

Martis Valley West Parcel Specific Plan

4. CIRCULATION

4.1 Circulation Concept

The MVWPSP is located adjacent to SR 267, approximately seven miles from the Town of Truckee and five miles from Kings Beach. Primary access to the MVWPSP will be provided by a connection to SR 267. The primary road will wind through the MVWPSP, with secondary roads providing access to phased development pods and/or parcels. Bike paths and trails will provide pedestrian access throughout the MVWPSP, with connections to off-site trails, such as the Tahoe Rim Trail and Tompkins Memorial Trail. A covered bus shelter will be located near the MVWPSP entrance.

The MVCP includes a number of goals and policies for circulation, ensuring the safe and efficient movement of people and goods; maintaining appropriate service levels on County roads; promoting transit to reduce congestion, improving the environment and promoting non-vehicular transportation; and reducing travel demand. As discussed in more detail below, the MVWPSP implements these goals and policies. The MVWPSP provides for improvements at the SR 267/Project Entrance intersection to maintain efficient operation on SR 267; provides for an efficient internal roadway system; promotes non-vehicular travel by providing trails and paths throughout the development and transit services and facilities; and provides emergency access.

4.2 Circulation Setting

The MVWPSP is located within the Martis Valley, southeast of Northstar resort. The Martis Valley is largely a resort destination area that attracts tourists and seasonal residents during the summer and winter. Traffic conditions therefore vary by season. Automobiles are the primary mode of travel in the region. Transit services are provided in the peak winter and summer months. There are myriad trails and paths throughout the region, but these tend to be used for recreational purposes, rather than commuting or basic transportation.

Both the East and West Parcels are accessed via private unpaved roads connecting to SR 267. There are no public or paved roads within the MVWPSP. There are numerous private dirt roads and trails throughout both the East and West Parcels. In addition, there is a U.S. Forest Service road (Fibreboard Freeway) that runs near the southern portion of the West

Parcel.

The local circulation system is described below and shown in Figure 4-1.

Roadways

Interstate 80 provides the main access to the Martis Valley, particularly for travelers from Sacramento, the Bay Area and Reno (see Figure 1-1). Interstate 80 runs from San Francisco, through the Town of Truckee, to Reno and then continues east.

State Route 267 is a two-lane highway running in a general north-west-southeast alignment from Interstate 80 in Truckee to SR 28 in Kings Beach. SR 267 bisects the East and West Parcels, and provides access to both via private, dirt roads and the Fibreboard Freeway. SR 267 runs generally east-west adjacent to the MVWPSP (see Figure 4-1). The State Route 267 Transportation Concept Report, prepared by Caltrans District 3 in April 2012, identifies a 20-year concept level of service (LOS) of D for the segment serving the MVWPSP.

According to the County Capital Improvements Program (February 2013), several improvements are planned for SR 267 in the vicinity of the MVWPSP, including the extension of the southbound climbing lane to Brockway Summit and a northbound passing lane at Brockway Summit.

When the MVCP was approved, a connection to SR 267 was identified roughly one-half mile below Brockway Summit to provide access to the East Parcel. The Specific Plan shifts that connection to the north to allow access to the West Parcel.

Highlands View Drive is the closest improved road to the MVWPSP, located approximately 1.3 miles northwest of the MVWPSP access. Highlands View Drive, a two-lane paved road, provides access to the approved mid-mountain Highlands development, Northstar ski facilities and The Ritz Carlton, Lake Tahoe.

Northstar Drive is a two-lane roadway connecting from SR 267 west to the Northstar resort community and its associated residential, commercial, and resort areas. The SR 267/Northstar Drive intersection is located approximately 1.6 miles northwest of the proposed MVWPSP access location. A Traffic and Parking Management Plan is implemented by Northstar California Resort on peak-days of winter traffic. The plan includes coordination with Caltrans to optimize signal timing along the SR 267 corridor and implementation of manual traffic control at key intersections on Northstar Drive.

