

# EXHIBIT A



## Placer Vineyards Specific Plan Placer County, California

*Revised November 2014  
Adopted July 2007*



# PLACER VINEYARDS

## Placer Vineyards Specific Plan Placer County, California

*Revised November 2014  
Adopted July 2007*



*2014 Revision Prepared by:  
Torrence Planning & Design Inc. &  
MacKay & Soms, Inc.  
2007 Plan Prepared by EDAW-AECOM*

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# PLACER VINEYARDS

Placer Vineyards Specific Plan  
Placer County, California

## EXECUTIVE SUMMARY



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The *Placer County General Plan* and the *Dry Creek/West Placer Community Plan* require the preparation of this Specific Plan to allow development of the area west of Walerga Road. Exhibit 1 of the General Plan designates Placer Vineyards as the “West Placer Specific Plan Area,” subject to various development standards. The Placer Vineyards Specific Plan is intended to provide a mechanism to ensure that the entire 5,230 acre Placer Vineyards Specific Plan Area, henceforth called the Plan Area, will be comprehensively planned.

The policies and guidelines contained in this Specific Plan and its adoption represent the first step in the approval process. This Specific Plan outlines future steps and approvals that will be required before development in the Plan Area occurs. It addresses land use and the sequencing of development and implementation measures that will ensure that construction of public infrastructure and implementation of public services occurs concurrently with new development.

Build-out of the Plan Area is anticipated to occur over a 20–30-year period. This Specific Plan attempts to balance the need for a coherent long-term vision with the equally important need to provide flexibility to accommodate changes in community needs and environmental regulations, to address specific site conditions, to simplify the review and implementation process by Placer County, and to accommodate other factors that will influence development during the build-out of the Plan Area.

In addition to the requirement that the Specific Plan conform to the *Placer County General Plan* and the directives of General Plan Exhibit 1, Placer Vineyards is designed to be a community guided by the following design principles (shown in bold font) and associated plan actions (described in further detail in the subsequent chapters of the Specific Plan).

**1. Promote a mixed use, compact development pattern linked to regional transportation systems.**

Placer Vineyards is planned to accommodate the long term growth needs of Placer County by establishing a series of concentrated urban centers and a mix of residential neighborhood developments uniquely situated around public amenities, employment, housing, shopping, recreational uses, and multiple transportation options. The Plan Area will be connected through a regional bus rapid transit system, a transit center located on Watt Avenue within the East Village Center, and through a system of on-and off-street trails, linked into the regional trail system.

- 2. Create a livable, pedestrian friendly environment with a distinct community identity and sense of place. Create safe and accessible links between neighborhoods, centers, and districts within the Plan Area and the surrounding community.**



Placer Vineyards is designed to provide locations for schools, parks, public facilities, and activity centers as focal points for its neighborhoods and communities, with emphasis on pedestrian access and activity at these facilities. Specific Plan policies and guidelines require that homes front onto pedestrian sidewalks, when feasible, provide design standards that allow commercial activities such as outdoor dining to extend into the pedestrian realm, and provide a variety of streetscape environments and pedestrian experiences along connecting trails and paths.

A town center, 2 village centers, and several neighborhood centers provide a mix of uses, designed to complement the needs and activities within the community. A large regional commercial corridor is strategically located along Base Line Road for maximum visibility and to provide buffers to residential developments to the south.

- 3. Provide a diversity of housing choices types, styles, densities, and costs.**

The Specific Plan encourages the provision of a range of housing choices types, styles, and densities to accommodate all income levels. Development standards are flexible to accommodate a wide array of housing types. The goal is to encourage new and creative development forms, especially in high density residential areas and mixed-use centers.

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**4. Provide a range of transportation choices—streets, paths, and trails with links to local and regional transportation systems.**

Placer Vineyards is designed to be a transit-friendly community with easy access from residential neighborhoods to a range of transportation facilities. A transit center is designed to serve the Plan Area and to provide links to a potential, future regional bus rapid transit system along Watt Avenue. A grid of streets and a continuous network of shaded sidewalks and bikeway trails loop through neighborhood centers, schools, and parks.

**5. Protect and enhance the highest quality natural features and resources on the site by incorporating native oaks, wetlands, creeks, and drainage systems into a community-wide open space system. Provide transitional buffers sensitive to the character of adjacent land uses.**

The Plan Area will permanently preserve 692.8 acres of contiguous open space to protect and enhance natural drainageways, wildlife and riparian corridors, significant wetlands, and oak groves that provide an open space refuge. As guided by this Specific Plan, over 35 miles of Class I bike paths and 7.2 miles of multi-purpose trails shall be constructed by private developers, including a system of backbone trails with connecting trail routes from the open space portions of their property.

**6. Promote a balance of uses—housing, employment, schools, parks, recreation, and places to shop—that support a balance of jobs to housing in the region.**

The result will be a planned community with a population of approximately 32,814 people (including the SPA), about 14,132 homes in various new residential neighborhoods, 108 acres of office development, 166 acres of retail development, 842.8 acres of new parks and open space, 6 elementary schools, 2 middle schools, and 1 high school. A transit and trail system is planned to provide connections throughout the Plan Area and to link into the existing and planned transit and trail systems within the greater region.

**7. Ensure that adequate public facilities are provided concurrent with development.**

The Plan Area will be served by a backbone infrastructure system (of water, wastewater, recycled water, and drainage) that will serve the needs of future residents and allow existing residents the opportunity to tie into upgraded facilities. The *Placer Vineyards Public Facilities Financing Plan* and the *Placer Vineyards Urban Services Plan* will ensure the timely delivery and funding for infrastructure and community service facilities in the Plan Area.

**CHAPTER I: INTRODUCTION**



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## 1.1 PURPOSE

The *Placer Vineyards Specific Plan* is a guide for the future urban growth and development in southwest Placer County. This Specific Plan defines a comprehensive set of policies to govern urban development in the Placer Vineyards Specific Plan Area (Plan Area).

Placer Vineyards is envisioned as a new urban-suburban community with a mix of residential, employment, school, and recreational uses. This Specific Plan sets policies for both on-site as well as off-site permanent preservation of significant natural resources and open space. The Specific Plan provides mechanisms for the implementation of public services and utilities and encourages the creation of cultural and educational community spaces. Placer Vineyards is guided by the following objectives:

- ◆ Protect and enhance the highest quality natural features and resources of the site and provide transitional buffers sensitive to the character of adjacent land uses;
- ◆ Promote compact mixed-use development that strives to provide a balance of uses, diverse housing and transportation choices, and contributes to a jobs to housing balance within the region;
- ◆ Establish a pedestrian-friendly community and access to a regional system of trails that link neighborhoods; and
- ◆ Develop a series of districts with their own unique site identity with urban centers, neighborhoods, and community serving facilities (schools, parks, and public amenities).

The Specific Plan and subsequent entitlement process is consistent with the goals and policies identified by the *Placer County General Plan* and allows for a sequence of community input and government review to ensure that development occurs in a logical, consistent, and timely manner.

## 1.2 PROJECT LOCATION AND CONTEXT

Placer Vineyards includes approximately 5,230 acres of land located in the southwest corner of Placer County, approximately 15 miles north of the City of Sacramento (see Figure 1-1). The Plan Area is bounded to the north by Base Line Road, to the south by the Sacramento County line, to the west by the Sutter County line and Pleasant Grove Road, and to the east by the Dry Creek Parkway and Walerga Road (see Figure 1-2). The major roads serving the Plan Area-Base

Line Road, Watt Avenue, and Walerga Road—are projected to be future thoroughfares. A number of smaller country roads and unpaved 2-lane roads traverse north-south through the Plan Area. A major electrical power line easement traverses east-west. 2 smaller electrical power easements traverse north-south.

The land is primarily level and has gently rolling terrain that slopes primarily southwest and partially toward Dry Creek. A number of small creeks and wetlands are also dispersed throughout the site. The land consists mostly of undeveloped grazing and agricultural land, with approximately 150 residences located in the Special Planning Area (SPA), concentrated in the northwest corner of the Plan Area (refer to Figure 1.2 for the location of the SPA). A number of home occupation and ancillary uses are located throughout the rural residential areas. A self-storage facility is located on Pleasant Grove Road at the southwest corner of the Plan Area. There are also a few commercial uses northwest of the Plan Area, including a convenience store and service commercial uses (trucking operation and boat and RV storage). An abandoned portion of the Union Pacific Railroad traverses the western portion of the Plan Area.

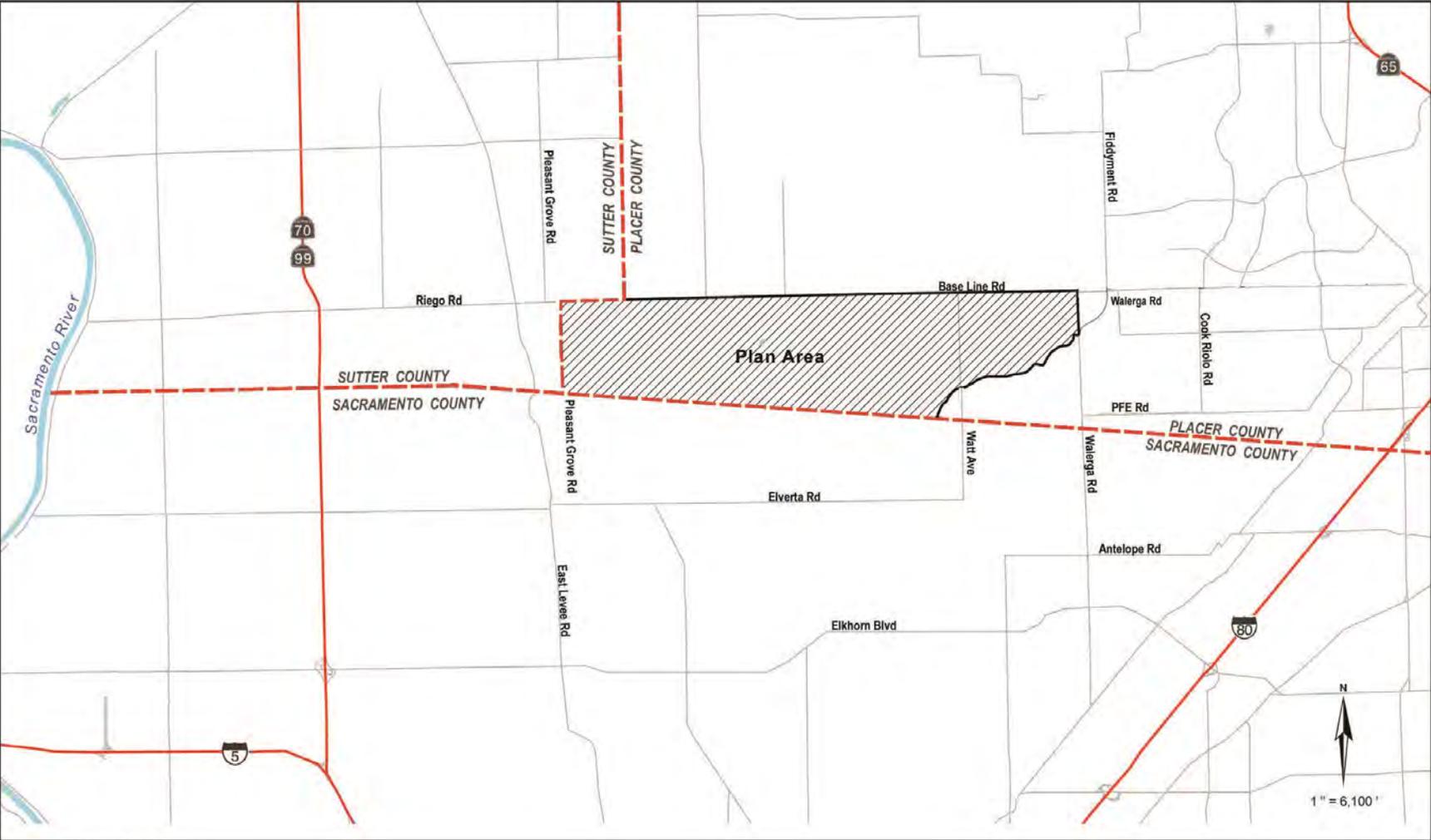
The primary land use in the area has historically been agriculture, with rice lands, vineyards, orchards, grazing land, and areas devoted to field crops. In some areas the land has lain fallow for several years. The Plan Area also contains a number of small tree groves and isolated oak stands, primarily along the southern border adjacent to the Dry Creek parkway and existing Dyer Lane.

Neighboring land uses in the area consist of agricultural grazing land, farming, and large-lot rural residential uses. Urban development is also found nearby in adjacent Sacramento County within the communities of Antelope, Rio Linda, and Elverta. Roseville continues to see growth east of Walerga Road and north of Base Line Road. Refer to Figure 1-3 for a map of planning areas and projects surrounding the Plan Area.



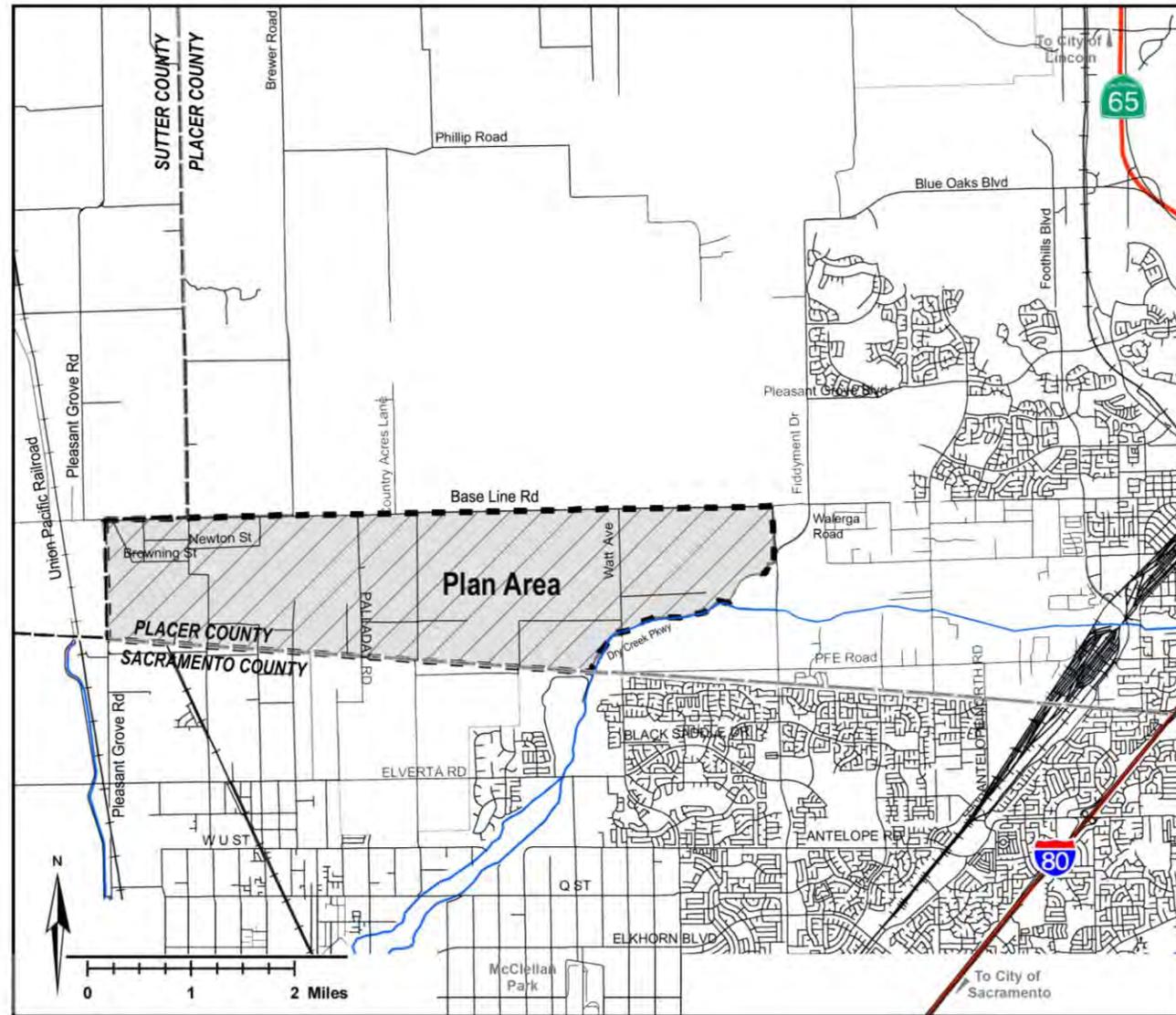
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Figure 1.1 - Placer Vineyards Regional



