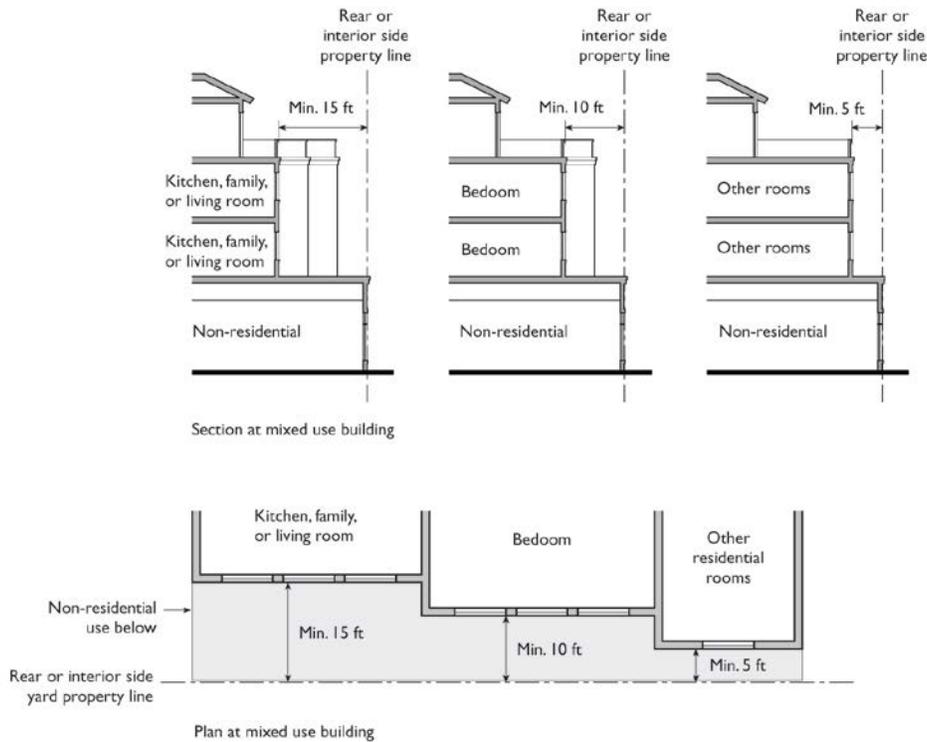


- i. **All Street Frontages.**
 - (1) Building Frontage. The area between the building frontage and back of sidewalk, pedestrian way, bicycle path, or edge of pavement where there is no sidewalk, shall be improved so that it functions as part of the wider sidewalk, improved as outdoor eating or seating areas (subject to Placer County Code Section 17.56.160, Outdoor Retail Sales), or landscaped.
 - (2) Curb. Six-inch vertical concrete curb at sidewalks or rolled curbs with gutter or valley gutter where sidewalks are not planned.
 - (3) Bicycle and Pedestrian Improvements. Bicycle and pedestrian improvements pursuant to the Lake Tahoe Region Bike and Pedestrian Plan or other improvement plan for the area.
 - ii. **Additional Improvements Applicable Along Highway 28 and 89 Frontage.**
 - (1) Street Trees. Street trees shall be planted at least 40 or 50 feet on center depending on species selected or pockets of shrubs planted 25 feet on center or a combination of both trees and shrubs.
 - (2) Pedestrian Street Lights. Twelve-foot-high pedestrian lights at 50 feet on center or low-level lights 25 feet on center.
 - iii. **Additional Improvements Applicable Along Other Streets in MU-TC and MU-N Subdistricts.**
 - (1) Five-foot-wide sidewalk or pedestrian way with landscaping or street trees.
 - iv. **Additional Improvements for Mixed Use Subdistricts within Tahoe City Town Center.**
 - (1) Minimum six foot wide sidewalk or pedestrian way for all projects along Highway 28 and 89.
- b. Required Interior Yards. In order to provide light and air for residential units, the following minimum setbacks apply to any building wall facing an interior side or rear yard. When the site is adjacent to a residential subdistrict, the project must comply with whichever standard results in the greater setback.
- i. **Standards for Interior Yards adjacent to Residential Subdistricts.**
 - (1) Structures shall not interrupt a line of a 1:1 slope extending upward from 25 feet above existing grade of the setback line adjacent to the residential district.

- ii. **Standards for all Interior Yards.** The required setbacks apply to that portion of the building wall containing residential windows and extending three feet on either side of any window.
 - (1) For any wall containing a living room, family room, or kitchen windows, a setback of at least 15 feet shall be provided.
 - (2) For any wall containing sleeping room windows, a setback of at least 10 feet shall be provided.
 - (3) For all other walls containing windows, a setback of at least five feet shall be provided.

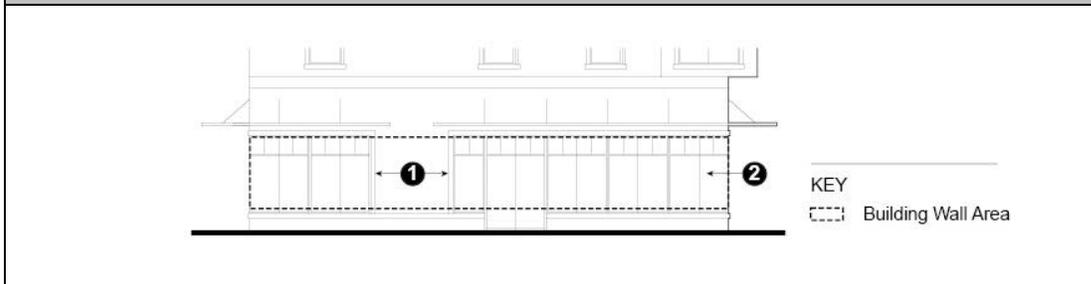
FIGURE 2.04(A)(3): REQUIRED SIDE AND REAR YARDS FOR RESIDENTIAL USES



- c. **Build-to Line for Nonresidential Uses.** Buildings with nonresidential uses on the ground floor fronting Highways 89 and 28 shall be constructed at the required setback for the identified percent of linear street frontage. This requirement may be modified or waived with project approval upon finding that:
 - i. Entry courtyards, plazas, entries, or outdoor eating and display areas are located between the build-to line and building, provided that the buildings are built to the edge of the courtyard, plaza, or dining area;
 - ii. The building incorporates an alternative entrance design that creates a welcoming entry feature facing the street; or

- iii. The building placement is necessary to allow significant views of the lake.
- d. **Corner Build Area.** Where feasible and compatible with environmental constraints, buildings in the MU-TC Subdistrict with nonresidential uses on the ground floor should be located in accordance with the required setbacks within 30 feet of any street corner. Public plazas may be at the street corner provided buildings are built to the edge of the public plaza.

TABLE 2.04.A-4: BUILDING FORM GUIDELINES – GREATER TAHOE CITY MIXED-USE SUBDISTRICTS

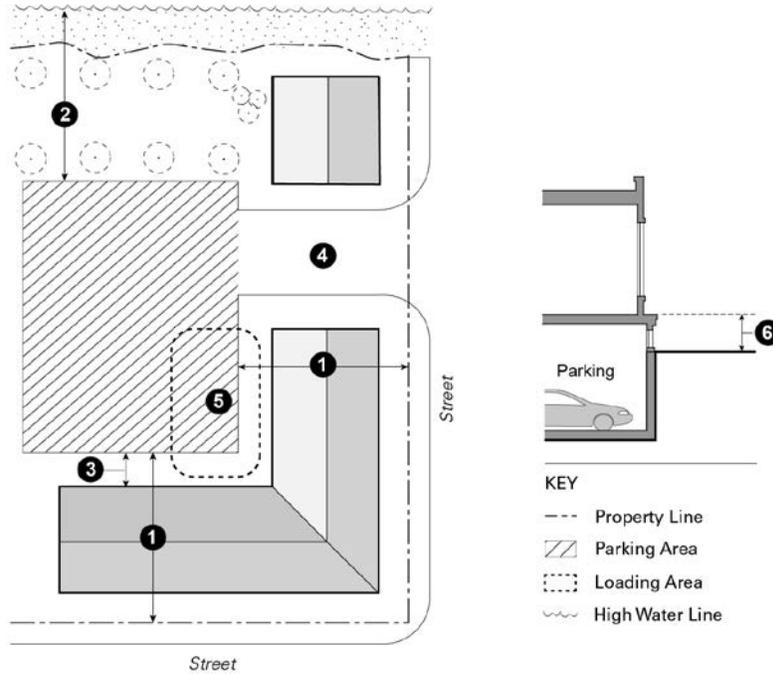


	Town Center Subdistricts					Village Center Subdistricts		#
	MU-TC	MU-N	MU-S	MU-NT	MU-REC	MUN-DH	MUN-LFG	
Maximum Length of Blank Wall	25 ft (e)	40 ft (e)	75 ft (e)	25 ft (e)	40 ft (e)	40 ft (e)	40 ft (e)	1
Required Transparency (% of building wall area)	75% (f)	50% (f)	n/a	75% (f)	40% (f)	n/a	n/a	2
Building Modulation	Any building over 50 feet wide should be broken down to read as a series of buildings no wider than 50 feet each.							

- e. **Limitations on Blank Walls.** Except for side walls built on property lines, no wall should run in a continuous horizontal plane for more than the length specified in Table 2.04.A-4 without windows or door or architectural details of minimum two foot recess or projection.
- f. **Building Transparency; Required Openings for Non-Residential Uses.** Exterior walls facing and within 20 feet of a front or street side property line should include windows, doors, or other openings for at least the percentage stated in Table 2.04.A-4 of the building wall area located between 2.5 and seven feet above ground level. Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, display areas, sales areas, lobbies, or similar active spaces, or into window displays that are at least three feet deep.
 - i. **Exceptions for Parking Garages.** Multi-level garages are not required to meet the building transparency requirement of this subsection. Instead, they must be either located behind buildings

or screened with a landscaped area at least ten feet wide between the parking garage and the public street.