Fibreboard Freeway is a paved, unstriped road of varying width that is managed by the US Forest Service and provides access to the southern

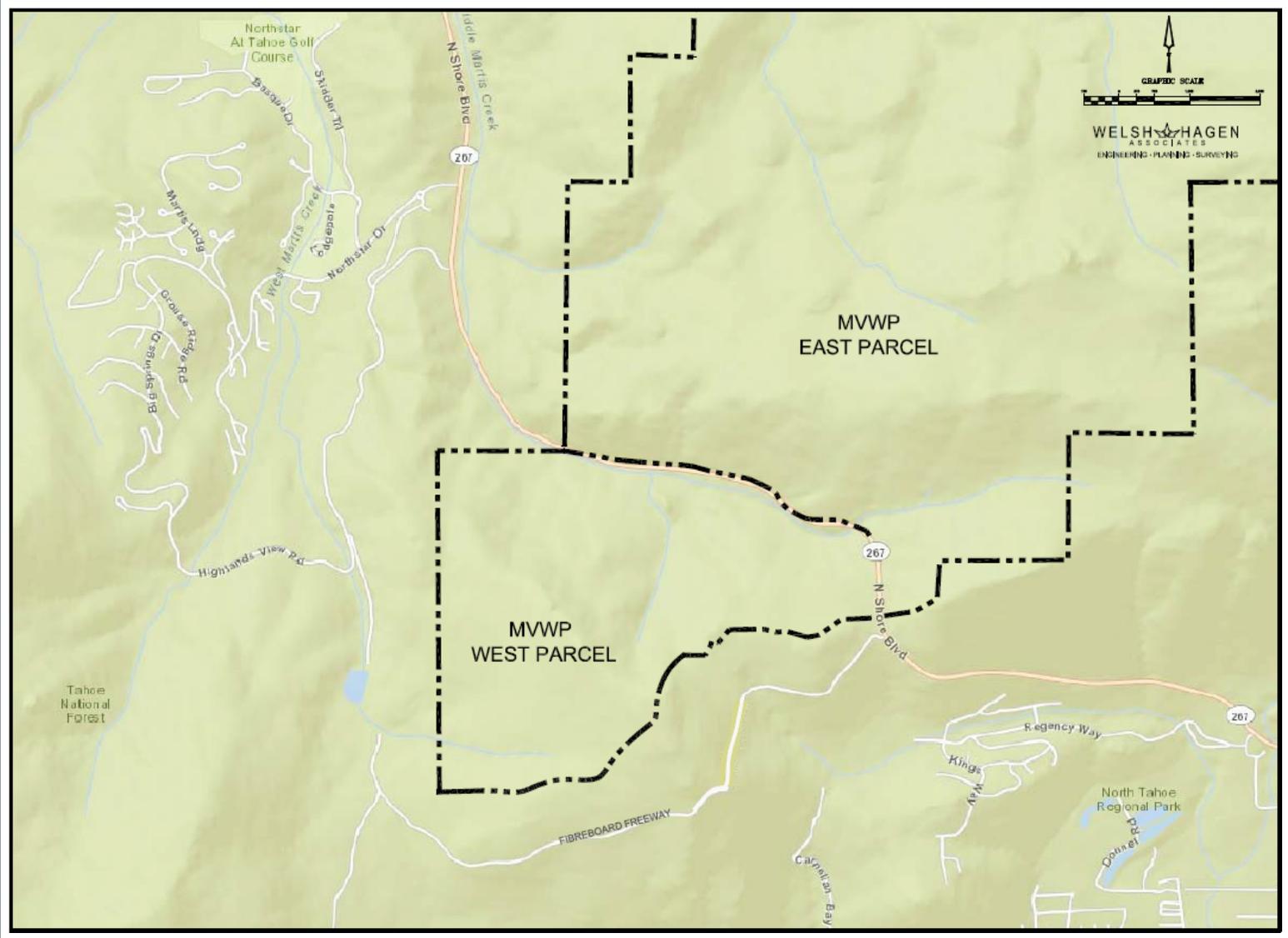


Figure 4-1 Local Circulation System

portion of the West Parcel via dirt roads, as well as lands to the south and west.

Bicycle and Pedestrian Circulation

As stated earlier, recreational biking occurs throughout the Martis Valley, but using bicycles as an alternative mode of transportation is not common, due in part to natural grades and the lack of bicycle facilities. There are no bicycle or pedestrian lanes or facilities along SR 267 in the vicinity of the MVWPSP. A relatively wide shoulder makes cycling along SR 267 feasible, and Caltrans considers SR 267 from the County line to Brockway Summit to be a “Share the Road” facility in its Caltrans District 3, State Highway Bicycle Facility Plan, June 2013.