Source: Quad Knopf, 2005

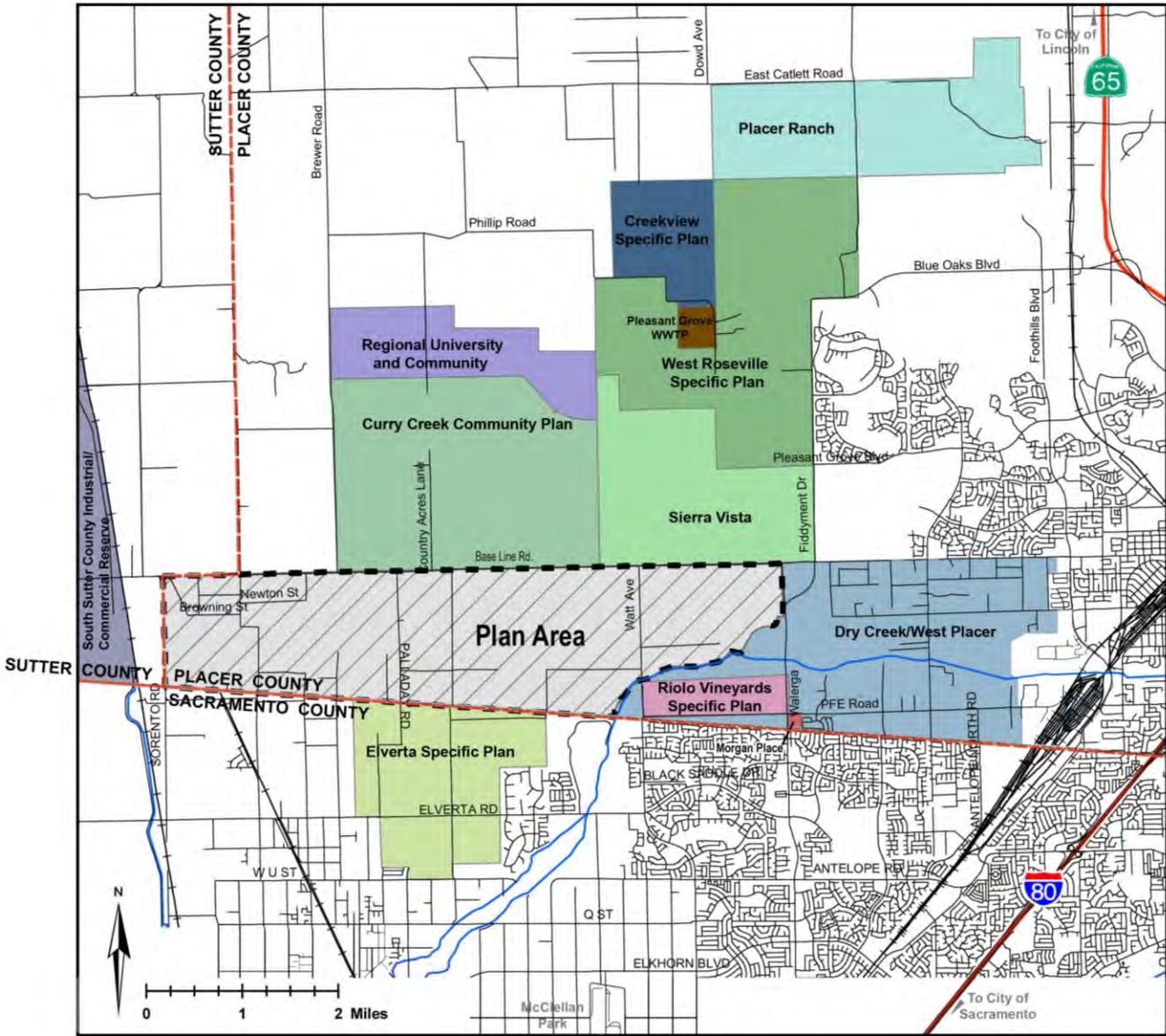
Figure 1.2 - Placer Vineyards Site Location



Source: Placer County Planning Dept., 2001 / Sacramento Planning Dept., 2002 / Quad Knopf, 2005

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Figure 1.3 - Proposed Development Surrounding Placer



Source: Placer County Planning Dept., 2001 / Sacramento Planning Dept., 2002 / Quad Knopf, 2005

### 1.3 PROJECT BACKGROUND

In the early 1990s, the Placer Vineyards Property Owners' Group, 21 land owners or owner representatives controlling approximately 4,250 acres or 81 percent of the 5,230-acre Plan Area, initiated the preparation of the first draft Specific Plan. After a planning effort coordinated over 5 years, the first draft Specific Plan was submitted to Placer County in December 1996. Subsequently, the first draft Specific Plan was revised and a public review draft of the Specific Plan was published in May 2003. On July 16, 2007, the Placer County Board of Supervisors approved the Specific Plan and certified the Final Environmental Impact Report. On February 14, 2012, the Board of Supervisors adopted an amendment to the Specific Plan and an addendum to the certified Environmental Impact Report. Additionally, modifications to the Mitigation Monitoring and Reporting Program and corresponding text revisions to the Certified Environmental Impact Report were approved by the Board on September 11, 2012. An Amendment to the Specific Plan and an Addendum to the Certified EIR were approved by the Placer County Board of Supervisors on December 9, 2014. This revised *Placer Vineyards Specific Plan* supersedes and replaces all previous versions of the Specific Plan.

The remaining 19 percent of the Plan Area (or approximately 979 acres) consists almost entirely of land in the far western part of the Plan Area, known as the Special Planning Area (SPA). These are mostly rural residential-agricultural parcels ranging in size from 1 to 40 acres. While included in the Plan Area, these rural residential lots will be governed under their existing land use and zoning classifications and are not limited or directed by the policies contained in this Specific Plan.

### 1.4 PURPOSE AND INTENT OF SPECIFIC PLAN

The *Placer County General Plan* directs the preparation of a Specific Plan to allow development of the *Dry Creek/West Placer Community Plan* area with the intent that the entire 5,230-acre Plan Area would be comprehensively planned. The Specific Plan sets forth regulations and programs which will carry out the goals and policies of the General Plan and ensure development is of the highest quality possible.

The *Placer Vineyards Specific Plan* addresses a comprehensive range of issues associated with the development of the Plan Area. This document is designed as a framework of policies, guidelines, and standards which shall guide the build-out of the Plan Area, expected to occur over a period of 20 to 30 years.

### 1.5 LEGAL AUTHORITY

Placer County is authorized to adopt this Specific Plan following the provisions of California Planning and Land Use Law (Title 7, Chapter 3, Article 8 [Sections 65450– 65457] of the California Government Code and of Section 17.58.200 of the *Placer County Zoning Ordinance*. These provisions require that a specific plan be consistent with the adopted general plan of the jurisdiction in which the plan is located. All projects that follow within the Plan Area shall be consistent with this Specific Plan and the *Placer County General Plan*.

### 1.6 RELATIONSHIP TO OTHER DOCUMENTS

#### 1.6.1 GENERAL PLAN AND COMMUNITY PLAN

The August 1994 *Placer County General Plan* identifies the Plan Area as appropriate for urbanization following adoption and implementation of a comprehensive Specific Plan. The Plan Area is currently designated “Urban” on the “Generalized Land Use Diagram” within the *Placer County General Plan* and as the “West Placer Specific Plan Area” in the *Dry Creek/West Placer Community Plan*. A list of development standards, referred to in the General Plan as “Exhibit 1,” is applicable to the Plan Area.

General Plan Exhibit 1 envisions the 5,230-acre Plan Area as a mixed-use community, including a maximum of 14,132 dwelling units. It also establishes 80 acres of commercial development, 160 acres of office and professional development, and 300 acres of professional/light industrial development as approximate acreages for a mix of acceptable non-residential land uses and the following plan features:

- A. An urban form design concept that provides for 2 mixed-use, pedestrian-oriented villages buffered with lower densities and a larger town center.
- B. The town center will operate as the institutional and social focal point of the community and provide a central location for public and institutional uses.
- C. The village core areas are mixed-use commercial areas with neighborhood commercial uses and high-density residential uses.
- D. Commercial areas within the town center and villages shall enhance outdoor public gathering areas.

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- E. Community open space should be contained in village greens and community parks that are connected with a greenbelt system.
- F. The town center and village core areas shall be planned and designed to be pedestrian, bicycle, and transit-oriented.
- G. Commercial buildings with the ground floors oriented to pedestrians; street-level windows, numerous entries, arcades, porches, and balconies; retail shops and display areas; street orientation with parking to the rear; and varied building facades and covered walkways.
- H. A mix of residential types with densities radiating outward from the village core areas.
- I. Existing and proposed stream and riparian areas, utility easements, and other such features should be incorporated into the open space corridors for the community.
- J. Roadway corridors shall be designed as landscape corridors, including separate bicycle and pedestrian facilities.

### 1.6.2 ENVIRONMENTAL IMPACT REPORT (EIR)

In conjunction with the original Specific Plan, an Environmental Impact Report (EIR) was prepared to study the environmental impacts the project may create. Elements analyzed in the EIR included project characteristics; the regulatory setting; population, employment and housing; visual quality and aesthetics; water quality; biological resources; geology and soils; archaeology; transportation/circulation; air quality; noise; public services and infrastructure; hazards; and plan alternatives. The purpose, policy, and detailed requirements of this Specific Plan will implement the mitigation measures contained in the EIR.

On July 16, 2007, the Placer County Board of Supervisors certified as adequate and complete the Final Environmental Impact Report (EIR) for the Specific Plan in accordance with the California Environmental Quality Act (CEQA). Mitigation Measures and a Mitigation Monitoring and Reporting Program were also adopted by the Board on July 16, 2007. On February 14, 2012, the Placer County Board of Supervisors adopted an Addendum to the certified EIR, in accordance with CEQA. On September 11, 2012 the Board adopted modifications to the approved Mitigation Monitoring and Reporting Program as well as corresponding text revisions to the Certified Environmental Impact Report. On \_\_\_\_\_ the Board adopted an addendum to the EIR, in accordance with CEQA, in connection with its approval of this Amendment of the Specific Plan.

### 1.6.3 ZONING

The Board's adoption of Ordinance No. 5476-B zoned the Plan Area "Specific Plan District" (SPL). The SPL zone, when combined with the Specific Plan, functions as the zoning text and map for the Plan Area (with the exception of the SPA). Zoning in the Plan Area and in the SPA is described in more detail in Chapter IX, "Implementation."

#### Relationship to County Zoning

The Specific Plan text and exhibits and the "Land Use and Development Standards" presented in Appendix A establish the zoning, land use, and development pattern for the Plan Area. The standards in this Specific Plan amend and supersede the standards and land use designations found in the *Placer County Zoning Ordinance*, Chapter 17 of the *Placer County Code*. Standards or regulations that are not specified in this Specific Plan shall default to the *Placer County Zoning Ordinance* for the applicable provisions.

As an example, definitions for various land uses identified in the *Placer County Zoning Ordinance* shall be used as the basis for interpreting the land uses allowed in the Specific Plan. However, this Specific Plan may modify certain definitions or define additional land uses not found in the Zoning Ordinance. Modifications or amendments made to the Specific Plan and/or Land Use and Development Standards shall follow Placer County procedures for amendments as defined in Chapter IX, "Implementation."

### 1.6.4 PUBLIC FACILITIES FINANCING PLAN (PFFP)

The County Board of Supervisors accepted the *Placer Vineyards Public Facilities Financing Plan* (PFFP) in 2007. The Financing Plan identifies the funding mechanisms required for the capital costs of all public facilities infrastructure necessary to accomplish Specific Plan build-out. A revised PFFP was accepted by the Board of Supervisors on \_\_\_\_\_

### 1.6.5 DEVELOPMENT AGREEMENT

Pursuant to Section 17.58.210 of the *Placer County Zoning Ordinance*, the County entered into a Development Agreement with individual property owners within the Placer Vineyards Development Group in 2007 (Ordinance No. 5477-B). The original Development Agreement was superseded by the Board adopted Amended and Restated Development Agreement dated February 14, 2012 and the First Amendment to Amended and Restated Development Agreement, dated September 11, 2012. On \_\_\_\_\_, the Second Amended and Restated Development Agreement, relative to the Placer Vineyard Specific Plan, was approved by the Board of Supervisors. The Second Amended and Restated

Development Agreement replaces and supersedes all previous Development Agreements. The Development Agreement sets forth the property owner's obligations related to the construction and financing of infrastructure, County facilities and public services, including financial contributions for public infrastructure and facilities maintenance, provision of urban services for the Plan Area, and other obligations that may be imposed by the County as a condition for development. The Development Agreement also provides property owners with certain vested development rights, subject to the conditions for development in the Development Agreement

### 1.6.6 DESIGN/SITE REVIEW

The purpose of the County's Design/Site Review process is to ensure that the design of buildings constructed in the Specific Plan area is of high quality and to prevent new construction from adversely affecting the residential and business desirability of the immediate and nearby neighboring areas. All development within the Plan Area will be subject to Design/Site Review.

## 1.7 SPECIFIC PLAN ORGANIZATION

### 1.7.1 CHAPTER STRUCTURE AND FORMAT

Generally, each of the Placer Vineyards Specific Plan chapters begins with a chapter overview or land use concept discussion, followed by project background information and references to other related documents pertinent to the chapter. The Specific Plan includes goals, policies, standards, and design guidelines. The following definitions describe the nature of the statements of goals, policies, standards, and design guidelines and the format in which these are used in this Specific Plan.

**Goals:** Goals are objectives or purpose statements written in a way that is general in nature and immeasurable. Goals are called out in bold type in sans serif font and are identified by chapter number, followed by the goal number, as follows: **Goal 3.X.**

**Policies:** Policies are specific statements in text or a diagram guiding and implying a commitment to action. Policy statements are requirements and use "shall," "must" or "will." Policies are indicated in bold, italicized type followed by a subject heading and identified by the chapter number and policy number, as follows: ***Policy 3.x <Subject Heading>***.

