as determined by Placer County.
 - (5) To the extent possible, both sidewalk and water quality features (such as a rock-lined ditch where existing or planned as part of a County water quality improvement project) shall be provided along the project frontage where sufficient right-of-way is available.
- B. The following figures and text display the design standards which are to be applied within the Placer County Tahoe Basin Area Plan. Specific standards may be established by the Placer County Community Development Resource Agency - Engineering and Surveying Division, Department of Public Works and Facilities, or approved areawide improvement plans.
 - (1) **22-Foot Paved Roads Typical Cross Section (No parking):** Two 11' wide travel lanes, concrete rolled curb and gutter, 5' or 6' wide sidewalks on one or both sides, lighting and landscaping.



- (2) **32-Foot Paved Roads Typical Cross Section:** Two 11' wide travel lanes, two 5' wide paved bike lanes (or shoulders for parallel parking where connection to bicycle facilities is greater than a half a mile), concrete rolled curb and gutter, 5' or 6' wide sidewalks on one or both sides, lighting and landscaping. If the roadway connects to bicycle facilities and has appropriate car volume and speed, bike sharrows pavement markings should be considered.



- (3) **40-Foot Paved Roads Typical Cross Section:** Two 10' wide travel lanes, two 5' wide paved bike lanes, two 5' wide paved shoulders for parallel parking, concrete rolled curb and gutter, and 5' or 6' wide sidewalks on one or both sides.¹



² Future conditions could include cycle track or Class IV bicycle facility with pavement striping and roadway alterations.

3.07 Parking and Access

This section supersedes Chapter 34, Driveway and Parking Standards, of the TRPA Code of Ordinances.



Off-street parking is required for all projects. Each site is expected to accommodate its customer and employee parking needs on site. In some cases, exceptions may be made and parking credit given for projects that participate in and contribute towards community parking facilities. Parking should be safe and accessible, with a simple layout that is readily understood by the driver. On-site parking should be designed and located so that it does not dominate the development. Parking in excess of that required to adequately serve a project is discouraged in order to avoid unnecessary auto use, extraneous impervious cover, and visual impact.

A. **Parking.** To ensure adequate parking facilities for uses in the region, Placer County and TRPA shall use the following standards and procedures to determine parking requirements:

1. **Parking Facility Defined.** A parking facility is a clearly identifiable location for vehicular parking. A parking facility may be a parking area, parking lot, or parking structure.
2. **Compliance Program.** The provisions set forth in subsections 3 through 10, inclusive, shall apply to projects which involve new or expansion of existing development that creates a demand for parking, including recreation and public service projects. Projects not involving new or expansion of existing development may have the provisions in 3, 4, 5, 6, and 7 required as conditions of approval, if Placer County and TRPA find that the resultant situation would otherwise cause or continue to cause significant adverse impacts on traffic, transportation, air quality, or water quality.
3. **Parking Demand.** Placer County shall adopt and maintain a parking demand table (Table 3.07.A-1: Parking Spaces Required) for the purpose of estimating the minimum and maximum parking demand of uses in the Area Plan. In lieu of the parking demand table, an applicant may submit for TRPA and County approval a technically adequate parking analysis. When parking demand for a use is calculated to a fraction, the fraction shall be rounded up to the nearest whole number if 0.5 or greater, and rounded down if 0.49 or less.

TABLE 3.07.A-1: PARKING SPACES REQUIRED	
Use	Required Number of Parking Spaces
Light Industrial/Wholesale/Storage	
Batch Plant	Determined by Use Permit
Food and Kindred Products	1.1 per 1,000 sq.ft.

TABLE 3.07.A-1: PARKING SPACES REQUIRED	
<i>Use</i>	<i>Required Number of Parking Spaces</i>
Fuel and Ice Dealers	3.33 per 1,000 sq.ft. retail/office area
Industrial Services	1.1 per 1,000 sq.ft.
Printing and Publishing	1.1 per 1,000 sq.ft.
Recycling and Scrap	Determined by Use Permit
Small-Scale Manufacturing	1.1 per 1,000 sq.ft.
Storage Yards	1 per peak employee
Vehicle and Freight Terminals	1 per peak employee
Vehicle Storage and Parking	2 per 1,000 sq.ft. non-storage area and 1 per 1,000 sq.ft. of storage area
Warehousing	.8 per 1,000 sq.ft.
Wholesale Distribution	.8 per 1,000 sq.ft.
Recreation, Education, and Public Assembly Uses	
Beach Recreation	Determined by Use Permit
Boat Launch Facilities	1 per peak employee and .75 car/trailer spot per anticipated daily launch user
Cross Country Skiing Courses	1 per every 3 day users
Day Use Areas	1 per every 3 day users
Developed Campgrounds	1 per peak employee and 1.1 per campsite
Downhill Ski Facilities	1 space per every 3 day users and .5 per peak employee
Golf Course	9.8 per hole
Group Facilities	Determined by Use Permit
Marinas	1 per peak employee and .33 per mooring or slip
Off-Road Vehicle Course	Determined by Use Permit
Outdoor Recreation Concession	Determined by Use Permit
Participant Sports (facilities)	1 per peak employee and 2 per 1,000 sq.ft.
Recreation Center	3.2 per 1,000 sq.ft.
Recreational Vehicle Park	1 per peak employee and 1.1 per campsite
Riding and Hiking Trails	Determined by Use Permit
Rural Sports	1 space per every 3 day users
Snowmobile Courses	1 space per every 3 day users and .5 per peak employee
Sport Assembly	.33 per seat
Undeveloped Campgrounds	None