The Tahoe Rim Trail is accessible from trails in the southern portion of the MVWPSP, and the Fibreboard Freeway provides access to a large network of hiking, cross-country ski/snowshoe and cycling trails in the plan area vicinity. While distances to activity centers and the mountainous terrain preclude significant pedestrian or cycling travel for shopping, school or employment, it can be expected that these non-auto connections will be attractive to MVWPSP residents and visitors who cycle and/or cross-country ski/snowshoe/walk/hike for recreation and exercise.

See Chapter 6 for more discussion on associated recreational uses, including cross-country skiing, hiking and biking trails.

Transit

SR 267 near the MVWPSP is served by Placer County’s Tahoe Area Regional Transit (TART) transit program. TART operates hourly service during the daytime period in the winter season connecting Crystal Bay and Kings Beach on the south with Northstar and Truckee on the north. In summer, TART connects Crystal Bay and Kings Beach on the south with Northstar on the north during the day. In addition, the “Night Rider” program managed by the Truckee- North Tahoe Transportation Management Association provides evening service on a regular basis in both winter and summer between Northstar, Kings Beach and Crystal Bay.

Parking

There are no formal parking facilities within the MVWPSP. Some parking is available at the Martis Peak turnoff, which is used for access to back country skiing, and at the Brockway Summit turnout on the northwest side of 267, which is used by snowmobilers.

4.3 Circulation Goals & Policies

Goal CP-1: Provide safe and efficient access to and circulation through the MVWPSP.



Safe convenient transit stops will be provided for MVWPSP residents and visitors.



The MVWPSP will join and/or partner with local transit organizations.

Goal CP-2: Maintain and enhance access to the regional trail network for hiking, biking, snowshoeing, and cross-country skiing.

Goal CP-3: Provide opportunities to reduce single-occupancy vehicle use.

Policy CP-1: Engineer and construct roadways to meet applicable County and Caltrans standards, and to maintain appropriate levels of service and maintenance.

Policy CP-2: Provide a second access to SR 267 for emergency vehicles.

Policy CP-3: Provide a covered bus shelter/transit stop within the plan area that can accommodate transit vehicles traveling north and south on SR 267, and that is safe and convenient for MVWPSP residents and visitors. Access to the bus stop/shelter shall be adequate to accommodate regional transit vehicles without extensive off-route travel. Parking shall be provided for residents and visitors who drive to the transit stop.

Policy CP-4: Join and/or partner with local transit organizations and/or providers to extend service to the MVWPSP and to encourage the use of regional transit services.

Policy CP-5: Develop an extensive network of pedestrian, cross-country skiing, hiking and bike trails within the MVWPSP and provide internal connections or associated easements to the regional trail network.

Policy CP-6: Provide clearly marked access to public trails and, where appropriate, parking and/or other trailhead amenities.

Policy CP-7: If any roads that are used to access public trails are gated, provide a pedestrian connection with parking, passes or other means for the public to continue to be able to access the public trails.

Policy CP-8: In order to enable residents and guests to put chains on before accessing SR 267 when chain controls are in effect, Improvement Plans for the main access road shall include a chain control area adjacent to the main access road inclusive of signage. The chain control area and connection to the main access road shall be designed to have a minimum of two standard vehicle spaces.

Policy CP-9: Parking for multifamily and commercial uses and homeowner amenities shall meet County standards.

Policy CP-10: For condominiums and hotels, where feasible, parking shall be provided under buildings in order to minimize paving. Shared parking is encouraged.

Policy CP-11: Any gated subdivision access roads shall meet the Private Gated Entrance requirements of the Placer County Engineering & Surveying Division.