**Standards:** Standards are specific, sometimes quantified regulations used to guide development of the Specific Plan. Development standards will serve to supplement the *Placer County Zoning Ordinance*. Development standards for Placer

Vineyards are provided in a separate document (Appendix A) accompanying the Specific Plan. Statement of standards in the Specific Plan use "shall," "must," or "will."

**Guidelines:** Guidelines are qualitative and provide a design intent through descriptions and illustrations. Guidelines allow for flexibility and interpretation so long as the intent of the guidelines is upheld. Guideline statements use words like "should," "may," "encouraged," and "discouraged." Guidelines are not numbered. They are identified in a sans serif font and indented a quarter inch from the body of the text, as follows:

**Design Guidelines for <Subject Heading>**

### 1.7.2 SPECIFIC PLAN CHAPTERS

The Specific Plan consists of 9 primary chapters and four supporting appendices as described below:

#### Chapter I: Introduction

Presents the purpose, intent, and project context of the Specific Plan;

#### Chapter II: Vision

Presents the project vision, guiding design, and planning principles for Placer Vineyards and their specific application to the Placer Vineyards context;

#### Chapter III: Land Use

Sets the framework for the development of Placer Vineyards, providing information on land use regulations, allowable land use types, development standards, and the intensity and density of development;

#### Chapter IV: Environmental Resources

Addresses the environmental conditions and sensitive resources on the site, including wetlands, special status species, oak and riparian woodlands, water quality and conservation, energy conservation, air quality, and noise;

#### Chapter V: Transportation and Circulation

Describes the roadways, trails, and transit system and provides standards and guidelines for their design within the Placer Vineyards site;

#### Chapter VI: Community Design

Sets the design standards and guidelines for the elements that make up and will give identity to the Placer Vineyards community, including its street and landscape corridors; gateways, signage, and street lighting; activity centers; and neighborhoods;

## INTRODUCTION

### **Chapter VII: Parks and Open Space**

Describes the parks, open space, and recreation system designed for the Placer Vineyards community;

### **Chapter VIII: Public Utilities and Services**

Describes the backbone infrastructure systems (water, wastewater, recycled water, drainage, solid waste disposal, electricity and natural gas, telephone, and gas services) and community service facilities (schools, county services, public safety, and other community facilities) necessary to serve the Plan Area; and

### **Chapter IX: Implementation**

Outlines the administration and implementation steps required to achieve the goals and regulations set forth in the Specific Plan.

### **Appendices:**

The Appendices contain references that support and supplement information in the Specific Plan. They include:

- ◆ Appendix A, “Land Use and Development Standards” (Provided under a separate cover);
- ◆ Appendix B, “Recommended Plant List;”
- ◆ Appendix C, “Traffic Intersection Designs;” and
- ◆ Appendix D, “Mitigation Monitoring and Reporting Program” (Provided under a separate cover).

## CHAPTER II: VISION



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## 2.1 OVERVIEW

The *Placer Vineyards Specific Plan* will direct the construction of a comprehensively planned new community in southwestern Placer County. This community will connect to emerging developments in the City of Roseville and Sacramento County and will help define an urban character for this area. The individual elements and requirements of this plan all serve to fulfill this vision. The regional vision for Placer Vineyards is identified in this chapter, while the details for how this vision shall be carried forward are contained in the chapters of the Specific Plan that follow.

## 2.2 PLACER VINEYARDS PLACE WITHIN THE REGIONAL PLANNING CONTEXT

Placer Vineyards is identified in the *Dry Creek/West Placer Community Plan*, the *Placer County General Plan*, and the Sacramento Area Council of Governments' (SACOG's) adopted *Preferred Blueprint Scenario* (the SACOG Plan) as a new compact, self-sufficient community. The SACOG Plan is a regional vision to accommodate the longer term growth needs of the Sacramento region, as the region's current population of 2 million is forecasted to grow to more than 3.8 million people over the next 50 years. The SACOG Plan has been prepared as an example of how land use and transportation choices might be integrated into the region. In the SACOG Plan, Placer Vineyards is one of the largest areas targeted for new urban growth in southwest Placer County. The SACOG Plan suggests that over 21,000+ new homes might be constructed in the Plan Area.

Placer Vineyards is planned to permanently preserve on-site and off-site natural resources and open space by creating a higher intensity urban core, connected to transit, and providing a broad range of housing choices, affordable to diverse income populations. This Specific Plan includes the construction of 14,132 homes to accommodate all income levels and household sizes. Placer Vineyards is near existing employment centers in Roseville and Rocklin and emerging employment centers at the Sacramento International Airport and McClellan Park. The Placer Vineyards plan will provide direct access to regional transportation systems and link this area to major employment centers in the region. The Placer Vineyards Plan shares the SACOG and local planning visions to link land use and transportation systems to connect and serve the region.

## 2.3 THE PLACER VINEYARDS PLAN

Envisioned as a compact, self-sufficient community, Placer Vineyards is comprehensively planned to include a mix of higher density residential neighborhoods, a central mixed-use town center, 2 mixed-use village centers, a

commercial and employment corridor, parks, recreation facilities, schools, religious facilities, and a network of open space and protected riparian corridors.

Placer Vineyards is a place where the natural and urban landscapes are woven together to create a distinct community.

A network of shaded paths and trails weaves throughout the community along drainage corridors to connect residential neighborhoods to natural resources, community recreation facilities, schools, and community centers.



As a transit-oriented community, Placer Vineyards will include a transportation network of roadways and bicycle and pedestrian trails that provide access throughout the community, to the larger regional trail system, and to the regional bus rapid transit system on Watt Avenue. Smaller buses traverse throughout the community to connect residents to the town center, village centers, and community facilities.

The town center at Placer Vineyards, located at the heart of the community, is intended to provide a mix of retail shops, services, community facilities, and entertainment activities. The town center will serve as the cultural and civic focal point for public life. Buildings with wide, shaded retail streets are proposed to be clustered around a central town green. Residents and visitors casually stroll, shop, and eat at outdoor cafes, restaurants, and coffee shops. The town center will include a community library, post office, a small government center and a sheriff substation.

2 small village centers serve the surrounding neighborhoods: one on the east side and a second on the west side of Placer Vineyards. The village centers will be the focal points of the surrounding residential life. Designed as small, mixed-use nodes of activity, each village center provides wide, shaded pedestrian-friendly streets, and locally serving shops, services, offices, and residential uses. A small public plaza with street furniture and shade trees is proposed to create an inviting place for residents to enjoy outdoor public activities and social gatherings.

## VISION

A regional commercial corridor on Base Line Road that includes a strategically located power center at the southwest corner of Watt Avenue and Base Line Road will provide regional services, employment, and a gathering place for the surrounding area communities. Neighborhoods in Placer Vineyards are intentionally designed to be open, accessible, and inviting, offering a range of housing choices, types, styles, and densities for all income levels. Centrally located schools and parks will serve as the focal point for each neighborhood with safe, shaded, tree-lined walkways and paths radiating to surrounding residences.

2 large community parks provide a range of organized recreational activities for Placer Vineyards. Located adjacent to the open space and community trail network, residents will have easy and safe access (to walk, bike, and jog) between parks and community facilities throughout the community. Additionally, a multi-purpose Community Center may be located in the West Community Park. 2 oak grove natural areas, the Dry Creek Corridor, and adjoining Gibson Ranch Park, provide a natural refuge from the urban fabric.

### 2.4 PROJECT SMART GROWTH PRINCIPLES

Placer Vineyards is planned to create an urban development pattern with diverse transportation and housing options affordable to a broad range of income groups while permanently preserving on-site and off-site natural resources and open space. Rooted in an existing regional community context, the planning and design vision for Placer Vineyards is grounded in the following principles of Smart Growth:

- ◆ Protect and enhance the highest quality natural features and resources on the site by incorporating native oaks, wetlands, creeks, and drainage systems into a community-wide open space system; also provide transitional buffers sensitive to the character of adjacent land uses.
- ◆ Promote a mixed-use, compact development pattern linked to regional transportation systems.
- ◆ Create a livable, pedestrian-friendly, and safe environment with a distinct community identity and sense of place.
- ◆ Provide a diversity of housing choices—types, styles, densities, and costs.
- ◆ Provide a range of transportation choices—streets, paths, and trails—with links to local and regional transportation systems.
- ◆ Promote a balance of uses—housing, employment, schools, parks, recreation, and places to shop—that supports a balance of jobs to housing in the region.
- ◆ Create safe and accessible links between neighborhoods, centers, and districts within the Plan Area and to the surrounding community.

### 2.5 PLANNING AND DESIGN PRINCIPLES

The planning and design principles that follow provide the overarching themes that establish the vision and physical framework for the Placer Vineyards plan. These concepts are organized as community-wide elements and places of activity intended to implement the above-stated Smart Growth and regional planning principles while recognizing the unique site and market conditions specific to Placer Vineyards. These concepts, as they apply to the community, are depicted graphically on the following pages.

## COMMUNITY-WIDE ELEMENTS

### A. Natural Resources and Public Open Space

The natural resources and open space vision encourages wildlife and plant habitat preservation; promotes energy efficient design; water conservation and stormwater management; and incorporates public open space (parks, town and village greens, trails, and greenbelts) for public recreation with the development.

#### 1. Natural Resources

The natural landscape consists of existing trees, wetlands, wildlife corridors, creeks, ponds, and other geological features on the site. Open space in Placer Vineyards is designed to permanently protect the significant on-site riparian corridors, wetlands, and oak tree groves characteristic to the site, such as Dry Creek. Dry Creek is a rich, natural open space resource that provides a green edge to the southeast corner of the community. As an element of Specific Plan implementation, Placer Vineyards will also provide off-site wetland mitigation, preserving large off-site open space preserves, located away from urban edge conditions.



#### 2. Public Open Space Network

The open space network describes the system of parks, recreational areas, on-site drainage ways, and storm retention areas that are connected by a network of trails and paths. The open space system integrates existing wetlands, drainageways, spaces under power line easements, significant on-site oak groves, and the Dry Creek riparian corridor into an enhanced system of on-site drainage, storm retention, and recreational areas. This community-wide open space greenway system generally runs east-west, linking neighborhoods and special districts together throughout the community.



VISION

Figure 2.1 - View of Open Space Corridor

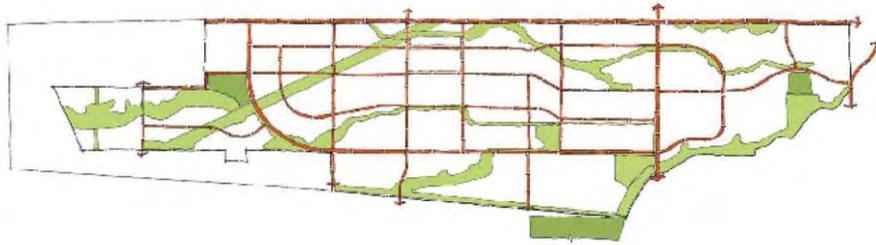


## B. Transportation Choices

Development designed around Smart Growth principles provide opportunities that encourage people to choose other modes of transportation—to sometimes walk, ride bicycles, ride the bus or light rail (if available), and to carpool.

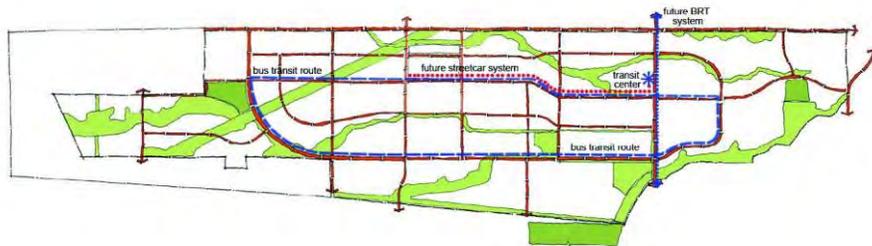
### 3. Grid of Streets

In Placer Vineyards, a network of major roadways, arranged as a grid of streets, provides efficient access and circulation for residents, shoppers, and workers. The north-south street system and circulation connections discontinue and terminate at major arterial roads, at parks, or at open space corridors. Roadways are designed to provide safe connections shared by cars, buses, cyclists, and pedestrians.



### 4. Transit Linkages

Alternative modes of transportation are integrated into the circulation system. The Specific Plan includes the provision of right-of-way for a future regional bus rapid transit system along Watt Avenue. East Town Center Drive will reserve right-of-way for a future streetcar system internal to Placer Vineyards. Routes for smaller buses will traverse the community to connect residents to the town center, village centers, and community facilities.



### 5. Connecting Trails and Paths

Pedestrian and bike paths and routes are integrated into the community-wide open space and street system. The roadways, greenways, and trails run parallel to each other and connect the major centers of the community.



### 6. Designed Landscapes

The designed landscape defines the form and character of the public realm: streetscapes along street corridors, gateways, entries, and landscape buffers and public spaces. The landscape design of the public realm provides a continuous visual thread that ties the community together under a common “green” framework, establishing the Placer Vineyards identity and community character.

VISION

Figure 2.2 - View of Dyer Lane Greenway through Oak Grove Corridor



## CENTERS OF ACTIVITY

### C. Quality Community Planning and Design

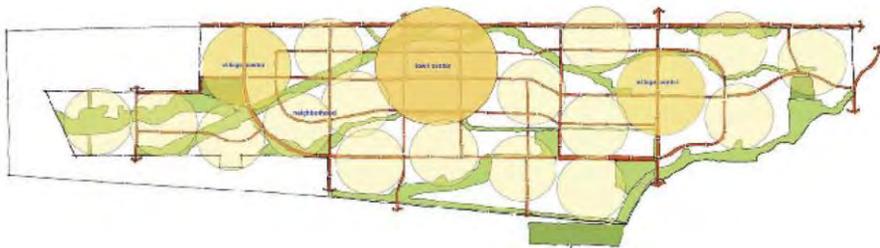
The Smart Growth principles of mixed-use/compact development, jobs/housing balance, and livability are addressed through the nature and design of the land use plan. Homes, shops, offices and entertainment places built near each other can create active, vital neighborhoods.

Project examples in Placer Vineyards include housing located near regional employment centers, small shopping centers in residential neighborhoods, and buildings with ground-floor retail and apartments or condominiums on the upper floors. These projects function as local activity centers, contributing to a sense of community, where people interact more with each other and tend to walk, bike, or ride the bus.

#### 7. Town and Village Centers

A series of strategically-located urban centers—the town center, east village center, and west village center—are conveniently located to all neighborhoods in the Plan Area. Adjacent to higher density residential neighborhoods, the centrally located town center provides a mix of uses serving both regional and local market demands.

The 2 village centers are smaller local commercial nodes of activity containing a diverse mix of uses, housing types and densities, commercial and employment uses, public and quasi-public uses, and open space uses within a short walking distance of surrounding neighborhoods.



#### 8. Regional Commercial Corridor

Strategically located along Base Line Road and the Watt Avenue bus rapid transit corridor, a regional commercial corridor links the Placer Vineyards community to the larger regional community. These commercial centers, including a power center, a range of retail, office, and business park uses along Base Line Road, are intended to be major centers of employment, services, and retail shopping for the region.

#### 9. Distinct Neighborhoods Anchored by Public Spaces

The Smart Growth principles of livable, pedestrian-friendly neighborhoods, housing choice, and diversity are guiding visions in the design of the Placer Vineyards plan. Providing a variety of places where people can choose to live—apartments, townhomes, and single-family detached homes on a variety of lot sizes—creates opportunities to meet the needs of diverse household sizes (i.e. single families, single or 2-person households, seniors, and people with special needs).