TABLE 2.04.A-5: PARKING AND ACCESS STANDARDS AND GUIDELINES – GREATER TAHOE CITY MIXED-USE SUBDISTRICTS



	<i>Town Center Subdistricts</i>					<i>Village Center Subdistricts</i>		#
	<i>MU-TC</i>	<i>MU-N</i>	<i>MU-S</i>	<i>MU-NT</i>	<i>MU-REC</i>	<i>MUN-DH</i>	<i>MUN-LFG</i>	
Minimum Setback from Edge of Traveled Way	20 ft (g)							1
Minimum Setback from Lake or River Shoreline	40 ft or per TRPA Code of Ordinances, whichever is greater							2
Setback from Buildings and Public Plazas	5 ft walkway and/or landscaping.	n/a	5 ft walkway and/or landscaping	n/a	n/a	n/a	3	
Access Location	Side street or rear wherever possible							4
Shared Access	See additional regulations (h)	n/a	See additional regulations (h)	n/a	n/a	n/a		
Curb Cuts	Minimized and in areas least likely to impede pedestrian circulation	No new curb cuts on Highway 89	Minimized and in areas least likely to impede pedestrian circulation					
Loading/Service Areas	Side or rear of lot is preferred. On lots with both street and lake/riverside frontages, side of lot preferred. Loading and service areas							5

TABLE 2.04.A-5: PARKING AND ACCESS STANDARDS AND GUIDELINES – GREATER TAHOE CITY MIXED-USE SUBDISTRICTS		
	should be screened from public right-of-way and from lake/river.	
Parking Podium	Maximum height of a parking podium located within the parking lot setback and visible from the street should be no greater than 3 feet from finished grade.	⑥

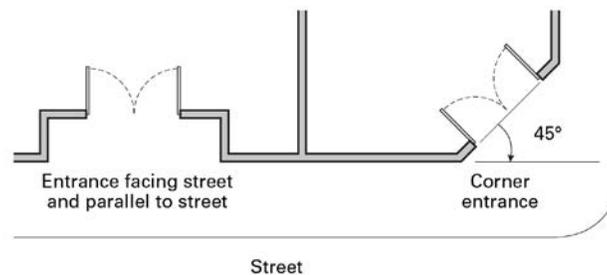
- g. Limitations on Location of Parking. Buildings should be placed as close to the street as possible, with parking underground, behind a building, or on the interior side or rear of the site. Parking may be located within the required setback, subject to the following requirements:
 - i. **Underground and Partially Submerged Parking.** Parking completely or partially underground, may match the setbacks of the main structure.
 - ii. **Surface Parking.** Aboveground surface parking may be located within 20 feet of a street facing edge of traveled way with the approval of a project when the following findings are made:
 - (1) Buildings are built close to the public sidewalk to the maximum extent feasible;
 - (2) The parking area is screened along the public right-of-way with a landscaped wall, hedge, trellis, and / or landscaping;
 - (3) The site is small and constrained such that parking located more than 20 feet from the street frontage is not feasible; and
 - (4) County Public Works determines traffic impacts from vehicle queuing and backing into the right-of-way are mitigated.

- h. Shared Access. To encourage shared parking and shared access points on public streets, new parking facilities should be designed to accommodate cross-access to/from adjacent properties to allow parking areas to become joint use facilities even if initially serving only one development. When cross-access for vehicles is deemed impractical by the Placer County Design Review Committee, the requirement for cross-access may be waived with project approval if bicycle or pedestrian connections are provided between adjacent development.

- i. Building Orientation and Entrances.
 - i. Buildings shall be oriented to face public streets. On lots with frontages along both public streets and Lake Tahoe or the Truckee River, buildings should include a complementary level of design detail on all façades.

- ii. Building frontages should be generally parallel to streets, and the primary building entrances should be located on a public street.
- iii. Building entrances should be emphasized with special architectural and landscape treatments.
- iv. Entrances located at corners shall generally be located at a 45-degree angle to the corner and should have a distinct architectural treatment to animate the intersection and facilitate pedestrian flow around the corner. Different treatments may include angled or rounded corners, arches, and other architectural elements. All building and dwelling units located in the interior of a site should have entrances from the sidewalk that are designed as an extension of the public sidewalk and connect to a public sidewalk.
- v. Entrances to residential units should be physically separated from the entrance to the permitted commercial uses and clearly marked with a physical feature incorporated into the building or an appropriately scaled element applied to the façade.

FIGURE 2.04(A)(4): BUILDING ORIENTATION AND ENTRANCES



- j. **Building Design and Articulation.** Buildings shall provide adequate architectural articulation and detail to avoid a bulky and “box-like” appearance. Building design shall reflect “Old Tahoe” or “Historic Alpine” architectural features, which promote the rustic, alpine character of the area and include the following:
 - i. **Exterior Building Materials and Colors.**
 - (1) A unified palette of quality materials shall be used on all sides of buildings.
 - (2) Natural colors of a mountain setting shall be used to help delineate windows and other architectural features to increase architectural interest.
 - ii. **Building Details.** Buildings shall provide adequate architectural articulation and detail to avoid a bulky and “box-like” appearance and designs shall consider the effects of snow and ice on building access.

- (1) Building façades shall include building projections or recesses, doorway and window trim, shutters, awnings, window boxes, natural stone or wood materials, and other details that provide architectural articulation and design interest.
 - (2) Clip-gambrel roofs, shed roofs, cornices, balconies, covered walkways, and other architectural elements should be used, as appropriate, to terminate rooflines and accentuate setbacks between stories.
 - (3) All applied surface ornamentation or decorative detailing shall be consistent with the architectural style of the building.
 - (4) Each side of the building that is visible from a public right-of-way, shoreline, or publicly accessible open space shall be designed with a complementary level of detailing. Particular attention shall be given to the detailing within the pedestrian's range of touch and view, such as the use of special store-front detailing and façade ornamentation to reinforce the pedestrian character of the street.
 - (5) Building designs shall account for the frequent presence of snow and ice consistent with Section 3.09.D, Design for Snow. Appropriate design strategies may include:
 - (a) Location of entrances under the gable ends of pitched roofs;
 - (b) Limiting the location of entrances, stairs, or walkways under the drip line of roof eaves;
 - (c) Covering of stairs and walkways;
 - (d) Use of snow cleats on roofs to prevent rapid shedding of snow and ice;
 - (e) Limiting the extension of open or uncovered balconies into the roof area;
 - (f) Use of heating elements to reduce snow shedding off of roofs; and
 - (g) Consideration of composition roofs over metal roofing materials.
- k. Pedestrian Connections. A minimum five-foot-wide, unobstructed pedestrian access should be provided from on-site vehicle and bicycle parking area to the main entrance of buildings, to adjacent public amenities such as the beach, shoreline, or viewpoints, and to public trails or transit stops located along the street frontage.
- l. Fencing and Wall Materials. Fences along street-facing property edges on Highways 28 and 89 should be limited and shall be landscaped

screens, wood, or stone with articulation or surface features of minimum two-inch depth at minimum every 50 feet.

- m. Residential Open Space, Town Center Subdistricts. For residential and mixed-use projects with residential uses, 75 square feet of private or common open space area should be provided per dwelling unit. Private areas typically consist of balconies, decks, patios, fenced yards, and other similar areas adjacent to private units. Common areas typically consist of landscaped areas, walks, patios, barbeque areas, or other such improvements as are appropriate to enhance the outdoor environment of the development. Landscaped courtyard entries that are oriented towards a public street are considered common areas.

- i. **Minimum Dimensions.**

- (1) Private Open Space. Private open space should have no horizontal dimension less than six feet.
 - (2) Common Open Space. Common open space should have no horizontal dimension less than 20 feet.

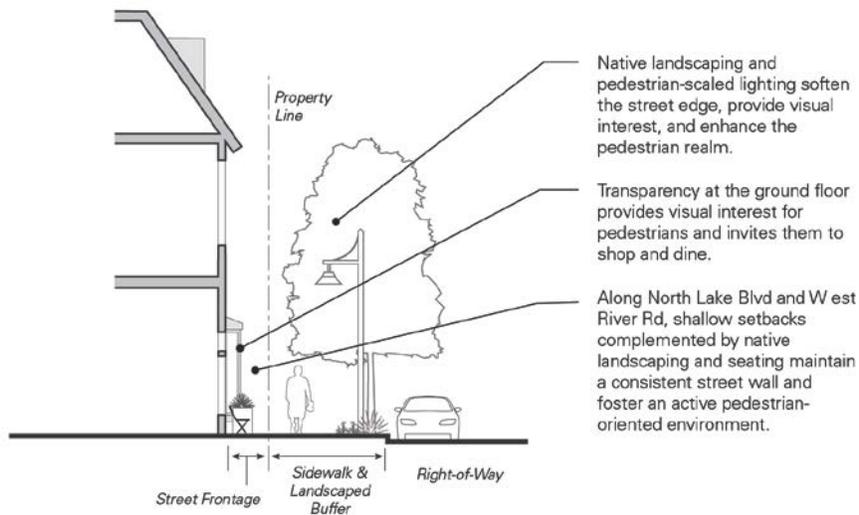
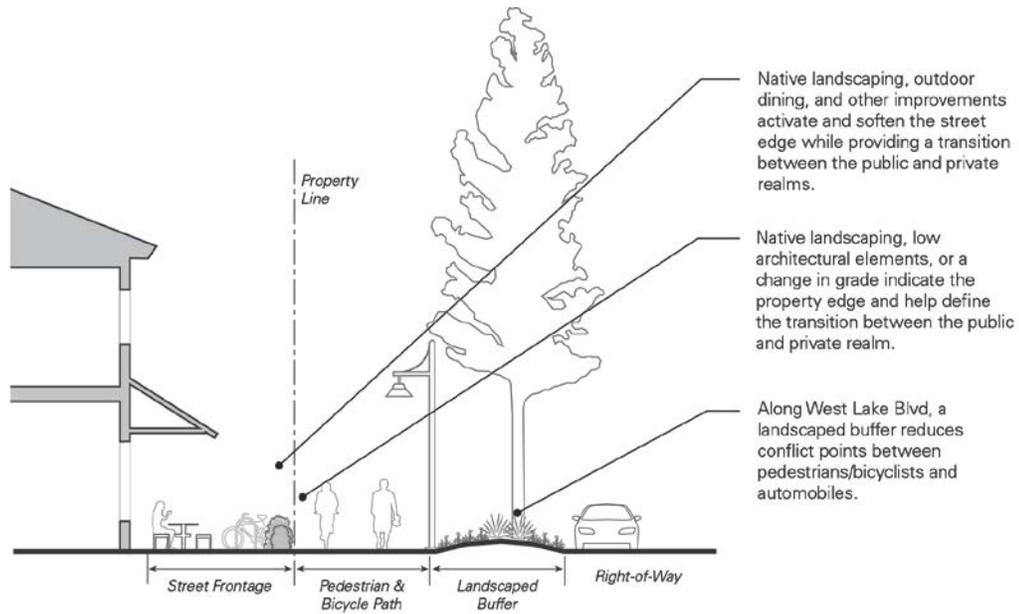
- ii. **Usability.** A surface should be provided that allows convenient use for outdoor living and/or recreation. Such surface may be any practical combination of lawn, garden, flagstone, wood planking, concrete, or other serviceable, dust-free surfacing. Seating areas and plazas should be located in areas with good solar exposure.

- iii. **Accessibility.**

- (1) Private Open Space. The space should be accessible to the living unit by a doorway to a habitable room or hallway.
 - (2) Common Open Space. The space should be accessible to the living units on the lot. It should be served by any stairway or other accessway qualifying as an egress facility from a habitable room.