Policy CP-12: Project development or its associated Homeowner Associations shall become a member in perpetuity of the Truckee – North Tahoe Transporta-

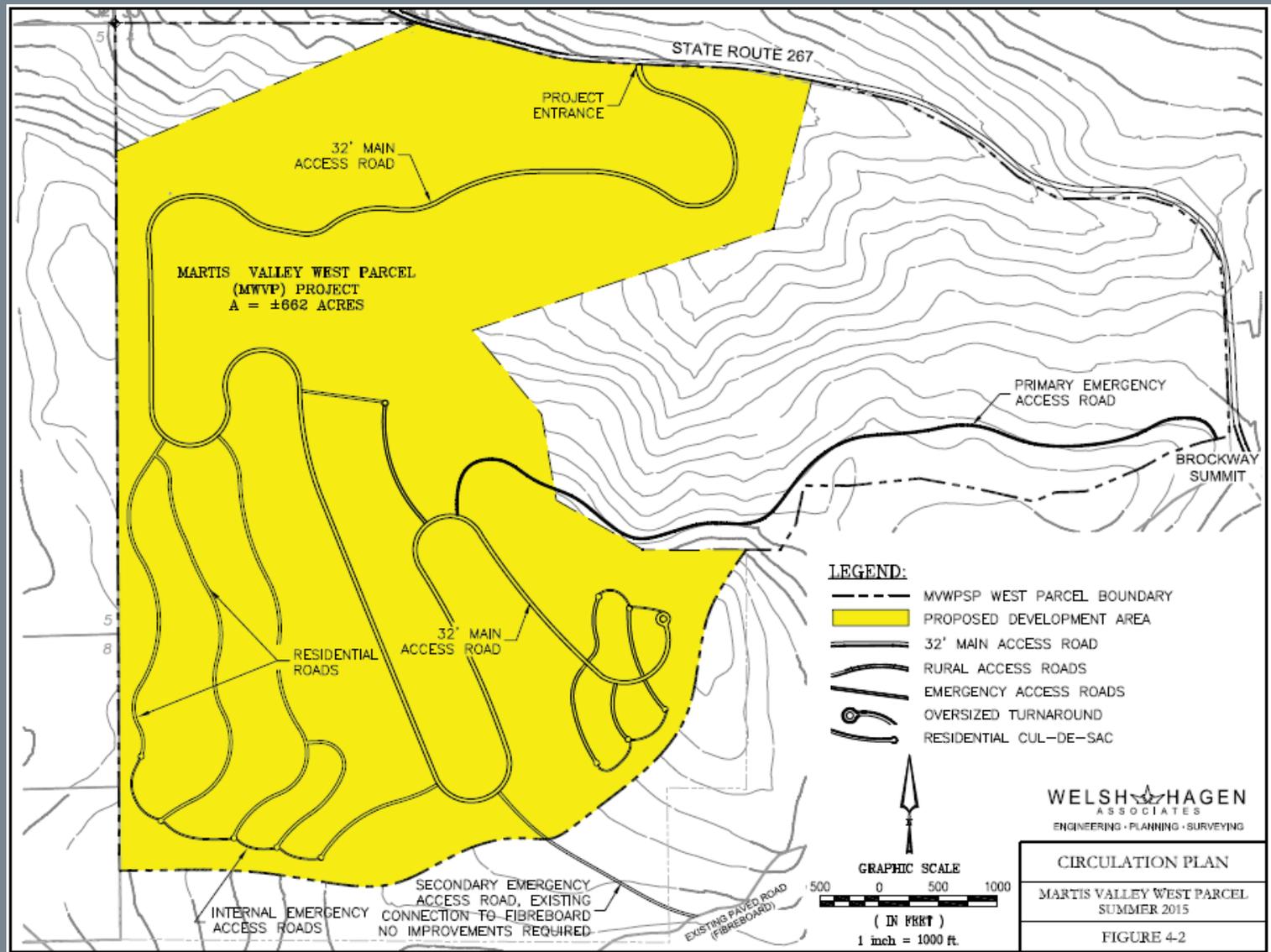


Figure 4-2 Circulation Plan

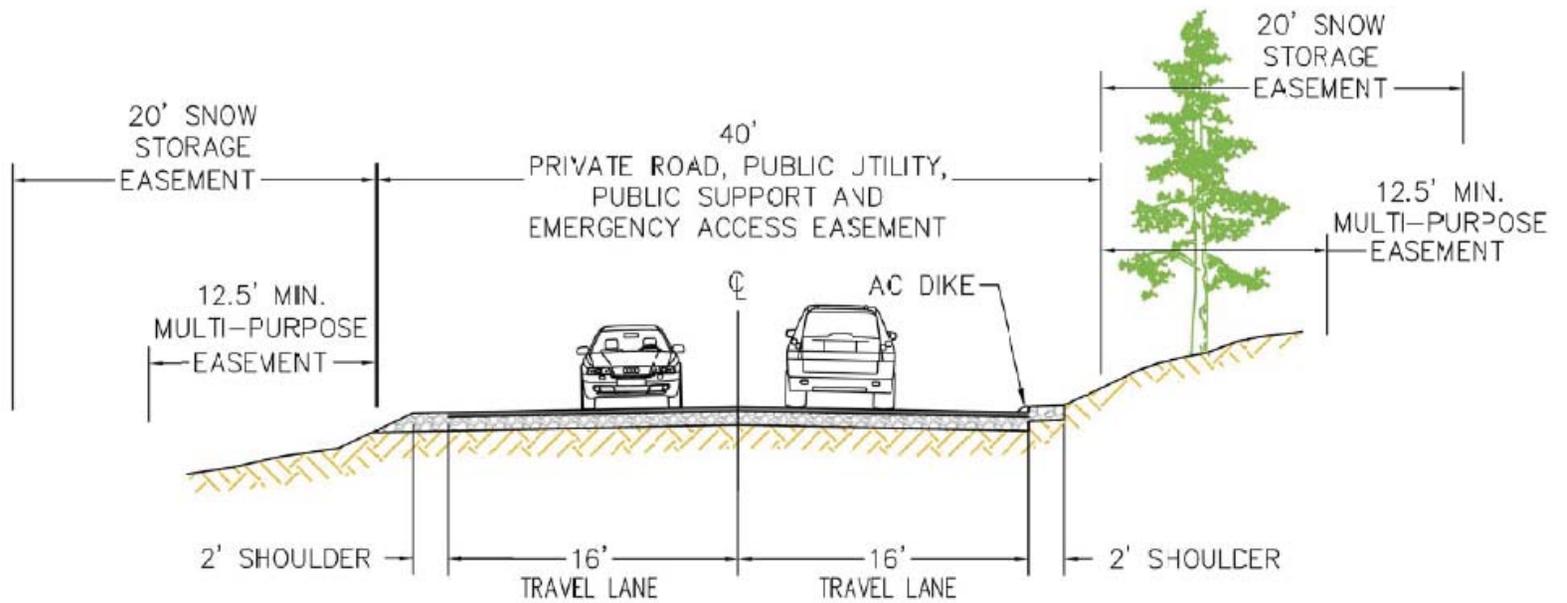


Figure 4-3 Main Access Road Cross-Section

tion Management Association.

Policy CP-13: Upon construction of the 340th residential unit, if justified by subdivision occupancy and alternating years' trip generation as demonstrated in a traffic report submitted to the Planning Director, a Homeowners Association (HOA) community shuttle will be implemented, reducing single-trip occupancy vehicles. The shuttle will transport residents and guests to local attractions, such as the neighborhood commercial, onsite recreation amenities, and Northstar Ski Resort.

4.4 Circulation Plan

Roadways

The MVWPSP roadways are shown on Figure 4-2 and described below. A main access road will provide a connection to SR 267 and extend through the entire MVWPSP. Secondary roads will branch off the main access road to provide direct access to residential and commercial parcels. All roads within the MVWPSP will be built to the standards shown in Figures 4-3, 4-4 and 4-5, and will provide easements for snow storage and utilities. Roads within the MVWPSP will be private.

Main Access Road

The main access road will include two lanes connecting to SR 267 approximately 1.3 miles south of Highlands View Road and 1.6 miles south of Northstar Drive and winding through the MVWPSP to its terminus in the southeast (see Figure 4-2). The typical cross section for the main access road will include two 16-foot vehicle lanes, a 2-foot shoulder, a snow storage easement and a multipurpose easement that could accommodate public utilities (see Figure 4-3). The main access road will terminate in an oversized turnaround.