Placer Vineyards is organized as an assembly of distinct neighborhoods, each designed with its own character and unique sense of place. A range of housing types and densities within each residential neighborhood provide housing choices and flexibility to meet market demands. Neighborhoods are anchored with community-serving facilities, such as schools, parks, religious facilities, and community centers.



VISION

Figure 2.3 - View of Townhomes along a Residential Street



CHAPTER III: LAND USE



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This “Land Use” chapter sets forth the overall framework for the development and preservation of the Placer Vineyards Plan Area (or Plan Area). This chapter establishes the overall goals, policies, and development standards applicable to the planned land use activities within the Plan Area. It includes the types and intensities of uses for land and buildings, and the overall forms that are desired for development sites and buildings. Figure 3.1, the “Land Use Diagram” illustrates the physical pattern of development permitted in the Plan Area.

This chapter is to be used in conjunction with the other chapters of the Specific Plan. More detailed discussion of environmental resource management is included in Chapter IV. Information on transportation and circulation systems is contained in Chapter V. Community design issues are addressed in Chapter VI, and details on public facilities are contained in Chapter VII. The “Land Use and Development Standards” presented in Appendix A establish the permitted uses and Development Standards for the various land use designations in the Plan Area. These standards amend and supersede Chapter 17 of the *Placer County Zoning Ordinance*. However, standards or regulations that are not specified in this Specific Plan shall default to the *Placer County Zoning Ordinance* for applicable provisions.

### 3.1 LAND USE CONCEPT

#### CONTEXT

Placer Vineyards represents one of the largest areas for urban growth in southwest Placer County. The intent of the plan is to accommodate the long-term growth needs of the County while establishing a concentrated, compact development pattern in the region with a balance of employment, residential, shopping, and recreational uses. The result will be a comprehensively planned community with a range of new housing neighborhoods, employment centers, shopping centers, parks, recreational facilities, and schools.

#### LAND USE DIAGRAM

The Land Use Diagram illustrates the underlying framework for the arrangement of land uses, transportation networks, and open spaces within the Plan Area (see Figure 3.1). Tables 3-1 and 3-2 provide a description of the types of uses permitted within each land use category as shown in the Land Use Diagram.

At plan build-out, the Plan Area will accommodate an estimated population of 32,814 people (including the SPA). It is also planned to accommodate up to 14,132 new homes, approximately 107 acres of office development, 166 acres of retail development, approximately 842.8 acres of parks and open space, 6 elementary schools, 2 middle schools, and 1 high school. Table 3.2, “The Land Use Summary Table” establishes the mix of uses in the Plan Area including commercial, employment, civic, and open space components. Refer also to Table 3.4 for the “Population and Housing Summary” and Table 3.6 for the “Employment Summary.”



The land use concept is based on the overall vision and Smart Growth development principles discussed in Chapter II, “Vision.” These planning principles are summarized by the goals and policies that follow.

### 3.2 LAND USE PLAN GOALS AND POLICIES

#### GENERAL LAND USE GOALS

- Goal 3.1** Develop an urban pattern that is consistent with the goals and objectives of the *Placer County General Plan* while also taking into consideration the SACOG Blueprint vision of growth in the region.
- Goal 3.2** Develop Placer Vineyards in a pattern that sets a new urban development standard for the area, while incorporating existing and planned transportation systems.
- Goal 3.3** Organize land use types and patterns that are sensitive to the existing topographic features and other physical constraints of the site.
- Goal 3.4** Minimize conflicts between urban development and existing and interim agricultural activities.

## LAND USE

### GENERAL LAND USE POLICIES

#### **Policy 3.1 Urban/Rural Transitions.**

*The Placer Vineyards Specific Plan Area shall provide buffers to existing rural and agricultural development located outside of its project boundaries, including the Special Planning Area, for as long as these rural/agricultural uses persist. Buffers may include streets, landscaping, and open space areas. Buffers provided next to the Special Planning Area are identified in Figure 7.10, “Special Planning Area Open Space Buffer and Trails Diagram.”*

#### **Policy 3.2 Agricultural Uses within the Specific Plan Area.**

*Agricultural use of the land within the Specific Plan urban area boundary may continue until such time as development pursuant to this Specific Plan is implemented. In the absence of a health and safety issue, urban developments shall not be required to provide buffers for adjoining agricultural uses located in the urban area boundary. Placer County will review and analyze development proposals for potential conflicts between proposed land uses and adjacent agricultural activities. Urban developments abutting continuing agricultural lands will be required, as a condition of tentative map approval, to provide home buyers with a document informing them that the adjacent land is in agriculture and that the agriculturalist has the right to continue operations under the County’s Right-to-Farm ordinance.*

#### **Policy 3.3 Timing of Development.**

*The construction of a Backbone Infrastructure system will permit development to proceed within the Plan Area, as the availability of services and infrastructure allow.*

### NATURAL RESOURCES GOALS

**Goal 3.5** Encourage a pattern of development that conserves and incorporates the natural resources of the site in a manner that enhances the quality of life within the urban community.

**Goal 3.6** Protect and conserve the Dry Creek riparian corridor and significant oak groves located throughout the site and compensate for development impacts with permanent off-site preservation of natural resources, open space, and farmland soils.

### NATURAL RESOURCES POLICIES

#### **Policy 3.4 Natural Resource Management.**

1. *The following natural features of the site are protected in open space areas and serve as the framework for the system of drainage corridors and greenways throughout the Plan Area:*
  - a. *Environmentally sensitive areas, such as significant wetlands and streams, shall be protected in open space with landscape buffers.*
  - b. *The existing oak tree groves on the site shall be preserved within oak parks while the existing oak trees along Dyer Lane shall be preserved within the landscape corridors.*
  - c. *The Dry Creek riparian corridor shall be a protected natural resource area, shared between the existing plant and wildlife communities and future residents.*
2. *The California Department of Conservation Farmland Mapping and Monitoring Program identifies much of the Plan Area as “important farmland.” Specific Plan policies 4.34 and 4.35 describe the requirements for preserving open space land elsewhere in Placer County to offset open space land impacted within the Specific Plan area.*

### HOUSING GOALS

**Goal 3.7** Provide an adequate supply of residential land, including opportunities for affordable housing, in a range of densities and housing types.

### HOUSING POLICIES

#### **Policy 3.5 Location and Density Range**

*The total of all residential units within the plan shall not exceed 14,132 units. Residential uses shall be located in areas designated in the Land Use Diagram and within the density ranges provided in Table 3-2, “Land Use Summary Table”.*

#### **Policy 3.6 Mix of Densities and Variety**

*Subject to the applicable density range, a variety of housing types and densities will be provided.*

**Policy 3.7 Secondary Dwelling Units.**

Secondary dwellings shall be counted as a residential unit for the purposes of calculating the total number of units allowed for a property under Table 3-3. Lots where secondary dwellings are proposed to be constructed shall be identified as part of any application for the tentative subdivision map for the property and shall comply with the following standards:

1. Design and Development Standards:

- a. *General Requirements:* Construction associated with a secondary dwelling shall conform to the height, setback, Design/Site Review, fees, charges, and other requirements generally applicable to a primary dwelling unit located within the same land use designation where the property is located.
- b. *Minimum Lot Area:* 5,000 square feet
- c. *Maximum Lot Coverage:* The maximum combined building coverage for the primary dwelling unit and secondary dwelling shall not exceed 50 percent.
- d. *Maximum Floor Area:* The floor area allowed for a secondary dwelling shall not exceed 40% of the primary dwelling unit's living area and shall not exceed 1,200 square feet.
- e. *Setbacks:* Secondary dwellings shall maintain the setbacks required in the land use designation in which it is located. In addition, a minimum of 10 feet shall be maintained between the primary dwelling and a detached secondary dwelling.
- f. *Outdoor Covered Areas:* Covered porches, decks, landing places and similar architectural features may be added to a secondary dwelling structure provided that any such covered feature is open on at least two sides and occupies an area no larger than 25% of the allowable living space of the secondary dwelling.
- g. *Appearance:* Secondary dwellings shall be architecturally compatible with the primary dwelling unit and shall be subordinate to the primary residence.
- h. *Parking Requirement:* Parking required is in addition to the parking that is required for the primary dwelling unit. Each secondary dwelling shall provide at least 1 off-street parking space. Parking may be located in tandem with other on-site parking.

i. *Deed Restriction:* Prior to issuance of a building permit for the secondary dwelling, the owner shall record a deed restriction which addresses the restrictions on such units contained herein.

- 2. *Fees:* Each secondary dwelling shall be subject to payment of all fees payable by the primary dwelling unit, including those required under the applicable provisions of the Development Agreement.

**Policy 3.8 Active Adult Community.**

The eastern portion of the Plan Area (designated under property 1A on Figure 3.3, "Land Use Ownership Diagram") shall be reserved as a residential community for active adults (55+ years and older).

**COMMERCIAL GOALS**

- Goal 3.8** Create a mixed-use town center, located centrally in the community, to serve as a gathering place that provides a range of public and private activities for residents, local workers, and visitors.
- Goal 3.9** Create two distinct village centers, geographically dispersed at the west and east ends of the community, with a diverse mix of land use activities that serve the surrounding population.
- Goal 3.10** Provide a range of commercial uses along Base Line Road, including a power center, to serve as major centers of employment and retail shopping for the region that contributes to the County's economic and fiscal soundness.

**COMMERCIAL POLICIES**

**Policy 3.9 Commercial Services.**

A hierarchy of regional and local commercial areas shall provide a range of products and services to the Plan Area.

- 1. The regional commercial centers are highly visible shopping areas and offices concentrated at high-traffic intersections along Base Line Road. The power center provides for large-scale volume purchases not generally available in smaller shopping centers. Together, these centers will serve the needs of the larger region; they will not compete directly with the retail and services provided in the Placer Vineyards town center, village centers, or convenience centers.

## LAND USE

- 2. The town center shall be located in the geographic center of the community, just south of Base Line Road, along Town Center Dr., in between 14th St. and 16th St. The town center is an easily accessible, pedestrian-friendly, mixed-use center with specialty retail shops and restaurants located on the ground floor for outside dining potential. Housing and offices may be located on the floors above. The town center is also a community focal point, providing locations for public and civic services such as a library, religious facility, and civic offices as well as recreational opportunities such as parks, open markets, and theaters.*
- 3. 2 village centers serve the east and west communities of the Plan Area. Like the town center, they are easily accessible to the community, but are smaller mixed-use centers, providing retail and more localized community shopping and services. The east village center will provide for bus rapid transit access, a transit terminal, a fire station location, and ground-floor commercial uses surrounded by high density housing. The west village center provides commercial uses in proximity to a community park and surrounding residential neighborhoods.*
- 4. Neighborhood centers provide convenience-type retail uses within local residential neighborhoods and are dispersed throughout the community within the Commercial/Mixed-Use (C/MU) land use categories.*

## EMPLOYMENT GOALS

- Goal 3.11** Further the County's goals for growth management, economic development, and community character by designating land uses that facilitate and encourage the creation of high-quality employment centers along Base Line Road.
- Goal 3.12** Help to achieve a balance of jobs and housing within the region, thereby minimizing the air pollution impacts caused by long commute trips.

## EMPLOYMENT POLICIES

### ***Policy 3.10 Employment.***

*Higher intensity employment uses shall be concentrated along Base Line Road and secondarily in the town center and along Watt Avenue.*

## PARKS AND OPEN SPACE GOALS

- Goal 3.13** Create an interconnected parks and open space system within Placer Vineyards that provides for the preservation and enhancement of natural resources and offers a variety of recreational opportunities for the community.
- Goal 3.14** Provide parks sized for a variety of recreational uses.
- Goal 3.15** Provide schools that are within a safe, convenient walking distance of residential neighborhoods as an element of the open space fabric in the community.

## PARKS AND OPEN SPACE POLICIES

### ***Policy 3.11 Parks.***

*A variety of parks shall be distributed throughout the Plan Area and shall provide recreational opportunities for residents of all ages and abilities.*

- 1. 8 neighborhood parks shall be located adjacent to school facilities to allow opportunities for shared use and maintenance of public facilities. (See Chapter VII, "Parks and Open Space," for additional park policies and guidelines, and Chapter VIII, "Public Utilities and Service," for additional school policies and guidelines. See the Development Agreement for the timing and details related to the construction of parks.)*
- 2. Parks are distributed throughout the Plan Area to provide general parity or access to all residents.*
- 3. Neighborhood developments are encouraged to provide landscape features and lot lots.*
- 4. Parks and park credits shall be provided at a minimum standard of 5 acres per 1,000 residents.*

## RELIGIOUS FACILITIES GOALS

- Goal 3.16** Encourage the establishment of religious facility sites to serve the diverse cultural and religious needs of the community.

**RELIGIOUS FACILITIES GOALS**

***Policy 3.12 Religious Facilities Location.***

*Figure 3.1, the “Land Use Diagram” indicates, at a minimum, the number of Religious Sites within the Plan Area. Additional religious and institutional uses may be provided elsewhere in the Plan Area, subject to County review and approval.*

**3.3 LAND USE DESIGNATIONS**

The following section describes the intent of each of the Specific Plan land use categories and the types of land uses that are allowed in each category. The land use designations and the accompanying table of land uses provide the framework for future land use decisions within the Plan Area. The descriptions of the permitted land uses in the following pages are illustrative and do not include all permitted allowable uses. Refer to Appendix A for the complete list of permitted uses allowed in the Plan Area.