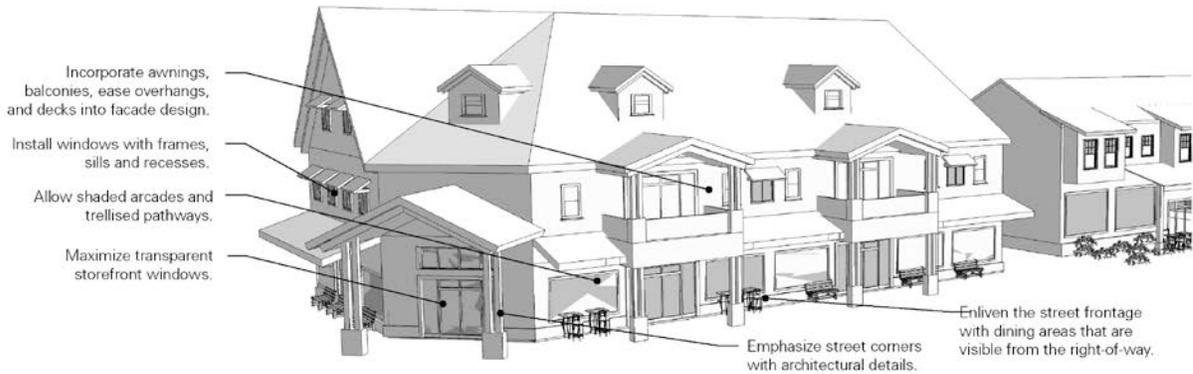
- 5. **Design Guidelines.**

- a. Street Frontage Design. Design front setbacks that maintain the mountain character of scenic highways 89 and 28. Incorporate varied landscaping with a combination of trees, shrubs, and groundcover, as well as changes in elevation or landscaped edges. Avoid heavily manicured or uniform landscape palettes.



b. Building Form and Design.

- i. **Building Articulation and Visual Interest at the Ground Level.** Vary building planes along the street frontage. Incorporate ground-level building details to create a pedestrian-oriented environment with a rural transitional character. Illustrations demonstrate preferred architectural detailing and articulation regardless of building height, size, and massing.



- ii. **Materials.** Employ a variety of building finish materials in the Old Tahoe or Historic Alpine style. Finishes should be wooden panels, masonry, logs, boards and batten, composite shingle siding, shiplap siding, heavy v-joint siding, and metal or composite roofs.
- iii. **Colors.** Design buildings exteriors in the subdued colors of historic Old Tahoe or Historic Alpine style
- iv. **Roof Design.** Design sloped roofs with a minimum slope of 3:12. Roof forms may include gable, hip, and gambrel roofs, and lowered eave lines with dormer windows on upper levels are encouraged. Changes in roof heights are encouraged to prevent boxy buildings. Where flat roofs are used, provide architectural cornices to enhance the roofline. Authentic roof forms that cover the entire width and depth of buildings are preferred over superficial roof forms, such as mansards, that are affixed to the building.

Pitched roofs help reduce snow load and reflect local architectural styles.

Colors blend in with the environment. Bright colors may be used for accent.

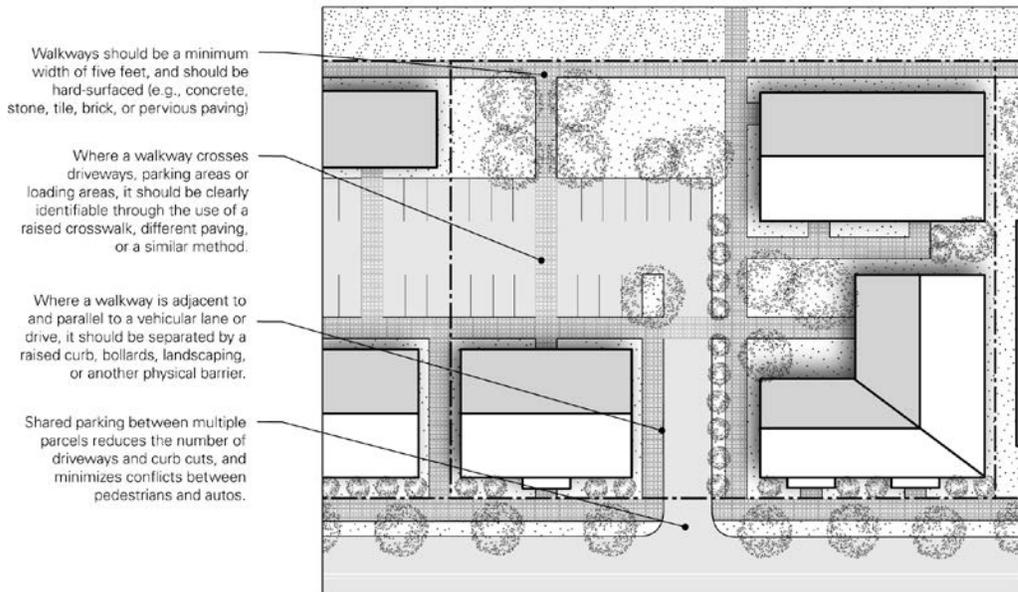
Buildings designed with a variety of materials lend visual interest to the built environment and are consistent with local style.



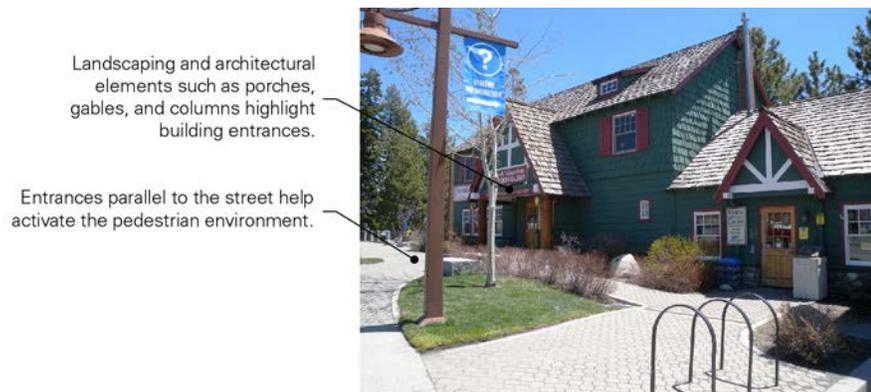
c. Building Access and Entrances.

- i. **Pedestrian Access.** On-site pedestrian circulation and access should provide a system of pedestrian walkways that safely connect all buildings to on-site automobile and bicycle park-

ing; to the public sidewalk and/or bicycle/pedestrian trail; and to any on-site open space areas or amenities, including transit stops. Where possible, walkways should provide direct and convenient access from commercial and mixed-use projects to adjoining residential and commercial areas.



- ii. **Entrance Design.** Building entrances should be designed to reflect the Old Tahoe Style and should be highlighted with covered architectural elements, pedestrian-scaled signage, paving, and building transparency. Walkways or stairways leading to the building entrance should be clearly visible from the right-of-way and from adjacent development.



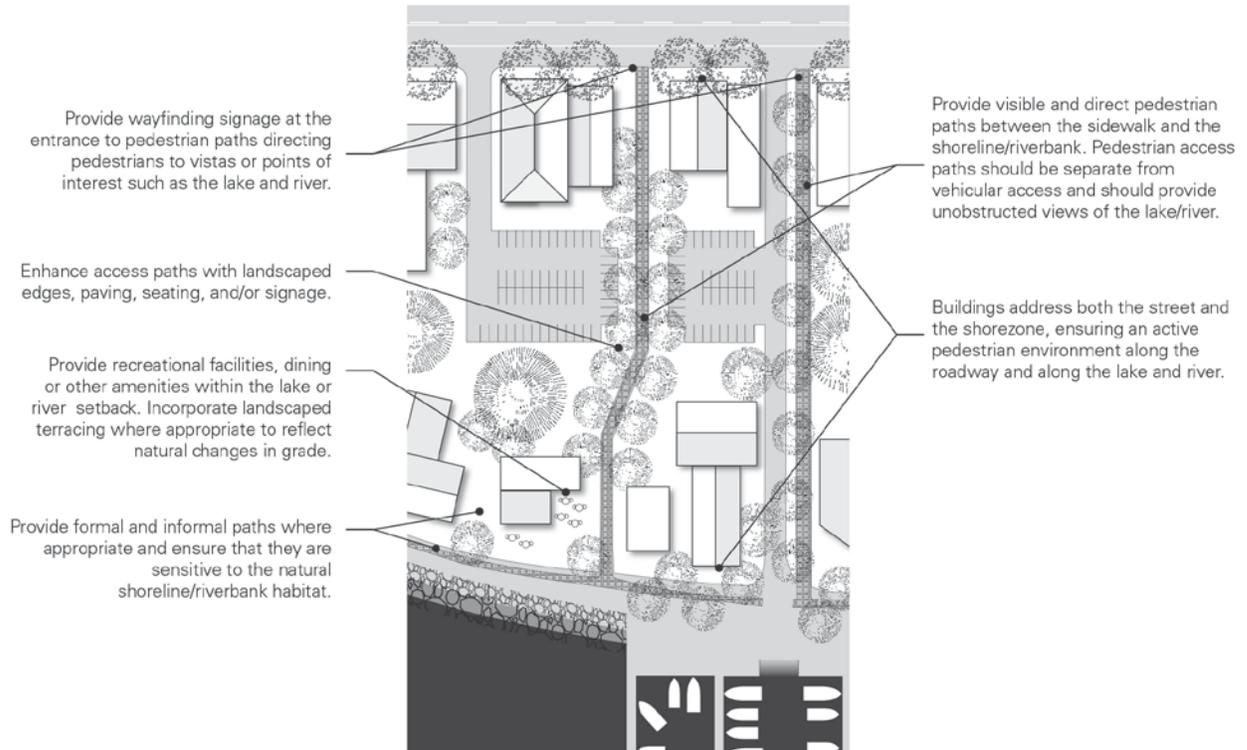
d. Shorezone and Riverzone Design

- i. **View Protection.** Orient pedestrian paths and vehicular circulation to maximize and protect shoreline views from the public rights-of-way. Identify areas that best allow lake views and design buildings and landscaping to maximize visual access.

Where buildings, structures, or parking or loading areas can be seen within a scenic corridor, minimize the impact with vegetation, landforms, or colors and materials that blend with the surroundings.



- ii. **Access.** Provide clearly demarcated public access to the shore and river from public rights-of way. Incorporate landscaped edges, furniture, changes in elevation, and paving, while ensuring that all landscape design is sensitive to the natural habitat.



6. **Shorezone.** Within the specified shorezone tolerance district, the following primary uses may be permitted by TRPA in the backshore, nearshore, and fore-shore. Accessory structures shall be regulated pursuant to the regulations applicable to the primary use upon which they are dependent in accordance with the TRPA Code of Ordinances. The following structures may be permitted by TRPA in the shorezone as an Allowed (A) or Special (S) use only if they are accessory to an existing, allowed use located on the same or adjoining littoral parcel.