A new three-legged intersection will be constructed at the project entrance and SR 267 with the following configuration:

- Northbound (westbound) through lane,
- Southbound (eastbound) through lane,
- Left-turn deceleration lane on northbound (westbound) SR 267 for left turns into the MVWPSP,
- Left-turn refuge area/acceleration lane northbound (westbound) SR 267 for left turns onto SR 267,
- Right turn acceleration lane on southbound (eastbound) SR 267 for right-turns out of the MVWPSP,

- Right-turn lane on southbound (eastbound) SR 267 for right turns into the MVWPSP,
- Dedicated Right and Left-turn lanes on the main access, and
- Five foot wide class II bicycle facilities on the said portion of SR 267, consistent with the Placer County Bikeway Master Plan.

During the winter, chain controls are often in place on SR 267. Traffic congestion can be increased by drivers who are unaware of the controls and/or wait until accessing SR 267 to put on chains. Therefore, MVWPSP residents and guests will be notified when chain-controls are in place, either by signage or other means (e.g., electronic media). In addition, turnouts will be provided on the Main Access road to allow drivers to put on chains before entering SR 267.

A free-span bridge will be installed across Martis Creek near SR 267. The Main Access road could be access-controlled to minimize potential parking and fire-life safety issues caused by parking on the street rather than designated parking areas by people accessing trails. Public pedestrian, bicycle, snowshoe and cross-country skier access will be provided where access-controlled roads lead to public trails.

Secondary Roads

Internal streets will have two vehicle lanes with snow storage and multipurpose easements (see Figure 4-4). As shown on Figure 4-2, some internal streets will terminate in cul-de-sacs (see Figure 4-5).

Internal roads providing access to subdivisions may be access-controlled (e.g., gated or provided with a check-in kiosk). Pedestrian, bicycle, snowshoe and cross-country skier access will be provided where access-controlled roads lead to public trails.

Trails

Hiking, cross-country skiing and bike trails will be provided throughout the MVWPSP and will allow connection to the existing and planned regional trail system. Trails are discussed in more detail in Chapter 6.

Transit

A bus shelter will be provided within the MVWPSP near SR 267. Buses will use the main access road to access the bus stop. By locating the bus stop within the MVWPSP, bus riders will not need to cross SR 267, and the stop can serve both west and eastbound travelers. Residents and visitors will access the transit stop by internal pedestrian/bike trails or will drive to it. Parking, a bike rack and a shelter will be provided at the bus stop.