***Policy 3.13 Land Use Designations.***

*The land use designations in the Specific Plan shall take precedence over existing comparable zoning designations in the Placer County Zoning Ordinance. Table 3-1 shows the relationship between Placer County zoning designations and Specific Plan land use designations (shown here for reference only). Permitted uses for each land use designation are included in Appendix A.*

**Table 3-1: Specific Plan Comparable County Zoning Designations**

Specific Plan Land Use Designations	Description	Comparable Placer County Zoning Ordinance Designations (See Section 17.06.050)
SPA	Special Planning Area	RA, C1, IN
LDR	Low Density Residential	RS
MDR	Medium Density Residential	RS, RM
HDR	High Density Residential	RM, RS
C/MU	Commercial/Mixed-Use	OP, CPD
COM/ TCC	Commercial/ Town Center Commercial	C1, C2, OP
BP	Business Park	BP, CPD, OP
PC	Power Center	BP, CPD, OP
O	Office	C1, C2, C3, CPD, HS, MT, OP, RES, AP, BP, IN, INP
REL	Religious Facility	RS, RM, RA, RF, C1, C2, C3, CPD, HS, MT, OP, RES, BP, IN, INP, F
PUB (C, CY, F, Gov, L, SS, T, RC)	Public Use	RS, RM, RA, RF, C1, C2, C3, CPD, HS, MT, OP, RES, AP, BP, IN, INP, F, FOR, O
ES, MS, HS	Schools	RS, RM, RA, RF, C1, C2, C3, CPD, MT, OP, RES, BP, F
OS	Open Space	O
P	Parks	RS, RM, RA, RF, C1, C2, C3, CPD, HS, MT, OP, RES, AP, BP, IN, INP, F, FOR, O

## LAND USE

### ***Policy 3.14 Land Use Designations to the Plan.***

*The development program (number of residential units in the Plan Area, permissible intensity of commercial development, and allocation of public uses for each parcel) is specified in Figure 3.1, the “Land Use Diagram” and Table 3-3, the “Land Use Ownership Summary.” The development program also allocates a specific allowable number of residential units and indicates the commercial development potential for each property of record in the Plan Area, as described in the following sections. Any subsequent development plan that exceeds the intensities assigned by the plan may be subject to the subsequent environmental review process. See Section 9.2.6, “Environmental Review,” in Chapter IX, “Implementation.”*

*The assigned residential intensities and total commercial acreages are listed in Table 3-3 and will also be identified in the Development Agreement for Placer Vineyards. Commercial intensities, defined by the floor area ratios (FAR), are provided under the commercial land use designation headings that follow and in Appendix A, “Land Use and Development Standards.” Likewise, allowable residential densities are identified under the residential land use designation headings that follow and are further defined and identified in the Development Standards in Appendix A.*

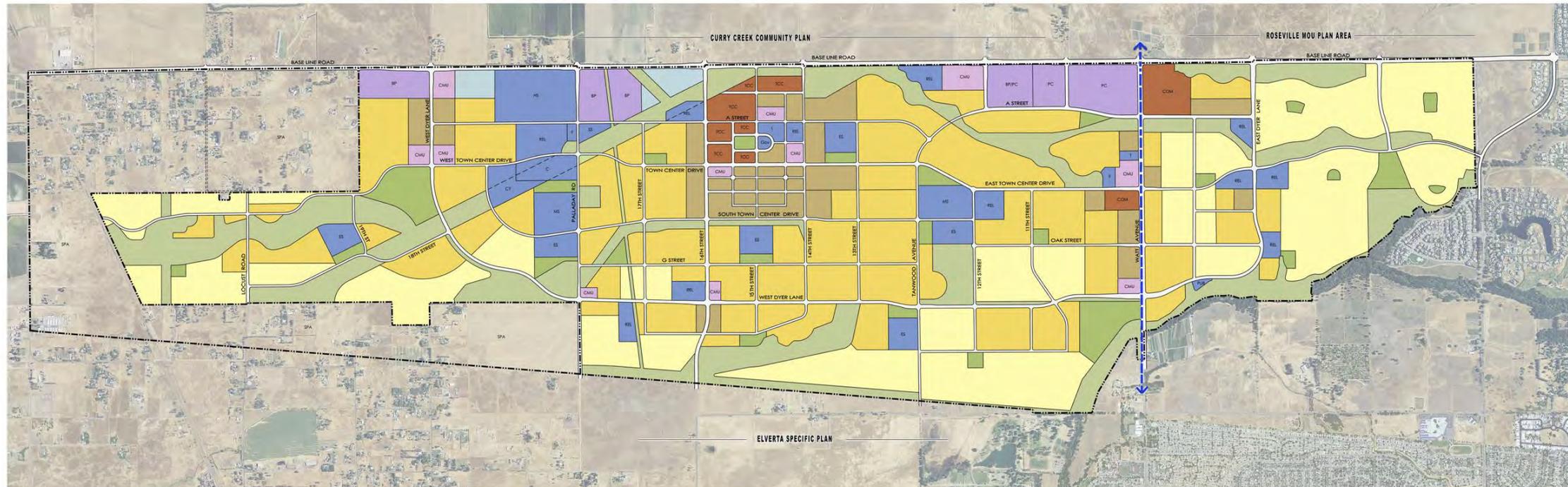
### ***Policy 3.15 Allowable Land Uses.***

*Permitted land uses and Development Standards for each land use designation are included as Appendix A of this Specific Plan. Appendix A identifies the uses allowed by right, uses allowed by Administrative Review Permit, uses allowed by Use Permit, and prohibited uses. The purpose of these permit requirements is to allow the County to evaluate proposed uses, to determine whether problems may occur, to ensure compatibility of adjoining uses, to provide the public the opportunity to review projects and identify issues and concerns in a public hearing (if determined to be necessary), to work with the applicants to adjust the project through conditions of approval, and to solve identified problems or to disapprove a project if problems cannot be adequately resolved.*

### ***Policy 3.16 Nuisance Uses.***

*Land uses that involve outdoor manufacturing or uses that may emit any visible gases, particulates, steam, heat, odor, vibration, glare, dust, or excessive noise from the exterior of a building are not allowed in the Plan Area. Such uses may include metal and plastic fabrication, heating and ventilation, dry cleaning, painting, and other similar activities that may be allowed if contained completely within the building interior.*

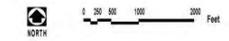
Figure 3.1 - Land Use Diagram



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**LEGEND**

<span style="background-color: #f08080; border: 1px solid black; padding: 2px;">C/MU</span> COMMERCIAL MIXED USE	<span style="background-color: #d3d3d3; border: 1px solid black; padding: 2px;">SPA</span> SPECIAL PLANNING AREA	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">ES</span> ELEMENTARY SCHOOL	<b>PUBLIC/ QUASI-PUBLIC USES</b>	<span style="background-color: #90ee90; border: 1px solid black; padding: 2px;">OS</span> OPEN SPACE
<span style="background-color: #ffa500; border: 1px solid black; padding: 2px;">COM</span> COMMERCIAL	<span style="background-color: #ffff00; border: 1px solid black; padding: 2px;">LDR</span> LOW DENSITY RESIDENTIAL	<span style="background-color: #6495ed; border: 1px solid black; padding: 2px;">MS</span> MIDDLE SCHOOL	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">REL</span> RELIGIOUS FACILITY	<span style="background-color: #90ee90; border: 1px solid black; padding: 2px;">P</span> PARK
<span style="background-color: #9370db; border: 1px solid black; padding: 2px;">PC</span> POWER CENTER	<span style="background-color: #ffa500; border: 1px solid black; padding: 2px;">MDR</span> MEDIUM DENSITY RESIDENTIAL	<span style="background-color: #6495ed; border: 1px solid black; padding: 2px;">HS</span> HIGH SCHOOL	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">F</span> FIRE	<span style="background-color: #90ee90; border: 1px solid black; padding: 2px;">RC</span> RECREATION CENTER
<span style="background-color: #800080; border: 1px solid black; padding: 2px;">BP</span> BUSINESS PARK	<span style="background-color: #808080; border: 1px solid black; padding: 2px;">HDR</span> HIGH DENSITY RESIDENTIAL		<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">Gov</span> GOVERNMENT	
<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">O</span> OFFICE			<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">L</span> LIBRARY	
			<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">Pa</span> POLICE	
			<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">CY</span> CORPORATE YARD	
			<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">SS</span> SUBSTATION	
			<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">T</span> TRANSIT	
			<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">C</span> CEMETERY	
				<span style="color: blue; font-size: 1.2em;">↔</span> BRT LINE



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Table 3-2: Land Use Summary

Land Use	Acres	% of Total Area	Units <sup>4</sup>	% Unit Mix
<b>Residential</b>				
Special Planning Area (SPA)	979.0	18.7%	411	2.9%
Low Density Residential (LDR)	1,022.7	19.6%	3,519	24.9%
Medium Density Residential (MDR)	1,214.0	23.2%	6,474	45.8%
High Density Residential (HDR)	221.5	4.2%	3,092	21.9%
<b>Commercial</b>				
Commercial/Mixed Use (C/MU) <sup>1</sup>	50.5	1.0%	636	4.5%
Commercial (COM)	34.0	0.7%		
Town Center Commercial (TCC)	42.5	0.8%		
Business Park (BP)	59.5	1.1%		
Power Center (PC)	60.0	1.1%		
Business Park (BP/PC) <sup>2</sup>	31.0	0.6%		
Office (O)	32.5	0.6%		
<b>Public/Quasi-Public</b>				
Public Use (CEM, CY, F, Gov, L, PO, SS, T) <sup>5</sup>	50.5	1.0%		
Schools (ES/MS/HS)	167.0	3.2%		
Religious Facilities (REL)	91.0	1.7%		
<b>Parks &amp; Open Space</b>				
Open Space (OS)	692.8	13.2%		
Parks (P) <sup>3&amp;6</sup>	150.0	2.9%		
Arterial and Collector Roads	331.5	6.3%		
<b>Total</b>	<b>5,230.0</b>	<b>100.0%</b>	<b>14,132</b>	<b>100.00%</b>

Notes:

1. Residential units in CMU acreage are devoted to housing at the density range indicated.
2. 31 acres may be a BP or PC land use (see Figure 3.1, "Land Use Diagram," for site location).
3. The 22 acres of parks in the active adult community (Property #1A) are private parks (park credit calculated at 50% or 11 acres).
4. Refer to Table 3-3 for the allocation of units to individual properties.
5. 2 acres of park maintenance facility given credit toward Community Park acreage located in CY zoning.
6. Parks credit equals 159 acres (150 acres [P zone] - 11 acres [50% reduced credit for private parks] + 2 acres [CY zone] + 18 acres [In-Lieu Fees] = 159 acres credit).

## LAND USE

### RESIDENTIAL USES:

#### Low Density Residential (2 to 6 du/ac)

The Low Density Residential (LDR) areas are intended for single-family detached homes on lots greater than 4,000 square feet and similar and compatible uses. Refer also to the housing type examples in Appendix A under the LDR land use designation. Special housing types for elderly, active-adult, and community living are also allowed. The LDR neighborhoods also permit public and quasi-public uses such as schools, parks, and recreation facilities with appropriate buffers and access to major local roadways and collector streets. Agricultural use of the land may continue as an interim use until development occurs, pursuant to this Specific Plan.

The Land Use Diagram designates 1,022.7 acres of LDR uses. This designation allows a density range of 2 to 6 dwelling units per acre (du/ac). Refer to Table 3-3 for the distribution of total units to individual properties of record and to Appendix A for Residential Development Standards.

#### Medium Density Residential (4 to 8 du/ac)

The Medium Density Residential (MDR) areas are intended for a range of housing types, such as standard-lot and small-lot single family detached homes and similar, compatible uses. Refer also to the housing type examples in Appendix A under the MDR land use designation.

The Land Use Diagram provides 1,214.0 acres of MDR uses. This designation allows a density range of 4 to 8 du/ac. Refer to Table 3-3 for the distribution of total units to individual properties of record and to Appendix A for Residential Development Standards.

#### High Density Residential (7 to 21 du/ac)

The High Density Residential (HDR) land use designation allows for a range of housing types, including small-lot single family detached homes, duplex, cluster housing or motor courts, attached and detached townhouses, condominiums, apartments, and similar, compatible uses. Refer to housing type examples in Appendix A under the HDR land use designation.

The Land Use Diagram indicates 221.5 acres of High Density Residential uses. This designation allows a density range of 7 to 21 du/ac. A Use Permit is not required for units in this category if the development is consistent with the requirements of this Specific Plan. However, Design/Site Review is required to determine design consistency. Refer to Table 3-3 for the distribution of total units to individual properties of record and to Appendix A for residential development standards.

Alternative housing designs are encouraged in all residential categories. The use of innovative subdivision designs, such as zero-lot-line patio homes, cluster home designs, garage units, and auto court arrangements or the use of duplex units that provide two single-family homes attached along a common wall, is encouraged. The number of units in the MDR and HDR categories should be maximized to the extent feasible to achieve a more transit-oriented development pattern.

### COMMERCIAL USES:

#### Commercial (FAR Range: 0.20 to 0.30)

The Commercial (COM) land use designation allows for a variety of retail uses and services. These include small convenience stores and centers, neighborhood-serving shopping centers, and community-scale retail centers. Typical COM land uses include neighborhood grocery stores, drugstores, and retail stores providing household goods and services for the surrounding residential neighborhoods. The COM designation would also allow for banks and financial institution offices, realty and insurance offices, medical offices and professional offices, and gas stations and auto repair uses (limited in extent and located where compatible with adjoining land uses and subject to approval of a Minor Use Permit). The COM designation also allows for public and quasi-public uses, parks, libraries and museums, public utility and safety facilities, and other similar, compatible uses. Refer to Appendix A for the complete list of permitted uses in the COM land use designation.

The range of floor area ratio (FAR) applicable to the COM designation is between 0.20 to 0.30. The intensity utilized for the purpose of distributing Commercial intensity to individual properties of record is 0.25. Refer to Appendix A for the applicable Commercial Development Standards.

#### Town Center

The purpose of the town center is to create a pedestrian-oriented, easily accessible, mixed-use retail core in the heart of the Placer Vineyards community. The town center is intended to be a highly visible, higher intensity, active, social, and cultural gathering place. The town center supports a mix of uses with office or residential uses located above ground-floor retail shops. Ground-floor retail uses with mid-rise buildings, placed at the back of sidewalks, open onto wide pedestrian sidewalks, allowing for outdoor dining and retail displays.

**Town Center Commercial (FAR Range: 0.35 to 2.0)**

Uses encouraged in the Town Center Commercial (TCC) designation include all types of office uses (e.g., banks and medical offices), a variety of retail stores and services (e.g., furniture stores, clothing and household goods, music stores and video outlets, hotels, motels, restaurants, and bars), a variety of entertainment uses (e.g., movie theaters and nightclubs), and public and quasi-public uses (e.g., community recreation center, library, fire station, sheriff's substation, and religious facility); along with public parks, an outdoor performance stage, and plazas. The TCC use may also include locally-serving retail uses (grocery store <25,000 square feet, drugstore, and other local services). Refer to Appendix A for a more specific list of permitted uses in the TCC land use designation. The town center shall be designed to encourage outdoor eating and dining along its sidewalks.

The TCC designation is assumed to provide 80 percent retail uses and 20 percent office uses. The development of residential uses within the TCC designation, however, is also encouraged.

The FAR applicable to the TCC designation ranges from 0.35 to 2.0. For the purposes of distributing development intensities, the Land Use Diagram assumes that approximately 80 percent of this area will be developed with retail uses at an FAR of 0.45. The remaining 20 percent is anticipated to develop with office uses with the same FAR of 0.45. Refer to the Development Standards in Appendix A for allowed Town Center Commercial uses and applicable Development Standards.

***Policy 3.17 Specialty Grocery Stores.***

*It is the intent of the Specific Plan to have the first specialty grocery store located in the town center, if feasible. The purpose of having the first specialty grocery store in the town center is to provide a catalyst that would assist in generating an intensity of commercial and pedestrian use in the heart of the Placer Vineyards community. To that end, the town center will have the exclusive rights for specialty grocery stores less than 25,000 square feet in size. Examples of specialty grocery stores are Trader Joe's, Whole Foods Market, or other similar type store concepts with a store size of less than 25,000 square feet.*

***Policy 3.18 Grocery Stores.***

*Similar to the intent expressed in the preceding policy, the Specific Plan encourages the siting of the first traditional grocery store in the town center. However, this policy also recognizes that if plans for other projects in the vicinity of Placer Vineyards include lands designated for a grocery store, market pressures may make it necessary to override this policy. As a result, the town*

*center will have the exclusive right within the Plan Area to open a traditional grocery store, such as a Safeway, Albertson's, Raley's, or similar type stores for a period of 12 months following the issuance of a certificate of occupancy on the 250th residential unit within the Specific Plan. Notwithstanding the foregoing, if the City of Roseville approves a specific plan for a project adjacent to the Plan Area that is immediately north of Base Line Road, and the project includes a traditional grocery store component, then the right of exclusivity for traditional grocery stores in the town center shall become null and void, and any commercial owner in the Plan Area with properties zoned as PC, BP/PC and COM will be allowed to open a traditional grocery store without the restrictions described above. This right of exclusivity shall not apply to non-traditional grocery stores, such as a Wal-Mart, Food 4 Less, or similar type store.*

***Policy 3.19 Movie Theater and other Entertainment Uses.***

*To encourage pedestrian activity and provide attractions that support local shops and restaurants in the town center, the town center shall have exclusive right to all movie theater uses within the Plan Area. Other entertainment uses including dance clubs, live music, theaters, and art galleries shall also be located primarily in the town center.*

**Commercial/Mixed-Use (14 to 22 du/ac) (FAR Range: 0.35 to 2.0)**

The Commercial/Mixed-Use (C/MU) designation is intended to encourage a variety of projects with a mix of uses, including high density residential, retail, and office uses within one development. The C/MU designation allows for mixed-use neighborhood nodes of office and commercial uses on smaller sites integrated into the surrounding residential neighborhoods. Mixed-use developments may include both vertical mixed-use—ground floor commercial uses with residences or offices above, or horizontal mixed-use—commercial and residential development located on the same site with shared open space and direct pedestrian connections.