TABLE 2.04.A-6: SHOREZONE-GREATER TAHOE CITY MIXED-USE SUBDISTRICTS			
<i>Tolerance Districts</i>	<i>4</i>	<i>6</i>	<i>7</i>
Maximum Community Noise Equivalent Level (CNEL)			
Shorezone	55	55	55
Lakezone	50	50	50
Primary Uses			
Water Oriented Recreation Concessions	A	A	A
Beach Recreation	A	A	A
Tour Boat Operations	A	A	A
Safety and Navigational Devices	A	A	A
Marinas	S	S	S
Boat Launching Facilities	S	S	S
Construction Equipment Storage	S	S	S
Waterborne Transit	A	A	A
Accessory Structures			
Buoys	A	A	A
Piers	A	A	A
Fences	S	S	S
Boat Ramps	A	A	A
Breakwaters or Jetties	S	S	S
Floating Docks and Platforms	A	A	A
Shoreline Protective Structures	A	A	A
Water Intake Lines	A	A	A

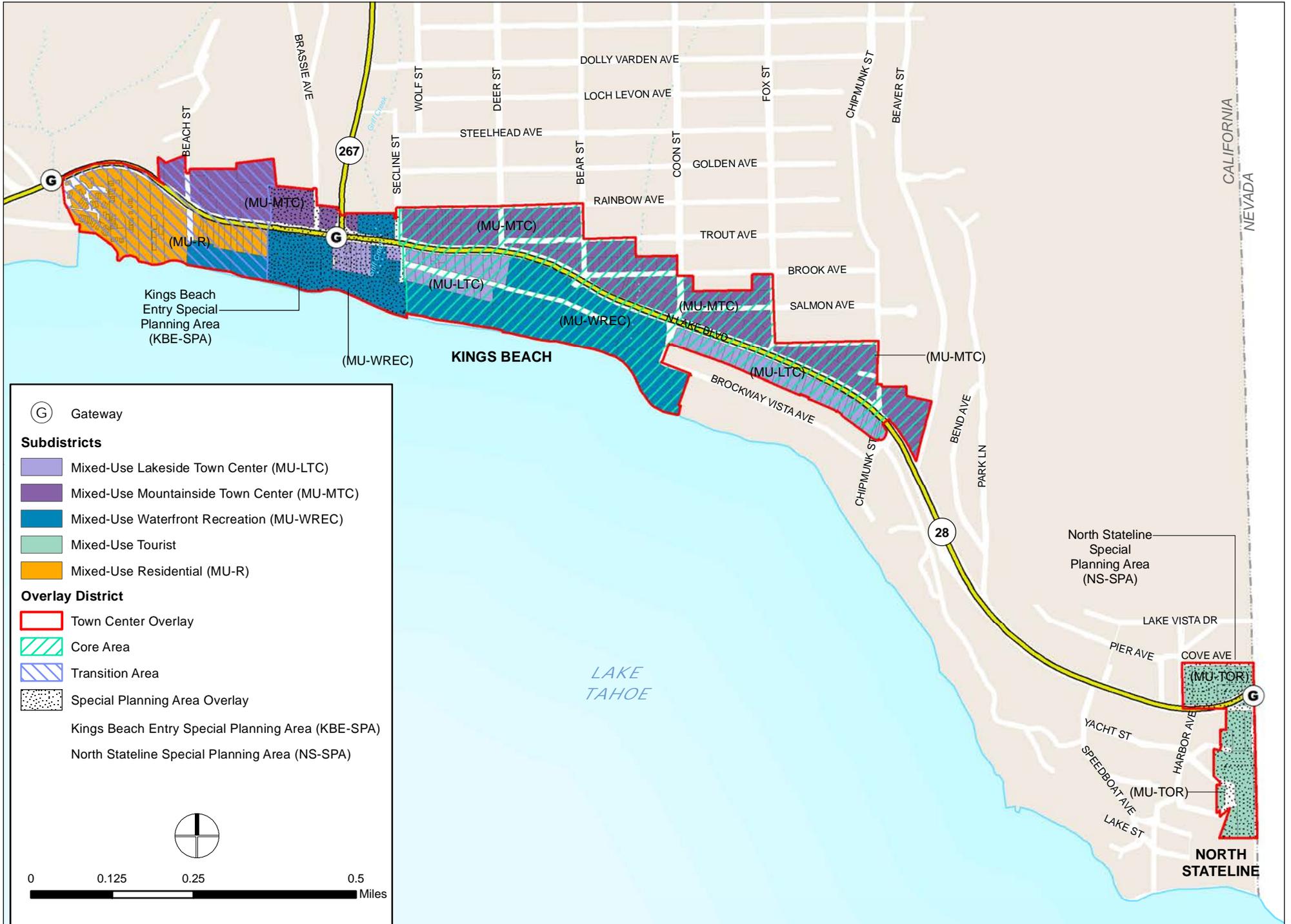
B. North Tahoe East Mixed-Use Subdistricts.

1. **Purpose.** The purposes of the North Tahoe East Mixed-Use Subdistricts are to:
 - a. Provide for the orderly, well-planned, and balanced growth of the King’s Beach and North Stateline area and support the area’s role as a gateway to North Lake Tahoe and a vibrant commercial, cultural, recreational, and tourist center.
 - b. Promote the King’s Beach and North Stateline area as active, pedestrian- and transit-oriented, mixed-use areas attractive to residents and visitors alike.
 - c. Encourage a mix of uses that promotes environmental improvement, year-round economic vitality, and a pleasant quality of life, and improve access to a greater range of recreational opportunities, facilities and services for residents and visitors.
 - d. Establish design standards that improve the pedestrian-orientation, inter-connectivity, and visual quality of development and create a unified, distinctive, and attractive character.
 - e. Foster environmental Threshold attainment.
2. **Planning Statements.** Planning statements for each Mixed-Use Subdistrict are as follows:
 - a. **Mixed-Use Mountainside Town Center (MU-MTC).** This subdistrict is the center of the King’s Beach area located mountainside of North Lake Boulevard. This subdistrict is intended to allow one or more of a variety of residential and non-residential uses with a focus on ground-level active storefronts along North Lake Boulevard. This subdistrict encourages a greater mix and intensity of uses at a scale and form that is appropriate to its town center context and adjacent residential areas.
 - i. **Special Designations.** (See Section 3.14)
 - (1) Town Center
 - ii. **Special Policies.**
 - (1) Development is preferred in and directed toward Town Centers.
 - (2) This subdistrict is appropriate for a variety of land uses with pedestrian and transit facilities.
 - b. **Mixed-Use Lakeside Town Center (MU-LTC).** This subdistrict is the center of the King’s Beach area located lakeside of North Lake Boulevard. It is intended to maintain and enhance the pedestrian- and transit-oriented environment of retail, restaurants, tourist accommodation, and mixed-use development and enhanced access to the lake and recreational activities. This subdistrict allows for vertical mixed-use projects with a focus on ground-level active storefronts along North Lake Boulevard.
 - i. **Special Designations.** (See Section 3.14)

- (1) Town Center
- ii. **Special Policies.**
 - (1) Development is preferred in and directed toward Town Centers.
 - (2) This subdistrict is appropriate for a variety of land uses with pedestrian and transit facilities.
 - (3) Redevelopment projects shall be designed to maintain and enhance views to Lake Tahoe in accordance with Chapter 66, Scenic Resources, of the TRPA Code of Ordinances and the building height findings in Section 2.09.A of these Area Plan Regulations.
- c. Mixed-Use Residential (MU-R). This subdistrict is intended to allow a variety of residential and nonresidential uses and provides a transition from surrounding, lower-density subdistricts to the Town Center. A mix of residential, tourist accommodation, retail, and recreational uses are allowed; however, nonresidential uses are limited west of Beach Street.
 - i. **Special Designations.** *(See Section 3.14)*
 - (1) Town Center
 - ii. **Special Policies.**
 - (1) Development is preferred in and directed toward Town Centers.
 - (2) This subdistrict should be maintained as a residential and tourist area.
- d. Mixed-Use Tourist (MU-TOR). This subdistrict is intended as a destination resort area with multiple uses and activities that complement each other located in a pedestrian-oriented setting. Visitors generally park once and walk between attractions. This subdistrict allows a wide range of entertainment, tourist, recreational, retail, residential, and supporting uses.
 - i. **Special Designations.** *(See Section 3.14)*
 - (1) Town Center
 - ii. **Special Policies.**
 - (1) Development is preferred in and directed toward Town Centers.
 - (2) This subdistrict is appropriate for a variety of land uses with pedestrian and transit facilities.

- e. Mixed-Use Waterfront Recreation (MU-WREC). This subdistrict is intended to provide areas for passive and commercial recreation uses and related services to improve public access and enjoyment of the Lake Tahoe waterfront. Recreational and a mix of supportive retail and service uses are allowed along with environmental enhancement improvements.
 - i. **Special Designations.** (See Section 3.14)
 - (1) Town Center
 - ii. **Special Policies.**
 - (1) This subdistrict should be maintained as a recreation area with limited retail and service uses.
 - (2) Recreation and environmental enhancement projects should continue to be completed in coordination with the California Department of State Parks, the California Tahoe Conservancy and the North Tahoe Public Utility District.
 - (3) The County, the California Department of State Parks, and the North Tahoe Public Utility District shall explore better integration, utilization, and shared use of the Kings Beach State Recreation Area parking lot areas to better serve the community during the off-season and evening hours, to better serve the North Lake Tahoe Event Center, and to showcase it as an integral community facility.

NORTH TAHOE EAST: MIXED USE DISTRICTS



3. **Permissible Uses.** The following primary uses may be permitted within all or a portion of the subdistrict. The list indicates if the use is Allowed (A), subject to an Administrative Review Permit (C), or must be considered under the provisions for a Conditional Use Permit (CUP) or Minor Use Permit (MUP). Numbers in parentheses (#) refer to specific limitations listed at the end of the table. Existing uses not listed shall be considered nonconforming uses. The establishment of new uses not listed shall be prohibited.

TABLE 2.04.B-1: LAND USE REGULATIONS – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS					
	<i>Town Center Subdistricts</i>				
<i>Use</i>	<i>MU-MTC</i>	<i>MU-LTC</i>	<i>MU-R</i>	<i>MU-TOR</i>	<i>MU-WREC</i>
Residential					
Single Family Dwelling	(6)	(6)		(6)	(6)
Multiple Family Dwellings	MUP	MUP	MUP	MUP	
Employee Housing	MUP	MUP	MUP	MUP	MUP
Tourist Accommodation					
Bed and Breakfast Facilities	C	C	C	A	
Hotels, Motels and Other Transient Dwelling Units	CUP	CUP	CUP	CUP	
Timeshare (Hotel/Motel Design)	CUP	CUP	CUP	CUP	
Timeshare (Residential Design)	CUP	CUP	CUP		
Commercial					
Retail Sales	A	A	A (2) (4)	A	MUP
Building Materials and Hardware	MUP	MUP			
Eating and Drinking Places	C	C	C (4)	A	MUP
Food and Beverage Retail Sales	A	A	A (2) (4)	A (2)	MUP (2)
Furniture, Home Furnishings and Equipment	A				
General Merchandise Stores	A	A	A (4)	MUP	MUP
Mail Order and Vending	A (1)	A (1)			
Nursery	C	C			
Outdoor Retail Sales	C	C	C (4)	C	
Service Stations	CUP (10)			CUP	
Amusements and Recreation Services	A	A	A (4)	MUP	
Privately Owned Assembly and Entertainment	A	A	CUP (4)	MUP	CUP
Outdoor Amusements	CUP	CUP	CUP (4)	CUP	
Animal Husbandry Services	CUP (1)	CUP (1)			
Broadcasting Studios	A	A		A	