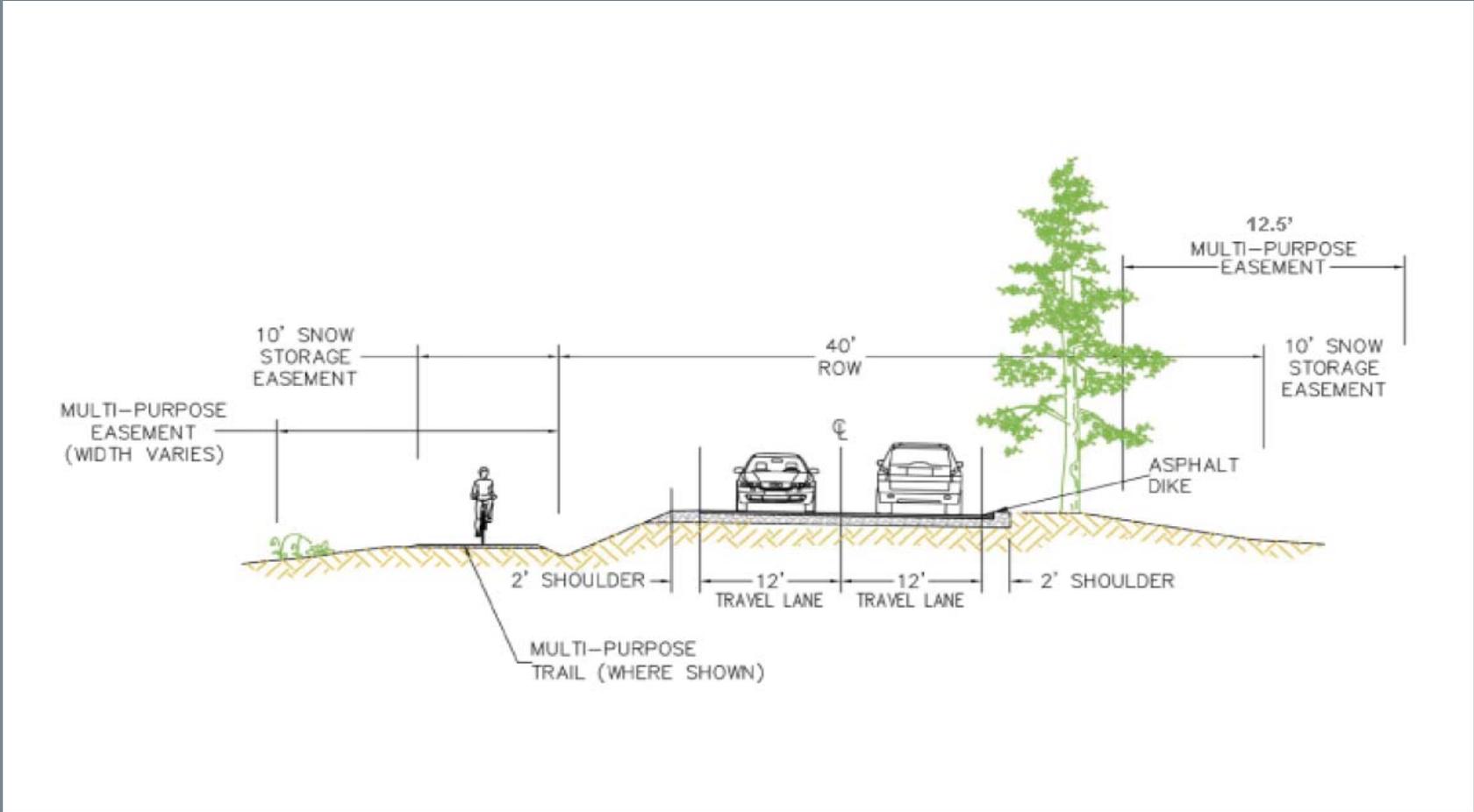


Figure 4-4 Secondary Road Cross-Section

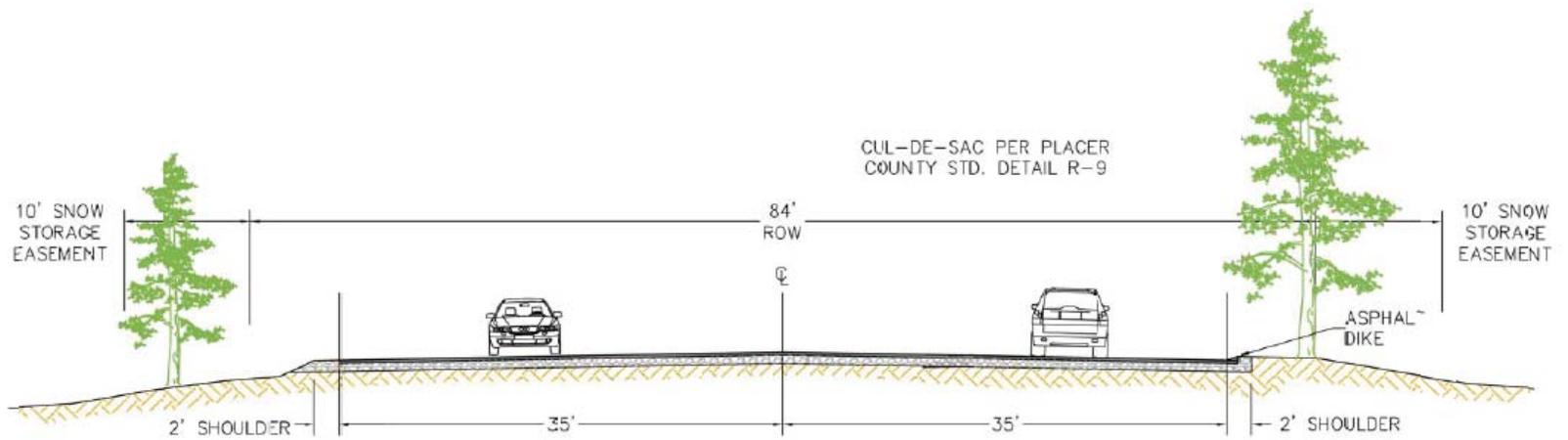
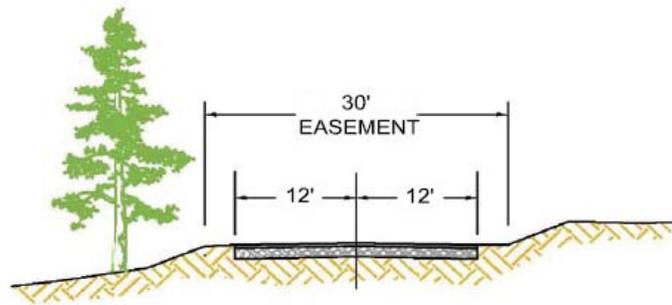
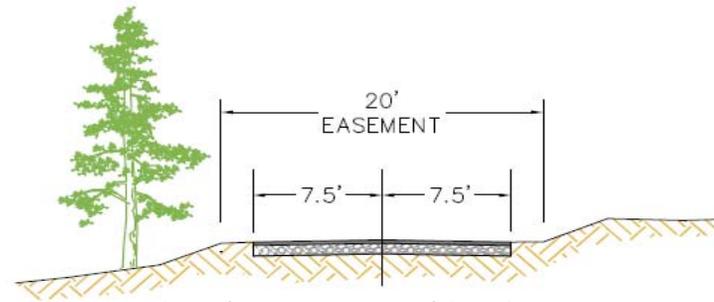