Typically found on the corners of collector and arterial streets, the C/MU designation allows flexibility for future market conditions and provides for local neighborhood services. The C/MU district also envisions uses such as live-work residential loft spaces with living units integrated into office spaces, commercial store fronts, and artist studios. C/MU areas will also allow religious facilities.

The C/MU designation allows for a residential density range of 14 to 22 du/ac. For the purposes of distributing development intensities, the land use plan assumes that approximately 70 percent of the development area will be

## LAND USE

developed with residential uses, as assigned in Table 3-3. The FAR applicable to the C/MU land use designation ranges from 0.35 to 2.0. The remaining 30 percent of the development area is assumed to develop with commercial uses evenly split between retail and office uses at a FAR of 0.45. Refer to Appendix A for allowed Commercial/Mixed-Use permitted uses and applicable Development Standards.

### ***Policy 3.20 Parking in Commercial/Mixed-Use Sites.***

*For mixed-use projects, including the town center and village centers, parking may be shared between uses. To facilitate shared parking, a parking analysis shall be prepared during the Design/Site Review process that lists the project-specific assumptions for the mix of uses, outlines peak parking periods for each use type, and demonstrates that adequate parking will be provided. To relax the individual parking requirements, the County Planning Director shall determine that, based on the findings in the shared parking analysis, the entire project will not result in a parking shortfall.*

### **Office (FAR Range: 0.25 to 0.45)**

The Office (O) land use category is intended for professional and administrative office uses (e.g., finance, insurance, and banking offices), office parks for research and development, medical and dental facilities, and related incidental office-supporting commercial uses (e.g., copy centers, cafes, communication retail sales and services, and office supplies). Refer to Appendix A for the complete list of permitted uses in the Office land use designation.

The FAR applicable to the O land use designation ranges from 0.25 to 0.45. The intensity utilized for the purpose of distributing Office intensity to individual parcels of record is 0.30. Refer to the Development Standards in Appendix A for the applicable Development Standards.

### **Business Park (FAR Range: 0.20 to 0.45)**

The purpose of the Business Park (BP) land use designation is to provide a wide-range of large-scale office, commercial, and light industrial land uses on large parcels. BP land uses will provide employment, commercial, and regional uses that will help foster a balance of jobs and housing and meet the economic goals of the Placer County General Plan. The BP land use designation also allows for flexibility to meet changing market conditions over time. The BP designation allows for a mix of office park uses (e.g., light industrial, “high-tech” manufacturing and assembly, distribution, warehousing, research and development, and medical and dental facilities) and supporting retail commercial uses (e.g., business services and office support services). The BP designation also

allows for public and quasi-public uses such as commercial recreation uses, religious facilities, and private school and university facilities.

The BP designation allows for office and retail development within a FAR range of 0.20 to 0.45. For the purposes of distributing development intensities, the land use plan assumes that the BP development area will develop with 10 percent retail uses and 90 percent office uses at a FAR of 0.246. Refer to the Development Standards in Appendix A for the allowed Business Park uses and applicable Development Standards.

### **Power Center (FAR Range: 0.20 to 0.35)**

The Power Center (PC) land use is envisioned for large-scale retail stores providing goods and services for the regional market. Stores include, but are not limited to, home improvement and large-scale gardening centers, large-scale discount centers, furniture, computers, household goods and groceries, auto sales and services, auto service stations, tire stores, large-scale clothing outlets, and other uses typically found in power centers. PC uses may also include restaurants and drinking establishments, and fast-food outlets (including drive-thru facilities). The PC designation will also allow for the location of public and quasi-public uses such as commercial recreation uses, religious facilities, and private schools and university facilities.

The FAR applicable to the PC land use designation ranges from 0.20 to 0.35. For the purposes of distributing development intensities, the land use plan assumes that the PC development area will be 100 percent retail uses at a FAR of 0.25. Refer to the Development Standards in Appendix A for the allowed Power Center uses and applicable Development Standards.

### **Business Park/Power Center**

31 acres are designated with a Business Park/Power Center (BP/PC) land use in the Plan Area. The BP/PC designation allows uses from both the BP land use and the PC land use categories, defined above. The FAR applicable to the BP/PC land use designation ranges from 0.25 to 0.45 for Business Park uses and 0.20 to 0.35 for Power Center uses. For the purposes of distributing development intensities, the land use plan assumes that the BP/ PC development area will develop as a Power Center with 80 percent retail uses and 20 percent office uses at a FAR of 0.25. Refer to Appendix A, Section 3.7, “Business Park” or Section 3.8, “Power Center” for BP/PC development standards and to Table A-14 for permitted Business Park and Power Center uses.

## **PARKS, RECREATION, AND OPEN SPACE USES:**

The parks, recreation, and open space system proposed for Placer Vineyards incorporates 842.8 acres of parks and open space to be used for active and passive recreation. 2 large community-sized parks are distributed in the Plan Area, as well as 2 large open space oak preserve areas. 9 neighborhood parks, 8 potential joint-use neighborhood park/school sites, a town center green, and several smaller image parks, are dispersed throughout the Plan Area. A dog park is also proposed in the Plan Area. In addition, a series of trails linked to several east-west and north-south open space corridors provide neighborhood connections throughout the Plan Area. Information on trails is provided in Chapter V, “Transportation and Circulation.” Additional details on the parks and open space system are described in Chapter VII, “Parks and Open Space.”

### **Open Space**

The Open Space (OS) land use designation is intended to protect the natural areas, creeks, wetlands, and specified tree groves within the Plan Area. Open space will include flood control and drainage channels, properties within power line easements, and special setback areas, such as along the Placer/Sacramento County line. Open Space areas may have compatible uses, including trails and other active and passive recreational uses, including their associated parking lots.

A total of 692.8 acres (approximately 13.2 % of the Plan Area) is designated Open Space. This category includes only natural and/or passive open space and does not include parks.

### **Oak Grove Open Spaces**

Concentrations of significant oak trees on the site are preserved in 2 large oak grove open space areas. These oak grove areas provide open space amenities to the community and provide passive recreational opportunities, such as picnic areas, quiet seating areas, and trail loops.

### **Park**

A variety of park types and sizes are planned for Placer Vineyards under the Park (P) designation. Each park is planned and designed with a range of park facilities, depending on the character of the park. The park types and characteristics are described below.

### **Town Center Green**

A 3.5-acre town center green is centrally located in the town center. The town center green is intended to serve as a civic, cultural, and community focal point, designed to complement the design themes within the town center. The town center green will include play areas, picnic areas, an outdoor space configured for performances, open areas for farmers’ markets or other informal uses, and a water feature or other landmark feature as a focal point.

### **Neighborhood Parks**

Neighborhood parks range from 2 to 4 acres in size. Eight potential joint-use school/park sites are also included in this category. Facilities planned for joint-use school/park sites should take into consideration proposed school facilities to maximize the recreation facilities of both. These parks should also be designed to operate independently of the school facilities, if necessary.

### **Community Parks**

2 large community parks are located in the Plan Area. The East Community Park is located at the southeast corner of Watt Avenue and Dyer Lane, adjacent to the Dry Creek Parkway. The West Community Park is bounded to the east by West Dyer Lane, just northwest of the power line easement corridor.

Community parks will include active recreation uses such as ball fields, passive recreation uses such as trails and picnic areas, and other support facilities such as restrooms, concession stands, and parking and staging areas for bicyclists and runners. Additionally, these parks may be used for meetings, neighborhood activities, special-interest groups, and youth and adult sports leagues. Additionally, a multi-purpose Community Center may be located in the West Community Park.

## LAND USE

### Private Parks

Private parks and a recreation center are provided within the active adult community in the eastern portions of the Plan Area, identified in Figure 7.1, “Parks and Open Space Plan Diagram.” One-half of the private park acreage shall count toward satisfying the park dedication requirements for the project, subject to the conditions for private parks found in Section 16.08.100-I, “Private Facilities Credit” in the *Placer County Code*.

### Landscaped Rights-of-Way and Entries

Landscape corridors and landscaped areas are provided along public street rights-of-way as networks of green spaces designed to enhance the visual and environmental quality of the Plan Area for pedestrians and other visitors. Landscape entry features and gateways are also provided in the community to help define different parts of the community. These are landscaped, themed features at the entrances to major intersections of the community or smaller signature features in residential neighborhoods. Landscaped rights-of-way and entries are not counted within the park or open space requirements for this project. Landscape rights-of-way shall be developed and dedicated to the County at the same time that the public street rights-of-way are developed and dedicated to the County. See Chapter VI, “Community Design,” for additional streetscape and community gateway design guidelines.

### Public and Quasi-Public Uses:

The Placer Vineyards Specific Plan provides for a broad range of cultural, public/quasi/public, and religious uses, strategically located throughout the Plan Area. Nestled within the Placer Vineyards neighborhoods and village centers and connected with local trails and bicycle paths, these areas provide for cultural activities that enrich the character and quality of the community in proximity to where people live. These uses are listed below.

#### Cemetery

Cemetery (C) designates property subdivided into cemetery lots and offering burial plots or air space for sale.

#### Corporation yard

A corporation yard (CY) site is provided in the Plan Area to accommodate facility needs of the County. 2 acres of the corporation yard site shall be given credit for Community Park acreage (refer to Table 3-2 Land Use Summary).

#### Fire

Sites for 2 fire stations (F) are provided in the Plan Area. One fire station site is intended to serve the eastern side of the Plan Area and is located on Town Center Drive near Watt Avenue. The other fire station site is located on the west side of the Plan Area, at the intersection of Palladay Road and A Street. A fire administrative center may be collocated with other County administrative offices in the Town Center.

#### Government/Sheriff

A government (Gov) office site is provided in the town center. A small Sheriff's Substation may be collocated with the government office in the town center to provide policing and local public safety services.

**Library**

A library (L) site is centrally located in the town center to provide literary, educational, and informational resources to the community.

**Religious Site**

The Religious Site (REL) designation is reserved for religious organization facilities operated for worship or promotion of religious activities, such as churches, synagogues, and temples. Religious sites also include religious accessory uses on the same site, including living quarters for staff, child day care facilities, and other uses normally associated with a religious facilities (e.g., schools and recreation facilities), as approved by the Planning Director.

Sites made available for religious uses other than the religious sites designated in Figure 3.1, “Land Use Diagram,” may be approved through a Minor Use Permit and shall be identified on tentative maps.

**Transit Station**

A multi-modal transit station or terminal site (T) is provided in the Plan Area within the east village center with access provided from Watt Avenue. The station/terminal will serve to distribute information on local transit options and serve as a passenger terminal and transfer station for public mass-transit systems including a future, potential bus rapid transit line along Watt Avenue.

**Utility Substation**

The utility substation (SS) designation allows for electrical substations, pumping stations, pressure regulation stations, or similar facility required to serve the Plan Area.

**SCHOOLS:**

Sites for 6 elementary schools, 2 middle schools, and 1 high school are designated by the ES, MS, and HS symbols on the Land Use Diagram. The Land Use Diagram designates 167 acres for schools. School sites are situated adjacent to park sites and open space to allow for joint use of facilities, trail access, and efficient use of the land. Joint school/park sites are centrally placed within each neighborhood to provide a focus for neighborhood interaction and to allow children to walk to school. School sites have been located based on the estimated number of students in each surrounding neighborhood and may need to be revised slightly based on actual build-out densities; one of the middle school sites may also be relocated in accordance with Section 9.2.6-C, consistent with these location parameters. Schools are sized for “stand alone”

facilities, which may develop independently of parks.

***Policy 3.21 Land Use for Unused School Sites.***

*If the responsible School District decides that a school site shown in the Land Use Diagram is not needed, the County and/or the Park District shall be offered the opportunity to purchase all or part of the unused school site, adjacent to a designated neighborhood park, for park purposes. For any remaining portion of a school site not purchased by the County and/or the Park District, residential development will be permitted at the residential density of the predominant land use adjacent to the designated school site. The total number of residential units allocated to the underlying parcel of record as of the date of the adoption of this Specific Plan (see Table 3-3) will not be increased by the readjustment of the land use. The neighborhood park site located adjacent to the excess school site shall remain as indicated on the Land Use Diagram, providing a central focus for the neighborhood.*

*If subsequent to the development of the property adjacent to a school site, the School District decides it no longer has a need for the property, it will be necessary to amend the Specific Plan to define the appropriate use of the property.*

**Elementary Schools**

Elementary schools (ES) serve kindergarten through grades 5 or 6 (depending on the school district). Elementary school sites are 12 acres in size with an adjacent neighborhood park. They are located within residential neighborhoods, within easy walking distance (1/2 mile radius), and provide easy auto access from local residential and collector streets.

**Middle Schools**

Middle school (MS) sites are planned for 22.5 acres in size and will serve grades 6 through 7 or 8 (depending on the school district). 2 middle schools sites are provided. These sites are located within residential neighborhoods on major collector streets.

**High School**

The High School (HS) site, located on Palladay Road between A Street and Base Line Road, is 50 acres. It is planned to serve grades 9 through 12 and is sited on collector streets, in proximity to the surrounding residential community, local commercial services, and adjacent to park facilities located in the community park.

## LAND USE

### SPECIAL PLANNING AREA:

The Special Planning Area (SPA) designation is located on approximately 979 acres at the western portion of the Plan Area and includes the existing Riego area. There are approximately 150 existing homes within the SPA. Approximately 200 (or 87 percent) of the 230± existing parcels within the SPA are 5 acres or less in size, with the majority being less than 2 acres and located primarily in the Riego township area. The remaining approximately 30 parcels range in size from 5 to 96 acres and are generally located in the vicinity of Newton Street, south of Browning and Colburn Streets. Refer to Figure 3.2 for a map of the SPA.

Of the 14,132 units within the Plan Area, a total of 411 total units are reserved in the SPA for the eventual build-out of this area. These 411 units include the 150 existing homes, leaving an additional 261 new homes allowed for development in the SPA. The 261 additional units reserved for the potential build-out of parcels within the SPA area include: 63 new units allowed to develop consistent with their current zoning and 198 units allowed to develop under the Placer Vineyards Specific Plan and the Specific Plan EIR on a first come, first serve basis. Property owners who choose to subdivide their properties beyond that allowed under current zoning in the SPA will be required to amend this plan (refer to Section 9.2.7).