TABLE 2.04.B-1: LAND USE REGULATIONS – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS					
	<i>Town Center Subdistricts</i>				
<i>Use</i>	<i>MU-MTC</i>	<i>MU-LTC</i>	<i>MU-R</i>	<i>MU-TOR</i>	<i>MU-WREC</i>
Business Support Services	A	A		A	
Financial Services	A	A	A(4)	A	
Health Care Services	A	A		A	
Personal Services	A	A	A(4)	A	
Professional Offices (/Services)	A	A	A(2)(4)	A	
Repair Services	MUP(5)			MUP	
Schools – Business and Vocational	A(1)	A(1)		MUP	
Printing and Publishing	A	A			
Small Scale Manufacturing	MUP(1)				
Vehicle Storage and Parking	MUP	MUP	MUP	MUP	MUP
Public Service					
Religious Assembly	MUP(2)			MUP(2)	
Collection Stations	A	A			
Cultural Facilities	A	A	A(4)	MUP	MUP
Day Care Centers/Pre-Schools	A(1)	A(1)		A	
Government Offices	A	A	A(2)(4)	A(1)	
Hospitals	CUP	CUP			
Local Assembly and Entertainment	A	A	MUP(4)	A(2)	MUP
Local Post Offices	A	A	A(4)	A	
Local Public Health and Safety Facilities	MUP	MUP	MUP(4)	MUP	MUP
Membership Organizations	A(1)	A(1)	MUP(1)(4)	A(1)(2)	
Publicly Owned Assembly And Entertainment	A	A	MUP(4)	A(2)	MUP
Regional Public Health and Safety Facilities	CUP	CUP	CUP(4)	CUP	CUP
Schools – College	CUP	CUP			
Pipelines and Power Transmission	CUP(7)	CUP(7)	CUP(7)(4)	CUP(7)	CUP(7)
Transit Stations and Terminals	CUP(8)	CUP(8)	CUP(8)(4)	CUP(8)	CUP(8)
Transportation Routes	CUP	CUP	CUP(4)	CUP	CUP
Transmission and Receiving Facilities (9)	MUP	MUP	MUP	MUP	MUP

TABLE 2.04.B-1: LAND USE REGULATIONS – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS					
	<i>Town Center Subdistricts</i>				
<i>Use</i>	<i>MU-MTC</i>	<i>MU-LTC</i>	<i>MU-R</i>	<i>MU-TOR</i>	<i>MU-WREC</i>
Recreation					
Day Use Areas	A	A	A (4)	A	A
Recreation Center	A	A	MUP (4)	A (2)	
Participant Sports (Facilities)	MUP	MUP	MUP (4)	MUP	MUP
Sport Assembly	CUP	CUP	CUP (4)	CUP	CUP
Beach Recreation		A	A (4)		A
Boat Launching Facilities		A	A (4)		A
Developed Campgrounds					
Outdoor Recreation Concessions	MUP	MUP	MUP (4)	A	MUP
Marinas		CUP	CUP (3) (4)		CUP
Resource Management					
Reforestation	A	A	A	A	A
Sanitation Salvage Cut	A	A	A	A	A
Thinning			A	A	
Timber Stand Improvement			A		
Tree Farms			A	A	
Early Successional State Vegetation Management			A	A	A
Nonstructural Wildlife Habitat Management			A	A	A
Structural Wildlife Habitat Management			A	A	A
Fire Detection and Suppression	A	A	A	A	A
Fuels Treatment (/Management)	A	A	A	A	A
Insect and Disease Suppression	A	A	A	A	A
Prescribed Fire Management	A	A			A
Sensitive Plant Management	A	A	A	A	A
Uncommon Plant Community Management	A	A	A	A	A
Erosion Control	A	A	A	A	A
Runoff Control	A	A	A	A	A
SEZ Restoration	A	A	A	A	A
Notes:					
(1) Not allowed on the ground floor along North Lake Boulevard frontage.					

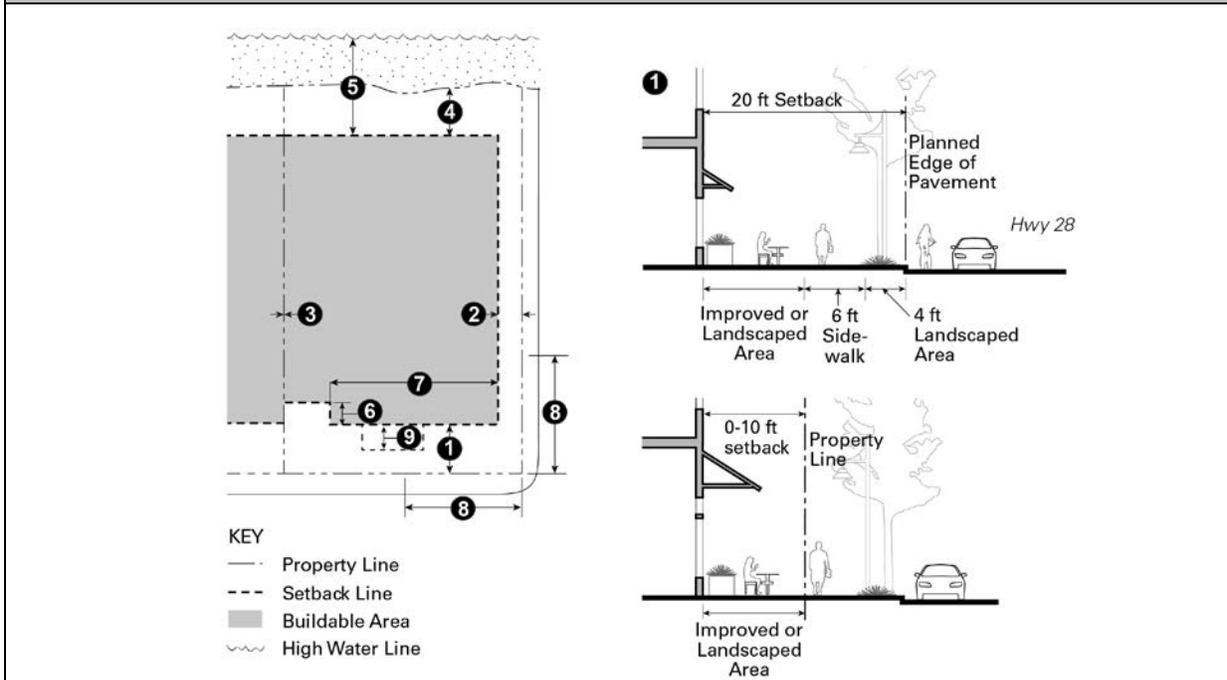
TABLE 2.04.B-1: LAND USE REGULATIONS – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS					
	Town Center Subdistricts				
Use	MU-MTC	MU-LTC	MU-R	MU-TOR	MU-WREC
(2) Limited to establishments with a gross floor area of 5,000 square feet or less. (3) Beachside only. (4) Not allowed west of Beach Street. (5) Not allowed along North Lake Boulevard frontage. Must be completely enclosed inside a building of soundproof construction. (6) New single family and secondary dwellings are not allowed along North Lake Boulevard. (7) A Minor Use Permit is required for aboveground pipeline and transmission lines. (8) A Minor Use Permit is not required for a bus shelter. (9) Facilities which are not visually obtrusive may be eligible for an Administrative Approval per Placer County Code Section 17.56.060.F. (10) New Service Stations are not allowed between SR 28 and Lake Tahoe.					

4. **Development Standards.** Tables 2.04.B-2, 2.04.B-3, 2.04.B-4, and 2.04.B-5 prescribe the development standards for the North Tahoe East Mixed-Use Subdistricts. Individual numbers in parentheses refer to additional regulations that directly follow the tables. The numbers in each illustration below refer to corresponding regulations in the “#” column in the associated table.

TABLE 2.04.B-2: DENSITY, BUILDING HEIGHT, RECREATION AND NOISE STANDARDS – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS					
	Town Center Subdistricts				
	MU-MTC	MU-LTC	MU-R	MU-TOR	MU-WREC
Density within Town Centers is governed by Chapter 13, Area Plans, of the TRPA Code of Ordinances. Chapter 31, Density, of the TRPA Code of Ordinances does not apply within Town Centers.					
Residential					
<i>Single Family Dwelling (unit/parcel)</i>	Permitted if existing, no new				
<i>Multiple Family Dwelling (units/acre)</i>	25	25	25	15	25
<i>Multi-Person Dwelling (people per acre)</i>	25	25	25	25	25
<i>Residential Care (people per acre)</i>	25	25	-	-	-
<i>Employee Housing (units/acre)</i>	25 ¹	25	25	-	25

TABLE 2.04.B-2: DENSITY, BUILDING HEIGHT, RECREATION AND NOISE STANDARDS – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS					
	<i>Town Center Subdistricts</i>				
	<i>MU-MTC</i>	<i>MU-LTC</i>	<i>MU-R</i>	<i>MU-TOR</i>	<i>MU-WREC</i>
Tourist Accommodation					
<i>Bed and Breakfast Facilities (units/acre)</i>	40	40	40	40	-
<i>Hotel, Motel and Other Transient Dwelling Units</i>	40	40	40	40	-
<i>Timeshare (units/acre)</i>	40	40	40	40	-
Building Height	See Section 2.09.A				
<i>Additional Developed Outdoor Recreation</i>	750 Summer Day Use PAOTs in the Kings Beach Town Center. Additional PAOTs may be granted by TRPA in Town Centers				
Maximum Community Noise Equivalent Level (CNEL)	65	65	55	60	55
<i>Highway 28 Corridor</i>	55	55	55	60; where applicable, a max 55 CNEL override is permissible	55
Hourly L_{eq} , dB	For stationary or industrial noise sources or projects affected by stationary or industrial noise sources (as measured at the property line of a noise sensitive receiving use): Daytime (7a.m.-7p.m.): 55 Nighttime (7p.m.-7a.m.): 45				