Figure 4-5 Cul-de-Sac Cross-Section



Primary Emergency Road Cross-Section



Secondary Emergency Road Cross-Section

Figure 4-6 Emergency Road Cross-Sections

Parking

For single-family residential uses, a minimum of 2 off-street parking spaces will be provided for each unit. For condos, 1.5 spaces will be provided per unit. Commercial uses will provide the number of spaces required for specific uses in the Placer County Zoning Ordinance, Section 17.54.060.

All parking spaces required for any land use other than single family dwelling, cabin or halfplex/duplex/triplex shall be designed and located to enable the maneuvering of vehicles on the site so that they may leave the site to enter any public or private road in a forward direction.

Designated parking areas will be considered with each phase providing public access to trailheads. If roads providing access to trails are access-controlled, public access will be granted for designated parking areas.

4.5 Emergency Vehicle Access

A primary emergency vehicle access (EVA) road would be constructed through the 325-acre forest land, connecting to SR 267 at Brockway Summit. The EVA would be a paved road that would be accessible in all seasons and used for emergency vehicles only. A cross section of the EVA is shown in Figure 4-6. A secondary EVA is proposed to be provided by existing connections to the Fibreboard Freeway, which runs near the southeastern corner of the West Parcel and connects to SR 267. The secondary EVA would not be a new access route or an improved road and would be used only in non-winter months and for catastrophic event evacuation. Both EVAs would be used for emergency vehicles only, unless needed to also evacuate residents.

Additional EVAs, internal to the MVWPSP, will connect internal cul-de-sacs, providing multiple means of exiting the MVWPSP development during emergencies.

4.6 Transportation Management

Transportation management is intended to reduce reliance on single-occupancy vehicles, thereby reducing congestion on local and regional roadways, air pollution and the emission of greenhouse gases. The MVWPSP is composed primarily of residences that would be used as second homes and/or transient lodging. The MVWPSP does not provide for any land uses that would have a large workforce. For these reasons, the MVWPSP would contribute little to commute traffic. The focus of transportation management within the MVWPSP is therefore on providing a mix of land uses that minimizes the need for residents and guests to leave the MVWPSP and/or to travel by automobile. This is accomplished in several key ways:

- Neighborhood-serving retail/commercial uses and recreational amenities will enable residents and guests to meet many needs onsite;
- Upon construction of the 340th residential unit, a Homeowners Association (HOA) community shuttle will be implemented, reducing single-trip occupancy vehicles. The shuttle will transport residents and guests to local attractions, such as Northstar;
- Cross-country skiing, hiking and biking trails throughout the MVWPSP will link residences to each other, to the regional trail system and to retail/commercial services and recreational amenities;
- A bus stop at the entrance of the MVWPSP will provide convenient access to transit;
- Partnerships with and support of local transit providers will promote transit services;
- A County Service Area within Martis Valley will financially support public transit services;
- The area will become a member in perpetuity of the Truckee – North Tahoe Transportation Management Association; and
- Visitors and residents will have access to bicycle, snowshoe and cross-country ski rentals.