The main trunk lines of the Placer Vineyards infrastructure system (water, wastewater, and drainage systems) will be sized to serve the anticipated build-out of up to 411 total units reserved within the SPA. Property owners of the SPA may connect to infrastructure systems in the Plan Area but shall be responsible for the costs incurred from the extension of services to their property, including any hook-up fees, Plan Area, or Special District fees.

#### ***Policy 3.22 Special Planning Area.***

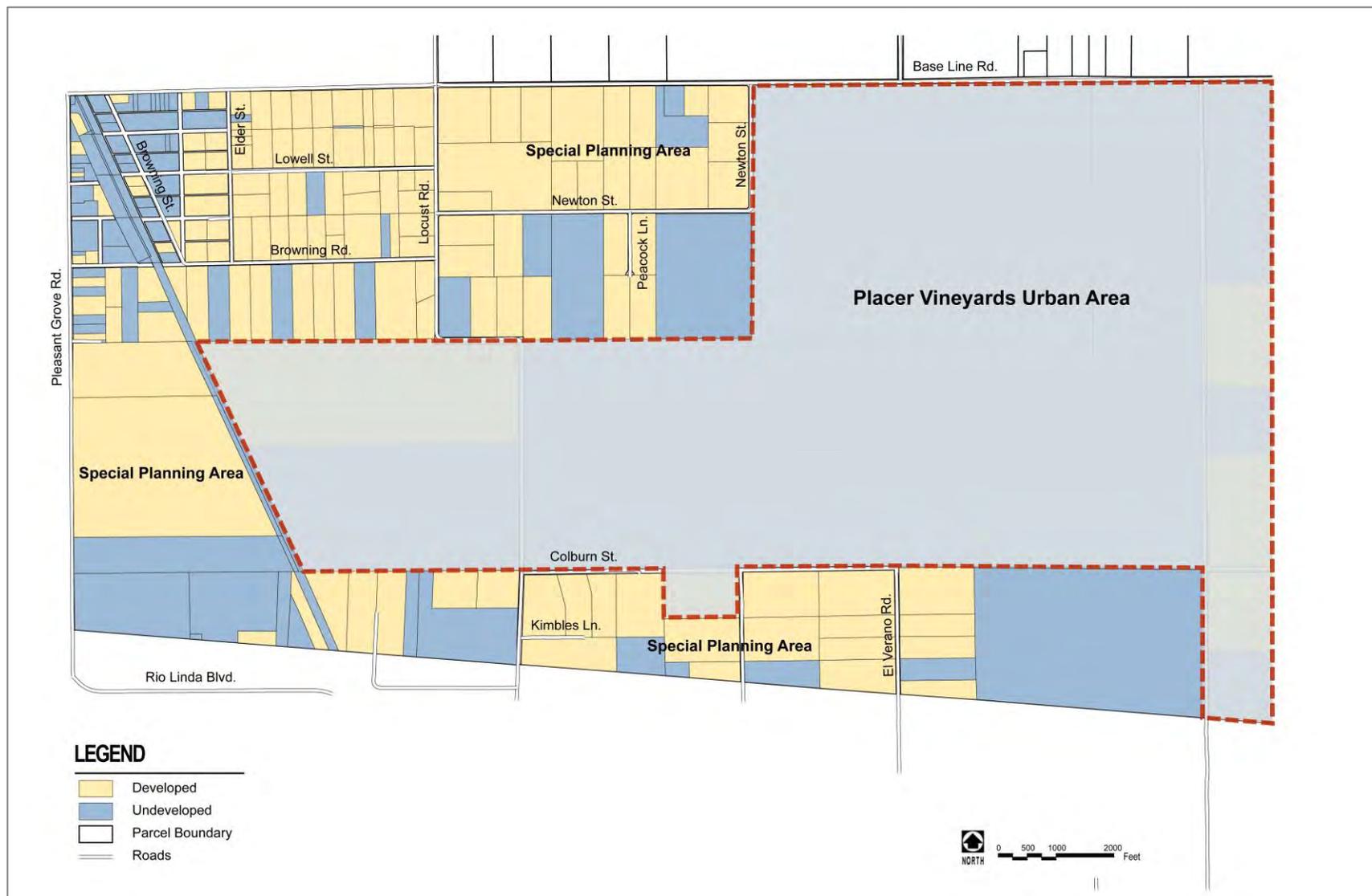
*This Specific Plan does not revise or designate zoning for the SPA properties. The corresponding existing zoning for these SPA properties and existing County administrative processes shall govern. Refer to Chapter IX, Section 9.2.7-D, "Entitlements to the Special Planning Area" for the procedures for requesting entitlements in the SPA.*

### 3.4 LAND OWNERSHIP AND LAND USE ALLOCATION

Figure 3.3 and Table 3-3 summarize the development program for each property owner in the Plan Area. Within the development program, the following standards shall apply.

1. No property may exceed the permitted density range for the land use designation or the allowable number of residential units and commercial intensities assigned to the property in Table 3-3 without a rezoning, Specific Plan amendment, and additional environmental review. However, to provide development flexibility, development transfers between land use parcels is permitted, provided that they meet the criteria set forth in "Transfer of Density," described in Chapter IX, "Implementation."
2. When constructed at the same time as the primary residential unit is being constructed on a lot, a secondary dwelling shall be counted as a residential unit for the purposes of calculating the total number of units allowed for a property under Table 3-3. Each such secondary dwelling shall be subject to payment of all fees payable by the primary unit, including those required under the applicable provisions of the development agreement. Lots where secondary dwellings are proposed to be constructed shall be identified as part of any application for the tentative subdivision map for the property.

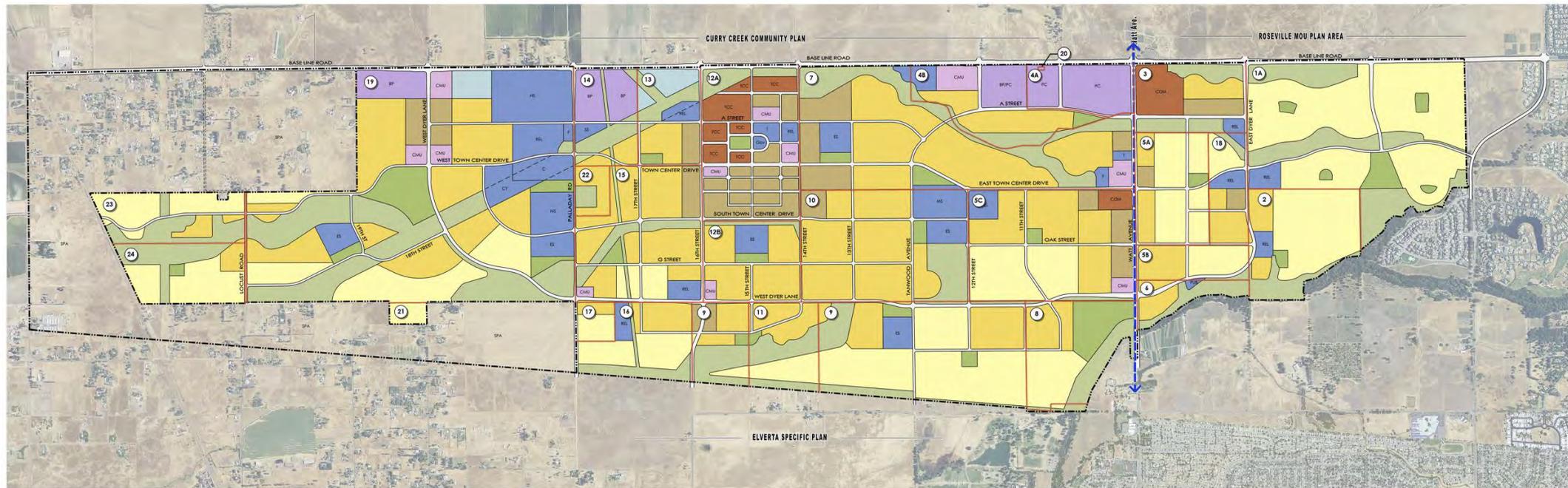
Figure 3.2 - Special Planning Area Location Map



Source: Placer County Planning 2005

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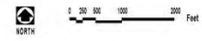
Figure 3.3 - Land Ownership Diagram



EDAW | AECOM

**LEGEND**

<span style="background-color: #f08080; border: 1px solid black; padding: 2px;">C/MU</span> COMMERCIAL MIXED USE	<span style="background-color: #d3d3d3; border: 1px solid black; padding: 2px;">SPA</span> SPECIAL PLANNING AREA	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">ES</span> ELEMENTARY SCHOOL	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">REL</span> RELIGIOUS FACILITY	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">CY</span> CORPORATE YARD	<span style="background-color: #90ee90; border: 1px solid black; padding: 2px;">OS</span> OPEN SPACE
<span style="background-color: #ffa500; border: 1px solid black; padding: 2px;">COM</span> COMMERCIAL	<span style="background-color: #fff2cc; border: 1px solid black; padding: 2px;">LDR</span> LOW DENSITY RESIDENTIAL	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">MS</span> MIDDLE SCHOOL	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">F</span> FIRE	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">SS</span> SUBSTATION	<span style="background-color: #90ee90; border: 1px solid black; padding: 2px;">P</span> PARK
<span style="background-color: #e6e6fa; border: 1px solid black; padding: 2px;">PC</span> POWER CENTER	<span style="background-color: #fff2cc; border: 1px solid black; padding: 2px;">MDR</span> MEDIUM DENSITY RESIDENTIAL	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">HS</span> HIGH SCHOOL	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">GOV</span> GOVERNMENT	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">T</span> TRANSIT	
<span style="background-color: #dda0dd; border: 1px solid black; padding: 2px;">BP</span> BUSINESS PARK	<span style="background-color: #fff2cc; border: 1px solid black; padding: 2px;">HDR</span> HIGH DENSITY RESIDENTIAL		<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">L</span> LIBRARY	<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">C</span> CEMETERY	
<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">O</span> OFFICE			<span style="background-color: #add8e6; border: 1px solid black; padding: 2px;">PD</span> POLICE		<span style="color: blue; font-size: 1.2em;">↔</span> BRT LINE



LAND USE

Table 3-3: Land Use Ownership Summary

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26		
Property ID#	AP#	Property Owner	Gross Parcel Acreage	SPA		LDR 2-6 du/ac		MDR 4-8 du/ac		HDR 7-21 du/ac		COM.	OFF	CMU 1 du/acre		BP/PC <sup>(N)</sup>	Pub	Rel <sup>(R)</sup>	Schools			Parks <sup>(M)</sup>	OS	Major Roads	Total Units		
				Acreage	Units	Acreage	Units	Acreage	Units	Acreage	Units			Acreage	Units				ES Acres	MS Acres	HS Acres						
1A	23-200-005 23-221-002, 057, 058	Placer 400 Investors, LLC	402.0			284.0	931											8.0				22.0	88.0	20.0	931		
1B	23-200-006	Hodel Family Enterprises, LP	56.0			10.0	35	20.0	128	6.0	90							9.0					4.0	7.0	253		
2	23-200-017	Mounier Family Revocable Lifetime Trust	138.0			84.5	289	21.0	115									6.0				3.0	19.0	4.5	404		
3	23-200-037	Basalme & Watt, LLC	100.5					28.5	153	9.0	105	25.0						4.0						26.0	8.0	258	
4A	23-200-069	BLW 60 LP	65.0												59.7										5.3	0	
4B	23-200-071	LDR AREP III Placer Owner, LLC	114.2					40.5	225					7.0	88	31.0		7.0				3.5	20.0	5.2	313		
5A	23-200-062	Richard Riolo (Non-Participant)	106.5			66.0	230	24.5	106	5.0	75											3.0		8.0	411		
5B	23-200-063	Riolo (Non-Participant)	51.0			23.5	74	22.5	103																5.0	177	
5C	23-200-016, 028	Riolo LP (Non-Participant)	241.5			74.5	250	103.5	562	25.5	375	9.0		4.5	57			9.5				3.0	2.0	10	1244		
6	23-200-019	O'Levey 1991 Living Trust, Francis Smadovich (Susan Platerki)	39.0					14.5	102								1.5							18.0	5.0	102	
7	23-200-045, 056	Bkt II Northern Cal 1	357.0					180.5	912	49.5	698			6.5	82		4.5	1.5	12.0	2.5		10.0	62.0	28.0	1692		
8	23-200-041	Sprnell Investments, LLC Mikajin Investments, LLC	120.0			32.0	112	27.5	117													30.0	24.5	6.0	229		
9	23-200-010, 012, 013	Placer 1 Owners Residency	326.0			152.7	515	58.0	359	8.0	120								12.0				9.0	56.3	30.0	994	
10	23-200-009	Frank Stathos	242.0					152.5	819	11.5	158								12.0	20.0			4.0	27.0	15.0	977	
11	23-200-011	P.O.D. Properties/GP	79.0			23.0	81	25.0	134														1.5	27.5	2.0	215	
12A	23-200-087	Il Centro, LLC	196.0							65.0	871	42.5		11.5	144		7.0	5.0					3.5	20.0	41.5	1015	
12B	23-200-088	Placer 102, LLC	102.0					64.0	342					3.0	38				12.0				4.0	6.5	12.5	380	
13	23-010-024, 23-200-080	Cabal, et al (Non-Participant)	80.0					20.0	122	7.0	105		17.5										3.0	13.0	12.0	227	
14	23-010-020	D.F. Properties, Inc.	80.0					12.0	58							36.0	4.5							20.0	7.5	58	
15	23-010-024, 029, 23-200-008	Holiday Greens, LLC	202.0			21.0	70	117.0	639	12.0	150			3.0	38								3.5	23.0	15.5	897	
16	23-010-005, 014	Placer Vineyards Development Group, LLC	94.0			47.0	151	20.0	126																16.0	5.5	277
17	23-010-010	J.A. Skoutas Family Partners, LP	19.5			12.0	42	7.5	38																	80	
18	23-200-042	Marionel Nassari (Non-Participant)	3.5			1.0	4																			4	
19	23-010-021, 022 & 023, 23-160-035, 027, 23-180-005, 006, 007, 008	Basalme ASB Holdings, LLC, Lerner Wineries, LLC	816.5			99.5	343	217.0	1,148	23.0	345		15.0	15.0	189	23.5	33.0	21.0	24.0	22.5	50.0	45.0	159.5	68.5	2025		
20	23-200-029	Jack Garfield (Non-Participant)	0.3													0.3										0	
21	23-010-018	Pandeleon	10.5			10.5	37																			37	
22	23-010-028	Silgru (Non-Participant)	22.5					16.0	73																6.5	73	
23	23-160-011	PMFSC, LLC	92.5			49.5	173	11.0	41																25.0	7.0	214
24	23-160-004	Pandeleon, et al	94.0			52.0	182	11.0	52														2.0	26.5	2.5	234	
SPA	various	various	979.0	979.0	411																					411	
<b>Totals</b>			5,230.0	979.0	411	1,022.7	3,519	1,214.0	6,474	221.5	3,092	76.5	32.5	50.5	636	150.5	50.5	91.0	72.0	45.0	50.0	150.0	692.8	331.9	14,132		

Notes:

1. Acres and units are approximations and subject to change with more detailed mapping, final alignment of roadways, etc.
2. CMU units are calculated at 70% area coverage.
3. 3 acres of Religious site on property #13 are located under the powerlines
4. 31 acres of BP on property #4 may be a BP or PC land use
5. 22 acres of parks in the active adult community (property 1A) are private parks.