TABLE 2.04.B-3: BUILDING PLACEMENT STANDARDS AND GUIDELINES – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS

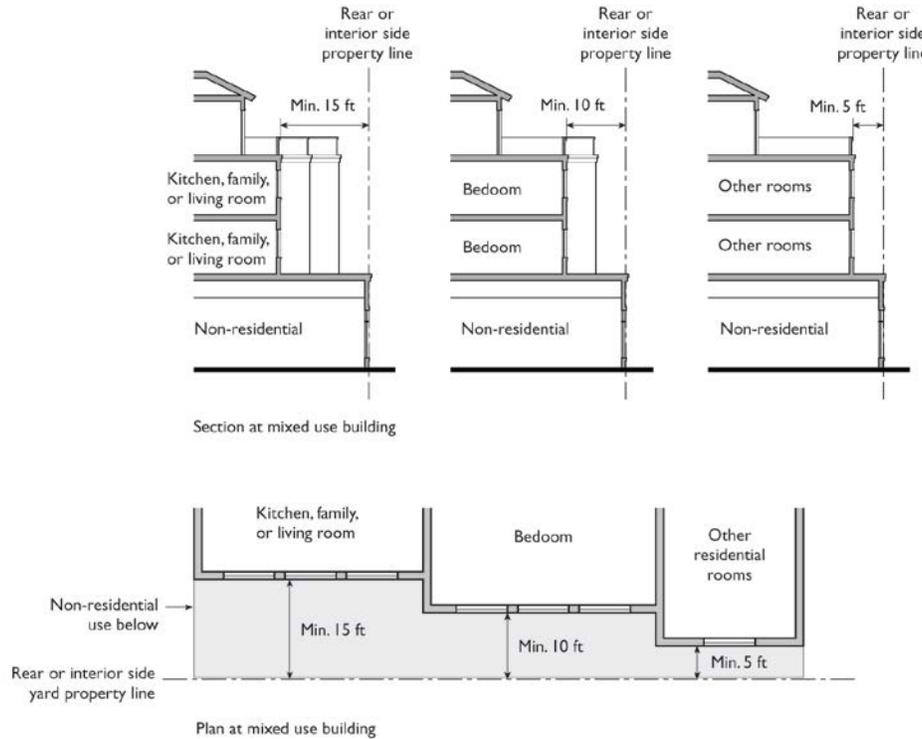


<i>Town Center Subdistricts</i>						
	<i>MU-MTC</i>	<i>MU-LTC</i>	<i>MU-R</i>	<i>MU-TOR</i>	<i>MU-WREC</i>	<i>#</i>
Minimum Setbacks (measured from property line unless otherwise noted)						
<i>Street Frontage, Hwy 28 (from edge of ultimate pavement)</i>	0 ft min, 10' max (a)	0 ft min, 5 ft max (a)	20 ft measured from the planned edge of pavement, with min 6 ft sidewalk and min 4 ft landscaped parkway (a)		10 ft min (a)	1
<i>Street Frontage, Other Streets (from edge of ultimate pavement)</i>	0 ft min, 10 ft max for nonresidential uses (a)					2
<i>Interior Side (from property line)</i>	0 ft; 10 ft foot landscaped setback required adjacent to residential use (b)					3
<i>Rear (from property line)</i>	20 ft (b)					4
<i>Attached Garage</i>	2 ft from primary façade				n/a	6
Minimum Building Frontage at the Build-to Line (% of linear street frontage)	40% (c)	40% (c)	40% (c)	50% (c)	n/a	7
Corner Build Area	30 ft (d)	30 ft (d)				8
Maximum Projection into Front Setback	Awnings and overhangs to provide a covered walkway, public plaza, or outdoor eating area may project up to 50% of the required setback but not into any public right-of-way.					9

- a. **Street Frontage Improvements.** New development, including substantial alterations of existing properties (total floor area of the proposed alternation, not including any internal alteration, is more than 50 percent of the floor area of the original building), shall provide frontage improvements in accordance with the following or an approved area-wide improvement plan:
 - i. **Building Frontage.** The area between the building frontage and back of sidewalk or edge of pavement where there is no sidewalk, shall be improved so that it functions as part of the wider sidewalk, improved as outdoor eating or seating areas (subject to Placer County Code Section 17.56.160, Outdoor Retail Sales), or landscaped.
 - ii. **Street Trees.** If located in sidewalk tree wells, trees shall be planted at minimum 20 feet on center along the Highway 28 street frontage. Otherwise, street trees shall be planted at least 40 or 50 feet on center depending on species selected.
 - iii. **Curb.** Six-inch vertical concrete curb and gutter at sidewalks or rolled curbs with gutter or valley gutter where sidewalks are not planned.
 - iv. **Pedestrian Street Lights.** Twelve-foot-high pedestrian lights at 50 feet on center or low-level lights 25 feet on center.
 - v. **Additional Improvements in Town Centers.** Minimum six-foot-wide sidewalk or pedestrian way with landscaping or street trees for all projects along Highway 28 and Highway 267.
- b. **Required Interior Yards.** In order to provide light and air for residential units, the following minimum setbacks apply to any building wall facing an interior side or rear yard. When the site is adjacent to a residential subdistrict, the project must comply with whichever standard results in the greater setback.
 - i. **Standards for Interior Yards adjacent to Residential Subdistricts.**
 - (1) Structures shall not interrupt a line of a 1:1 slope extending upward from 25 feet above existing grade of the setback line adjacent to the residential district.
 - ii. **Standards for all Interior Yards.** The required setbacks apply to that portion of the building wall containing residential windows and extending three feet on either side of any window.
 - (1) For any wall containing living room, family room, or kitchen windows, a setback of at least 15 feet shall be provided.
 - (2) For any wall containing bedroom windows, a setback of at least 10 feet shall be provided.

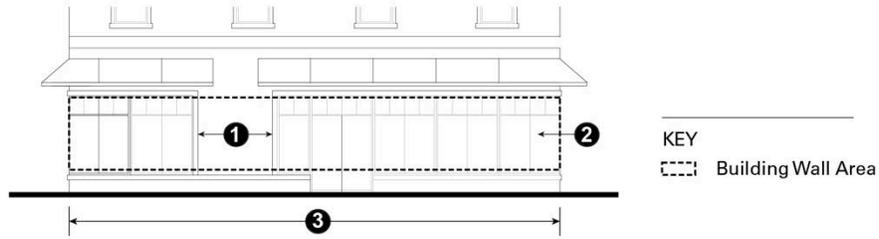
- (3) For all other walls containing windows, a setback of at least five feet shall be provided.

FIGURE 2.04(B)(3): REQUIRED SIDE AND REAR YARDS FOR RESIDENTIAL USES



- c. Build-to Line for Nonresidential Uses. Buildings with nonresidential uses on the ground floor shall be constructed at the required setback for the identified percent of linear street frontage. This requirement may be modified or waived with project approval upon finding that:
- i. Entry courtyards, plazas, entries, or outdoor eating and display areas are located between the build-to line and building, provided that the buildings are built to the edge of the courtyard, plaza, or dining area;
 - ii. The building incorporates an alternative entrance design that creates a welcoming entry feature facing the street; or
 - iii. The building placement is necessary to allow significant views of the lake.
- d. Corner Build Area. Where feasible and compatible with environmental constraints, buildings with nonresidential uses on the ground floor in the MU-MTC Subdistrict should be located in accordance with the required setbacks within 30 feet of any street corner. Public plazas may be at the street corner provided buildings are built to the edge of the public plaza.

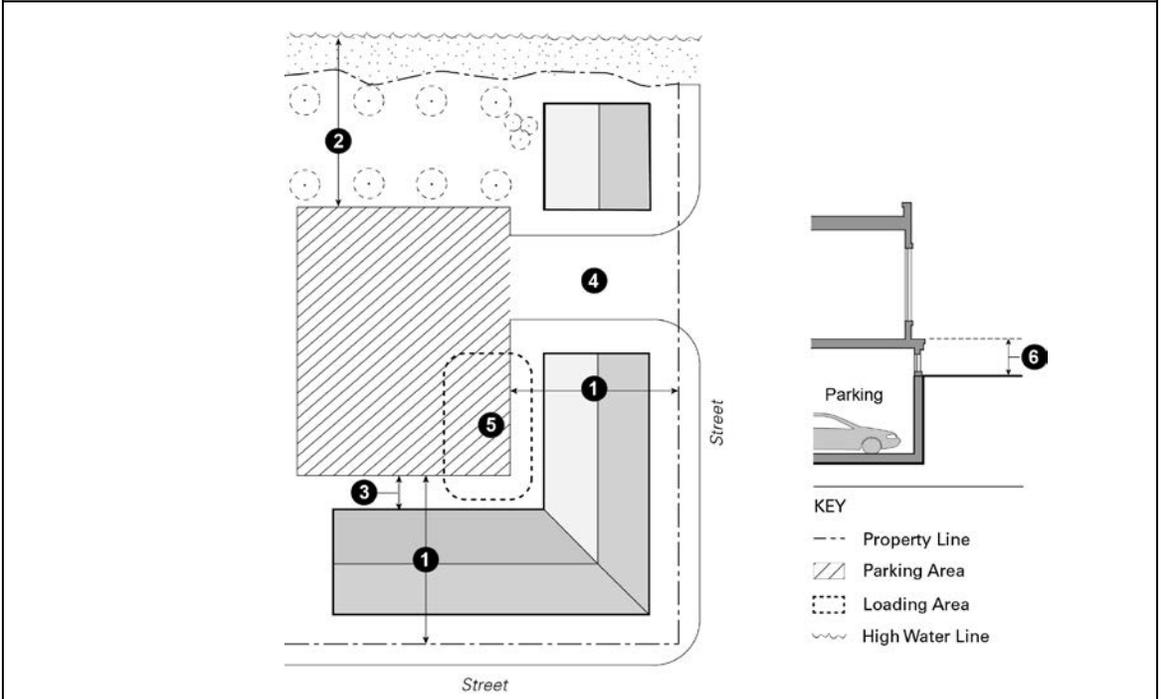
TABLE 2.04.B-4: BUILDING FORM GUIDELINES – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS



	<i>Town Center Subdistricts</i>					
	<i>MU-MTC</i>	<i>MU-LTC</i>	<i>MU-R</i>	<i>MU-TOR</i>	<i>MU-WREC</i>	#
Maximum Length of Blank Wall	Except for side walls built on property lines, no wall should run in a continuous horizontal plane for more than 25 feet without windows or doors or architectural details of minimum 2 foot recess or projection					1
Required Transparency (% of building wall area)	50% (e)	50% (e)	40% (e)	75% (e)	40% (e)	2
Building Modulation	Nonresidential buildings over 75 feet wide should be broken down to read as a series of buildings no wider than 75 feet each					
Maximum Building Length	250 ft	200 ft	200 ft	350 ft	200 ft	3

- e. **Building Transparency; Required Openings for Retail and Service Uses.** Exterior walls facing and within 20 feet of a front or street side property line should include windows, doors, or other openings for the required percentage stated in Table 2.04.B-3.3 of the building wall area located between 2.5 and seven feet above ground level. Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, display areas, sales areas, lobbies, or similar active spaces, or into window displays that are at least three feet deep.
 - i. **Exceptions for Parking Garages.** Multi-level garages are not required to meet the building transparency requirement of this subsection. Instead, they must either be located behind buildings or screened with a landscaped area at least 10 feet wide between the parking garage and public street.

TABLE 2.04B-5: PARKING AND ACCESS STANDARDS AND GUIDELINES – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS

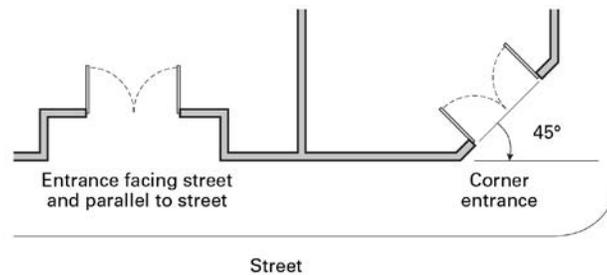


	<i>Town Center Subdistricts</i>					
	<i>MU-MTC</i>	<i>MU-LTC</i>	<i>MU-G</i>	<i>MU-TOR</i>	<i>MU-REC</i>	#
Minimum Setback From Edge of Traveled Way	20 ft (f)					1
Minimum Setback From Lake Tahoe	40 ft, or per TRPA Code of Ordinances, whichever is greater					2
Setbacks from Buildings and Public Plazas	5 ft walkway and/or landscaping					3
Access Location	Side street or rear wherever possible					4
Shared Access	See additional regulations (g)		n/a		(g)	
Curb Cuts	Minimized and in areas least likely to impede pedestrian circulation					
Loading/Service Areas	Side or rear of lot is preferred. Loading and services areas should be screened from public ROW and from lake.					5
Parking Podium	Maximum height of a parking podium visible from the street should be no greater than 3 feet from finished grade.					6

f. Limitations on Location of Parking. Buildings should be placed as close to the street as possible, with parking behind a building or on the interior side or rear of the site. Parking may be located within the required setback, subject to the following requirements.

- i. **Underground and Partially Submerged Parking.** Parking completely or partially underground (with maximum three feet above ground), may match the setbacks of the main structure.
- ii. **Surface Parking.** Aboveground surface parking may be located within 20 feet of a street, but no less than 20 feet from the edge of the traveled way with the approval of a project when the following findings are made:
 - (1) Buildings are built close to the public sidewalk to the maximum extent feasible;
 - (2) The parking area is screened along the public right-of-way with a wall, hedge, trellis, and/or landscaping; and,
 - (3) The site is small and constrained such that parking located more than 20 feet from the street frontage is not feasible.
 - (4) County Public Works determines traffic impacts from vehicle queuing and backing into the right-of-way are mitigated.
- g. **Shared Access.** To encourage shared parking and shared access points on public streets, new parking facilities for Retail Trade or Service Uses should be designed to accommodate cross-access to / from adjacent properties to allow parking areas to become joint use facilities even if initially serving only one development. When cross-access for vehicles is deemed impractical by the Design Review Committee, the requirement for cross-access may be waived with project approval if bicycle or pedestrian connections are provided between adjacent developments.
- h. **Building Orientation and Entrances.**
 - i. Buildings shall be oriented to face public streets.
 - ii. Building frontages should be generally parallel to streets, and the primary building entrances should be located on a public street.
 - iii. Building entrances should be emphasized with special architectural and landscape treatments.
 - iv. Entrances located at corners shall generally be located at a 45-degree angle to the corner and should have a distinct architectural treatment to animate the intersection and facilitate pedestrian flow around the corner. Different treatments may include angled or rounded corners, arches, and other architectural elements. All building and dwelling units located in the interior of a site should have entrances from the sidewalk that are designed as an extension of the public sidewalk and connect to a public sidewalk.
 - v. Entrances to residential units should be physically separated from the entrance to the permitted commercial uses and clearly marked with a physical feature incorporated into the building or an appropriately scaled element applied to the façade.

FIGURE 2.04(B)(4.1): BUILDING ORIENTATION AND ENTRANCES



- i. **Building Design and Articulation.** Buildings shall provide adequate architectural articulation and detail to avoid a bulky and “box-like” appearance. Building design shall reflect “Old Tahoe” and “Historic Alpine” theme and architectural features, which promote the community’s rustic setting and beach character and include the following:
 - i. **Exterior Building Materials and Colors.** Buildings shall be designed in accordance with Section 36.6.1 of the TRPA Code of Ordinances and the following supplemental standards.
 - (1) A unified palette of non-reflective, natural materials such as stone and wood shall be used on all sides of buildings.
 - (2) Natural colors of a mountain setting shall be used to help delineate windows and other architectural features to increase architectural interest.
 - ii. **Building Details.** Buildings shall provide adequate architectural articulation and detail to avoid a bulky and “box-like” appearance and designs shall consider the effects of snow and ice on building access.
 - (1) Building façades shall include building projections or recesses, doorway and window details, shutters, awnings, window boxes, natural stone or wood materials, and other details that provide architectural articulation and design interest.
 - (2) Clip-gambrel roofs, shed roofs, upper-floor balconies, covered walkways, and other architectural elements should be used, as appropriate, to terminate rooflines and accentuate setbacks between stories.
 - (3) All applied surface ornamentation or decorative detailing shall be consistent with the architectural style of the building.
 - (4) Each side of the building that is visible from a public right-of-way, shoreline, or publicly accessible open space shall be designed with a complementary level of

detailing. Particular attention shall be given to the detailing within the pedestrian's range of touch and view, such as the use of special storefront detailing and façade ornamentation to reinforce the pedestrian character of the street.

- (5) Building designs shall account for the frequent presence of snow and ice consistent with Section 3.09.D, Design for Snow. Appropriate design strategies may include:
 - (a) Location of entrances under the gable ends of pitched roofs;
 - (b) Limiting the location of entrances, stairs, or walkways under the drip line of roof eaves;
 - (c) Covering of stairs and walkways;
 - (d) Use of snow cleats on roofs to prevent rapid shedding of snow and ice;
 - (e) Limiting the extension of open or uncovered balconies into the roof area;
 - (f) Use of heating elements to reduce snow shedding off of roofs; or
 - (g) Consideration of composition roofs over metal roofing materials.

j. **Pedestrian Connections.** A minimum five foot wide unobstructed pedestrian access should be provided from on-site vehicle and bicycle parking area to the main entrance of buildings, to adjacent public amenities such as the beach, shoreline, or viewpoints, and to public trails or transit stops located along the street frontage.

k. **Residential Open Space.** For residential and mixed-use projects with residential uses, 75 square feet of private or common open space area should be provided per dwelling unit. Private areas typically consist of balconies, decks, patios, fenced yards, and other similar areas adjacent to private units. Common areas typically consist of landscaped areas, walks, patios, barbeque areas, or other such improvements as are appropriate to enhance the outdoor environment of the development. Landscaped courtyard entries that are oriented towards a public street are considered common areas.

i. **Minimum Dimensions.**

- (1) **Private Open Space:** Private open space should have no horizontal dimension less than six feet.
- (2) **Common Open Space:** Common open space should have no horizontal dimension less than 20 feet.

ii. **Usability.** A surface should be provided that allows convenient use for outdoor living and/or recreation. Such surface may be

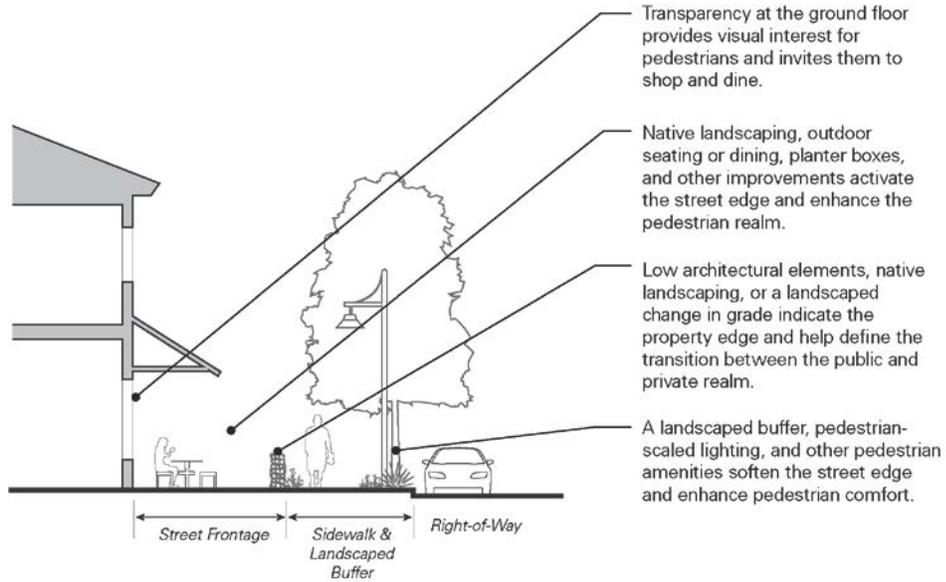
any practical combination of lawn, garden, flagstone, wood planking, concrete, or other serviceable, dust-free surfacing. Seating areas and plazas should be located in areas with good solar exposure.

iii. **Accessibility.**

- (1) Private Open Space: The space should be accessible to the living unit by a doorway to a habitable room or hallway.
- (2) Common Open Space: The space should be accessible to the living units on the lot. It should be served by any stairway or other accessway qualifying as an egress facility from a habitable room.

5. **Design Guidelines.**

- a. **Street Frontage Design.** Design front setbacks that maintain the mountain character of scenic Highway 28 and other neighborhood streets. Incorporate varied landscaping with a combination of trees, shrubs, and groundcover, as well as changes in elevation or landscaped edges. Avoid heavily manicured or uniform landscape palettes.



An assortment of native landscaping is appropriate for all front setbacks. Low walls or bollards should be used to indicate property lines.



Front setbacks should be designed to provide attractive landscaping and architectural features as well as active uses and ground floor transparency.

- b. Building Form and Design
- i. **Building Articulation and Visual Interest at the Ground Level.** Vary building planes along the street frontage. Incorporate ground-level building details to create a pedestrian-oriented environment with a rural transitional character. Illustrations demonstrate preferred architectural detailing and articulation regardless of building height, size, and massing.
 - ii. **Materials.** Employ a variety of building finish materials in the Old Tahoe or Historic Alpine style. Finishes should be wooden panels, masonry, logs, boards and batten, composite shingle siding, shiplap siding, heavy V-joint siding, and metal or composite roofs.
 - iii. **Colors.** Design buildings exteriors in the subdued colors of historic Old Tahoe or Historic Alpine style.
 - iv. **Roof Design.** Design sloped roofs with a minimum slope of 3:12. Roof forms may include gable, hip, and gambrel roofs, and lowered eave lines with dormer windows on upper levels are encouraged. Changes in roof heights are encouraged to prevent boxy buildings. Where flat roofs are used, provide architectural cornices to enhance the roofline. Authentic roof forms that cover the entire width and depth of buildings are preferred over superficial roof forms, such as mansards, that are affixed to the building.

Pitched roofs help reduce snow load and reflect local architectural styles.

Colors blend in with the environment. Bright colors may be used for accent.

Buildings designed with a variety of materials lend visual interest to the built environment and are consistent with local style.



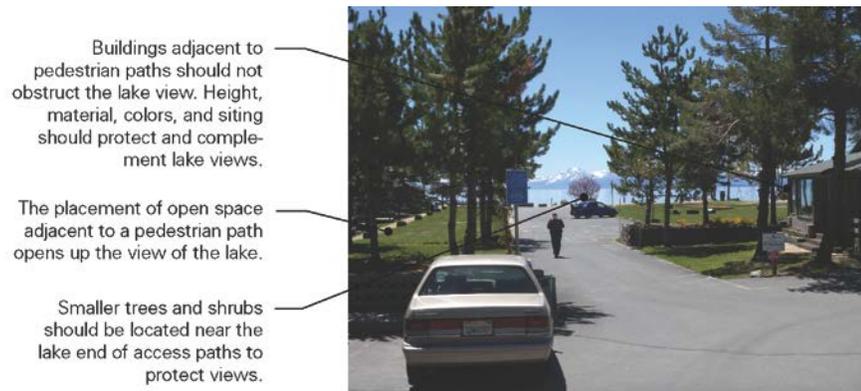
c. Building Entrances

- i. **Entrance Design.** Building entrances should be designed to reflect the Old Tahoe Style and should be highlighted with covered architectural elements, pedestrian-scaled signage, paving, and building transparency. Walkways or stairways leading to the building entrance should be clearly visible from the right-of-way and from adjacent development.



d. Shorezone Design

- i. **View Protection.** Orient pedestrian paths and vehicular circulation to maximize and protect shoreline views from the public rights-of-way. Identify areas that best allow lake views and design buildings and landscaping to maximize visual access. Where buildings, structures, or parking or loading areas can be seen within a scenic corridor, minimize the impact with vegetation, landforms, or colors and materials that blend with the surroundings.



- ii. **Access.** Provide clearly demarcated public access to the shorezone from public rights-of way. Incorporate landscaped edges, furniture, changes in elevation, and paving, while ensuring that all landscape design is sensitive to the natural habitat. When located adjacent to a parking area, design the access path to be visually distinct from the parking area.
6. **Shorezone.** Within the specified shorezone tolerance district, the following primary uses may be permitted by TRPA in the backshore, nearshore, and fore-shore. Accessory structures shall be regulated pursuant to the regulations applicable to the primary use upon which they are dependent in accordance with the TRPA Code of Ordinances. The following structures may be permitted by TRPA in the shorezone as an Allowed (A) or Special (S) use only if they are accessory to an existing, allowed use located on the same or adjoining littoral parcel.

TABLE 2.04.B-6: SHOREZONE – NORTH TAHOE EAST MIXED-USE SUBDISTRICTS		
<i>Tolerance Districts</i>	<i>6</i>	<i>7</i>
Maximum Community Noise Equivalent Level (CNEL)		
Shorezone	55	55
Lakezone	50	50
Primary Uses		
Water- Oriented Outdoor Recreation Concessions	S	S
Beach Recreation	A	A
Waterborne Borne Transit	A	A
Tour Boat Operations	S	S
Safety and Navigation Facilities	A	A
Marinas	S	S
Boat Launching Facilities	S	S
Salvage Operations	S	S
Accessory Structures		
Buoys	A	A
Piers	A	A
Fences	S	S
Boat Ramps	A	A
Breakwaters or Jetties	S	S
Shoreline Protective Structures	S	S
Water Intake Lines	A	A
Floating Platforms and Docs	A	A

C. North Tahoe West Mixed-Use Subdistricts.

1. **Purpose.** The purposes of the North Tahoe West Plan Area Mixed-Use Subdistricts are to:
 - a. Provide for the orderly, well-planned, and balanced growth of the Tahoe Vista and Carnelian Bay areas and support their role as important commercial, service, tourist, and community gathering places for local residents and visitors with easy access to recreational opportunities.
 - b. Promote Tahoe Vista and Carnelian Bay areas as pedestrian- and transit-oriented, mixed-use community centers.
 - c. Encourage a mix of uses that promotes convenience, economic vitality, and a pleasant quality of life and improve access to a greater range of facilities and services for surrounding residential neighborhoods.
 - d. Establish design standards that improve the pedestrian-orientation and visual quality of development and create a unified, distinctive, and attractive character.
2. **Planning Statements.** Planning statements for each Mixed-Use Subdistrict are as follows:
 - a. Mixed-Use Community Center (MU-CC). The Mixed-Use Community Center is made up of Mixed-Use Community Center East (MU-CCE) and Mixed-Use Community Center West (MU-CCW). Together, these subdistricts are the heart of the Tahoe Vista area. MU-CC is intended to maintain and enhance the pedestrian- and transit-oriented environment of retail, restaurants, services, and tourist accommodation with easy access to the lake and recreational activities.
 - i. **Special Designations.** (See Section 3.14)
 - (1) TDR Receiving Area for: Existing Development; Multi-Residential Units
 - (2) Preferred Affordable Housing Area
 - (3) Multi-Residential Incentive Program Area
 - ii. **Special Policies.**
 - (1) A mix of tourist and residential serving commercial is encouraged in the MU-CCW area.
 - (2) Public outdoor recreation and tourist uses are encouraged in the MU-CCE area and this area is targeted for increased public access to Lake Tahoe.
 - (3) All projects, as a condition of approval, shall implement frontage improvements consistent with Area Plan requirements.
 - (4) All projects shall be subject to the design guidelines consistent with Area Plan requirements.
 - (5) For the Placer County project review process for design review and signage, retain the existence and participa-

tion of the North Tahoe Design Review Committee. The County should consider the recommendations of the Committee prior to taking action on any project subject to Committee review.

- (6) Projects which provide substantial rehabilitation by remodeling, upgrading, or other aesthetic improvements shall be eligible for incentives.
- (7) The focus of redevelopment should be on the beach area.
- (8) TRPA and Placer County staff should provide quick and responsive project review through a coordinated review process.
- (9) Outdoor advertising shall be subject to Area Plan standards and guidelines.
- (10) Nonconforming signs shall be subject to an amortization plan and incentive program to provide for the eventual elimination or replacement of such signs.
- (11) All projects within the TRPA scenic corridor shall be responsible for removing, relocating, or screening overhead utilities as a condition of project approval. The decision making body may waive this requirement if the project is part of an underground program or the undergrounding has been determined by TRPA not to be necessary to meet TRPA scenic targets.
- (12) Projects with existing coverage in excess of 75 percent of their project area shall be required to provide an increase in landscaping equal to 5% of the project area. The landscaping requirement shall be met within the project area or, if not feasible, off site in a related area. This condition may be waived by the Design Review Committee if the project is part of an assessment district which is providing the required increase in landscaping or the landscaping requirement has been met by a previous approval.
- (13) The Design Review Committee shall consider the recommendations contained within the Scenic Quality Improvement Program (SQIP) when reviewing projects and where appropriate, incorporate conditions of approval to implement the recommendations.
- (14) Projects located between the designated scenic corridors and Lake Tahoe shall not cause a reduction of the views of Lake Tahoe from the corridors. TRPA may consider as an alternative, off-site improvements if it is determined there is a net increase in the lake views within the scenic unit.

- (15) Projects in Tolerance District One (1) shall not increase disturbance or land coverage and shall be limited to maintenance dredging.
- b. Mixed-Use Gateway (MU-G). The Mixed-Use Gateway is made up of Mixed-Use Gateway East (MU-GE) and Mixed-Use Gateway West (MU-GW). Together, these subdistricts intended to create an attractive mixed-use commercial corridor that provides a welcoming gateway to Tahoe Vista. The physical form varies to reflect the commercial mixed-use character of the gateway corridor and to transition from surrounding, lower-density districts to the community center.
- i. **Special Designations.** (See Section 3.14)
 - (1) TDR Receiving Area for: Existing Development; Multi-Residential Units
 - (2) Preferred Affordable Housing Area
 - (3) Multi-Residential Incentive Program Area
 - ii. **Special Policies.**
 - (1) Tourist oriented uses are encouraged in the MU-GW area and priority should be given to locating tourist accommodation uses and beach access in this area.
 - (2) All projects, as a condition of approval, shall implement frontage improvements consistent with Area Plan requirements.
 - (3) All projects shall be subject to the design guidelines consistent with Area Plan requirements.
 - (4) For the Placer County project review process for design review and signage, retain the existence and participation of the North Tahoe Design Review Committee. The County should consider the recommendations of the Committee prior to taking action on any project subject to Committee review.
 - (5) Projects which provide substantial rehabilitation by remodeling, upgrading, or other aesthetic improvements shall be eligible for incentives.
 - (6) The focus of redevelopment should be on the beach area.
 - (7) TRPA and Placer County staff should provide quick and responsive project review through a coordinated review process.
 - (8) Outdoor advertising shall be subject to Area Plan standards and guidelines.
 - (9) Nonconforming signs shall be subject to an amortization plan and incentive program to provide for the eventual elimination or replacement of such signs.

- (10) All projects within the TRPA scenic corridor shall be responsible for removing, relocating, or screening overhead utilities as a condition of project approval. The decision making body may waive this requirement if the project is part of an underground program or the undergrounding has been determined by TRPA not to be necessary to meet TRPA scenic targets.
 - (11) Projects with existing coverage in excess of 75 percent of their project area shall be required to provide an increase in landscaping equal to 5% of the project area. The landscaping requirement shall be met within the project area or, if not feasible, off site in a related area. This condition may be waived by the Design Review Committee if the project is part of an assessment district which is providing the required increase in landscaping or the landscaping requirement has been met by a previous approval.
 - (12) The Design Review Committee shall consider the recommendations contained within the Scenic Quality Improvement Program (SQIP) when reviewing projects and where appropriate, incorporate conditions of approval to implement the recommendations.
 - (13) Projects located between the designated scenic corridors and Lake Tahoe shall not cause a reduction of the views of Lake Tahoe from the corridors. TRPA may consider as an alternative, off-site improvements if it is determined there is a net increase in the lake views within the scenic unit.
 - (14) Projects in Tolerance District One (1) shall not increase disturbance or land coverage and shall be limited to maintenance dredging.
- c. Mixed-Use Neighborhood Center (MU-NC). MU-NC is intended to provide a central gathering place where residents and visitors can enjoy a range of commercial, retail, service, tourist accommodation, and residential uses in a compact development pattern, creating an aesthetically-pleasing environment for pedestrians, cyclists, and automobile drivers.
- i. **Special Designations.** (See Section 3.14)
 - (1) TDR Receiving Area for: Existing Development
 - ii. **Special Policies.**
 - (1) All projects, as a condition of approval shall implement or commit to a five year schedule to implement sidewalk improvements. This condition may be waived if the project is in an assessment district already committed to such improvements.

- (2) All projects shall be subject to the Area Plan design standards and guidelines. Standards may be adjusted to preserve natural features. Setback standards on State Route 28 may only be reduced upon making the required findings of the TRPA Code for scenic corridors.
- (3) For the Placer County project review process for design review and signage, retain the existence and participation of the North Tahoe Design Review Committee. The County and TRPA should consider the recommendations of the Committee prior to taking action on any project subject to Committee review.
- (4) Projects which provide substantial rehabilitation by remodeling, upgrading, or other aesthetic improvements shall be eligible for incentives.
- (5) TRPA and Placer County staff should provide quick and responsive project review through a coordinated review process.
- (6) Outdoor advertising shall be subject to Area Plan standards and guidelines.
- (7) Nonconforming signs shall be subject to an amortization plan and incentive program to provide for the eventual elimination or replacement of such signs.
- (8) All projects within the TRPA scenic corridor shall be responsible for removing, relocating, or screening overhead utilities as a condition of project approval. TRPA may waive this requirement if the project is part of an underground program or the undergrounding has been determined by TRPA not to be necessary to meet TRPA scenic targets.
- (9) Projects with existing coverage in excess of 75% of their project area, shall be required to provide an increase in landscaping equal to 5% of the project area. The landscaping requirement shall be met within the project area or, if not feasible, off site in a related area. This condition may be waived by the Design Review Committee if the project is part of an assessment district which is providing the required increase in landscaping or the landscaping requirement has been met by a previous approval.
- (10) The Design Review Committee shall consider the recommendations contained within the Scenic Quality Improvement Program (SQIP) when reviewing projects and where appropriate, incorporate conditions of approval to implement the recommendations.

- (11) Projects located between the designated scenic corridors and Lake Tahoe shall not cause a reduction of the views of Lake Tahoe from the corridors. TRPA may consider off-site improvements as an alternative if it is determined there is a net increase in the lake views within the scenic unit.