### 3.5 LAND USE ANALYSIS

#### 3.5.1 POPULATION SUMMARY

At final build-out, the population of Placer Vineyards is estimated to be approximately 32,814 residents (including the SPA area). Table 3.4 is a summary of the number of residential unit types and an estimate of the total population of the Plan Area at build-out.

#### 3.5.2 HOUSING SUMMARY

##### Residential Density:

Residential land uses encompass approximately 65 percent of the total property within the Plan Area, of which 18.7 percent is designated as a Special Planning Area (SPA). The distribution of residential unit types is indicated below in Table 3-4.

**Table 3-4: Population and Housing Summary**

Unit Type	Density range (du/ac)	Dwelling Units (du)	% Mix of Units	Household Size (population / du)	Estimated Population
Low Density (Active Adult)	2–6	931	6.6%	1.8	1,675
Low Density	2–6	2,588	18.3%	2.5	6,470
Medium Density	4–8	6,474	45.8%	2.5	16,185
High Density	7–21	3,092	21.9%	2	6,184
Commercial Mixed-use	14–22	636	4.5%	2	1,272
Special Planning Area		411	2.9%	2.5	1,028
<b>Total</b>		<b>14,132</b>	<b>100.0%</b>		<b>32,814</b>

Notes:

(1) Includes density bonus from Religious Site

#### Residential Development Standards

Residential Development Standards for residential land use designations can be found in Appendix A, “Land Use and Development Standards.” Residential design guidelines are also provided in Chapter VI, “Community Design.”

## LAND USE

### Affordable Housing

The Placer County Housing element sets forth policies and programs intended to provide the opportunity for the County to meet its fair share regional housing allocation and to provide an adequate housing supply for County citizens of all income levels. Placer Vineyards will be required to construct 10% of the dwelling units, within the boundary of the project as affordable to very low-, low-, and moderate-income households. The affordable housing program and any affordable housing agreement for the Plan Area shall be determined in accordance with the policies of this Specific Plan, the General Plan Housing Element, State affordable housing laws and requirements, and the approved Development Agreement.

Assuming the development of 14,132 residential units, the project will be required to provide land to accommodate 1,372 units for very low-, low-, and moderate-income households, as distributed in Table 3-5. Refer to Policy 3.27 for standards on distributing affordable housing units.

For the purposes of this plan housing affordability is based on household incomes defined by the State of California Housing and Community Development (HCD). Very low income households have incomes no greater than 50% of the Placer County median income. Low income households have incomes between 51% and 80% of the Placer County median income. Moderate income households have incomes between 81% and 120% of the Placer County median income. Incomes are adjusted by household size. The HCD determines the median income for Placer County annually.

### Affordable Housing Goals

**Goal 3.17** Provide at least 10 percent of the residential units in the Specific Plan Area as affordable housing and distribute affordable residential units throughout the community.

### Affordable Housing Policies

**Policy 3.23 Affordable Housing Requirement.**

*In compliance with State Law, new development shall provide at least 10 percent of the total residential units in the Plan Area (1,372 units) at a price affordable to very low-, low-, and moderate income households (see Table 3.5).*

**Policy 3.24 Affordable Housing Obligation.**

*Owners of residential land will be required to satisfy the affordable housing obligation by constructing a minimum of 10 percent of the units for occupancy by very low-, low-, and moderate-income households, concurrent to and in proportion with development of market rate units within the balance of the property, as defined in the Development Agreement.*

**Table 3-5 Summary of Affordable Housing Obligation**

Affordability Category	Required Allocation (Percentage of Total Units)	Number of Affordable Units Required
Very Low Income	4%	549
Low Income	4%	549
Moderate Income	2%	274
Total	10%	1,372

\* These totals represent the affordable obligations of the urban portion of the Plan Area which contains 13,721 homes and does not include the 411 homes allocated to the SPA.

**Policy 3.25 Affordable Housing Options.**

*While individual property owners are responsible for ensuring that land sufficient to accommodate the number of affordable units is available, the Development Agreement shall provide flexibility regarding how the units are provided. Property owners have two options:*

- 1) They shall construct the units as part of their market-rate developments generally concurrently and in proportion with the development of market-rate units within the balance of the property, as defined in the Development Agreement, or*
- 2) They shall choose to use one or more of the affordable housing options also specified in the Development Agreement.*

**Policy 3.26 Distribution.**

*Affordable housing units shall be provided as either purchase or rental units, or a mixture of both. For sale/purchase affordable units, including duplex units, may be located anywhere within the Plan Area, provided that the affordable units shall not be clustered together in one location.*

**Policy 3.27 Concurrent Development of Affordable Units.**

*Landowners and the County shall enter into Development Agreements that ensures, generally, concurrent development of affordable housing units and development of market-rate units. At the time of approval of the Specific Plan, a Development Agreement will be required to identify the terms for the implementation of affordable housing units. Affordable units shall also be identified on all individual tentative subdivision maps and disclosed to future property owners.*

**3.5.3 EMPLOYMENT SUMMARY**

Table 3.6 estimates potential jobs based on the projected number of employees per acre for office and retail uses. The estimate for school employees assumes 1 employee per 17 students. At full build-out, the Plan Area will provide approximately 7,691 jobs, or approximately 0.5 jobs per household.

## LAND USE

**Table 3-6: Employment Summary**

Land Use Category	Acres	FAR	Gross Square Feet (GSF)	GSF/Employees	Total Jobs
<b>Retail Uses</b>					
COM: Commercial Retail	34.00	0.25	370,260	500	741
C/MU: Commercial/Mixed-Use Retail (15% of C/MU Total Area)	7.58	0.45	148,485	500	297
TCC: Town Center Commercial Retail (80% of TCC Total Area)	34.00	0.45	666,468	500	1,333
BP: Business Park Retail (10% of BP Total Area)	5.95	0.246	63,707	500	127
PC: Power Center (100% Retail)	60.00	0.25	653,400	500	1,307
BP/PC: Business Park/Power Center <sup>(1)</sup> Retail (80% of BP/PC Total Area)	24.80	0.25	270,072	500	540
Subtotal Retail Uses	166.33		2,172,392		4,345
<b>Office Uses</b>					
O: Office	32.50	0.30	424,710	400	1,062
C/MU: Commercial/Mixed-Use Office (15% of C/MU Total Area)	7.58	0.45	148,485	333	446
TCC: Town Center Commercial Office (20% of Town Center Commercial Total Area)	8.50	0.45	166,617	333	500
BP: Business Park Retail (90% of BP Total Area)	53.55	0.246	573,359	750	764
BP/PC: Business Park/Power Center <sup>(1)</sup> Office (20% of BP/PC Total Area)	6.20	0.25	67,518	750	90
Subtotal Office Uses	108.32		1,380,689		2,862
Schools <sup>(2)</sup>	167.00		8,234 students	1 per 17 students	484
<b>Total</b>	<b>441.65</b>		<b>3,553,081</b>		<b>7,691</b>

Notes:

1. 31 acres may be a BP or PC land use (see Figure 3.1, "Land Use Diagram," for site location).
2. Refer to school enrollment summary.

### 3.7 LAND USE DESIGN AND COMPATIBILITY

Good planning and design can help to increase compatibility between different land uses, reduce potential conflicts, and make for better neighbors. The design and land use pattern of Placer Vineyards is organized to provide an appropriate level of compatibility to adjoining rural residential and agricultural uses as well as between higher intensity commercial and adjoining residential uses.

Compatibility of adjoining land uses is concerned with potential impacts and nuisances that may be created as a result of different levels of activity. Examples of such nuisances may include noise, odors, light and dust, use of chemicals and fertilizers, unwanted traffic and congestion, unwanted views, shade and shadow impacts, and the height, scale and character of the built environment. Creating greater compatibility between disparate land use activities can be achieved through use of appropriate land use patterns and site designs, providing appropriate buffers, or through increased setbacks.

**Goal 3.18** Organize the site design and land use patterns in Placer Vineyards to provide an appropriate level of compatibility to adjoining land uses and reduce potential nuisance conflicts.

***Policy 3.28 Compatibility to adjoining Large-Lot Rural and Agricultural Uses.***

*The design and land use pattern of Placer Vineyards shall be organized to provide an appropriate level of compatibility to adjoining rural residential and agricultural uses.*

1. *Land use intensities and densities in Placer Vineyards should be stepped down approaching or adjoining rural residential and agricultural uses to create a more compatible transition between uses.*
2. *Open space buffers shall be provided along the entire edge of the Special Planning Area (SPA). Refer to Section 7.3, "Open Space," in Chapter VII, "Parks and Open Space," for the design of open space landscape buffer areas adjacent to the SPA (see Figure 7.10 for required open space buffer setbacks and locations).*
3. *To buffer land in agricultural preserve under the Williamson Act, located north of Baseline Road, a minimum 200-foot setback shall be maintained from the northern existing edge of pavement of Baseline Road for all non-open space and non-infrastructure-related land uses located south of Baseline Road. This 200-foot setback shall only apply to those parcels that are located south of lands that are in active rice farming or located in an*

*agricultural preserve that remains under Williamson Act contract at the time of development. See Figure 3.4 for a map of the location of the Williamson Act agricultural preserve.*

4. *Open space setbacks and surface drainage swales shall be used as a buffer between residential and adjoining agricultural and rural land uses.*
5. *Site design of residential neighborhoods shall employ a number of techniques as a buffer to adjoining uses, including larger lots, larger front or rear yard setbacks, loop roads, cul-de-sacs, single-loaded streets, and open space and landscaped setback buffers. Refer to Section 6.4.1, "General Lot Design," in Chapter VI, "Community Design," for residential lot design guidelines.*
6. *Local streets in new residential neighborhoods shall have limited connections to existing local rural lanes and streets to avoid traffic and parking in adjoining agricultural areas.*
7. *To provide an appropriate level of compatibility between rural residential uses in the Special Planning Area (SPA) and the development of the business park property at the southeast corner of Baseline Road and Newton Street, the following provisions shall apply.*
  - a. *Provide a minimum 115-foot buffer from the west side of Newton Street to the nearest business park building. This buffer shall consist of a 30 foot distance provided by existing Newton Street, a 40-foot landscaped berm, and 45-foot on-site building side yard setback. The 45 foot on-site side yard setback shall be landscaped and may also include parking (see Figure 7.10 for the conceptual berm and buffer plans and Figure 7.11 for the conceptual buffer section).*
  - b. *Loading docks and circulation, including ingress and egress into the Business Park property, shall not be provided from or located adjacent to Newton Street.*
  - c. *Lighting fixtures shall be selected and designed to minimize glare and excess lighting spillage into the SPA. All street and parking lights shall utilize cut-off fixtures to minimize visibility from adjacent areas of the community. Lighting shall not be blinking, flashing, or be of unusual intensity or brightness.*
  - d. *Buildings shall be designed at heights and intensities that provide an appropriate level of compatibility to the SPA. Business Park development projects shall be subject to the requirements of the Design/ Site Review process.*

## LAND USE

### ***Policy 3.29 Compatibility of Residential Uses Adjacent to Commercial and Employment Uses.***

*The design and land use pattern of Placer Vineyards shall be organized to provide an appropriate level of compatibility between residential neighborhoods and adjoining commercial and higher intensity office uses.*

- 1. Residential densities in Placer Vineyards should create a transition between commercial and office uses and adjoining low density residential neighborhoods. Transitional uses may include higher density residential uses, public and quasi-public uses, open spaces, and parks.*
- 2. Local streets will avoid direct connections between large commercial centers and surrounding residential areas that could allow commercial traffic to pass through the residential neighborhoods and affect local streets with commercial parking.*
- 3. Open space setbacks, parks, and surface drainage swales are appropriate buffers between residential uses and adjoining higher intensity commercial and office uses.*
- 4. Site design of residential neighborhoods can employ a number of techniques as a buffer to adjoining uses, as indicated in Figure 6.18, "Lotting Conditions Adjacent to Major Roadways," in Chapter VI, "Community Design." Such techniques include larger setbacks, loop roads, cul-de-sacs, frontage roads, single loaded streets, open space, and landscaped buffers.*

Figure 3.4 - Map of Williamson Act Parcels



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## CHAPTER IV: ENVIRONMENTAL RESOURCES



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## 4.1 ENVIRONMENTAL RESOURCE CONCEPTS

### CONTEXT

This chapter identifies the environmental conditions and sensitive resources found in the Plan Area. Goals and policies contained in this Specific Plan shall guide the conservation, protection, or mitigation of existing environmental conditions and sensitive resources. This chapter addresses seven key areas: wetland resources, special-status species, oak and riparian woodlands, water quality and conservation, energy conservation, air quality, and noise.

The existing environmental conditions present in the Plan Area were taken into account during development of the Land Use Plan. The Land Use Plan is designed to protect significant, sensitive resources in open space and to minimize the impacts of urban development on the existing and natural communities in the Plan Area, to the extent feasible. This Specific Plan, however, recognizes and provides off-site mitigation of environmental resources. Through this approach of on-and-off-site environmental mitigation, the potential to protect large areas of significant open space is maximized.

**Goal 4.1** Establish a comprehensive approach for the replacement of affected open space and agricultural and habitat areas.

### Placer County Conservation Plan

Placer County is currently preparing a Natural Community Conservation Plan and Habitat Conservation Plan to address the conservation of natural communities, endangered species, and other less sensitive species of native wildlife. At the same time the County is pursuing a California Department of Fish and Game (DFG) Master Streambed Alteration Agreement and Clean Water Act Section 401 water quality certification. Collectively, these activities have been termed the Placer County Conservation Plan (PCCP). The County is also in the process of applying for a Clean Water Act Section 404 Programmatic General Permit through a County Aquatic Resource Permit (CARP).

The purpose of the PCCP will be to encourage and simplify the process of conserving sensitive habitats for special-status species. The Placer County General Plan and the PCCP will set the minimum mitigation ratios needed for the off-site mitigation component of the eventual federal, state, and local mitigation package. All projects designed during the preparation of the current first phase of preparation of the PCCP are to be consistent with the principles and objectives of the conservation process. Because activities related to implementation of the Specific Plan may commence before the approval of Phase 1 of the PCCP, this Specific Plan and the mitigation measures included in the project Environmental Impact Report (EIR) are designed to be implemented before approval of the PCCP.

### Placer Vineyards Open Space Mitigation and Management Plan

The property owners within the Plan Area have developed a strategy and program designed to reduce the impacts of the Specific Plan on open space and biological and agricultural resources. The intent of this mitigation strategy is to provide a single, all-inclusive mitigation measure designed to simultaneously address possible impacts on all biological resources of concern, while also addressing potential impacts on open space and agricultural lands. The goal in devising this strategy is to formulate a biological protection, preservation, and mitigation program that includes measures likely to find their way into the PCCP, while also mitigating impacts to open space and agricultural lands. The mitigation strategy is formulated to simultaneously satisfy the requirements of CEQA, the Placer County General Plan, and other federal, state, and local statutes, regulations, and policies that affect open space, agricultural lands, and biological resources. The program also seeks to strike a reasonable balance between on-site resource avoidance and off-site preservation and restoration.

To address the need for replacement habitat, agriculture, and open space areas, 6 potential off-site mitigation sites have been identified for the project. Placer Vineyards property owners may either choose to acquire land located in the 6 mitigation sites, or alternate mitigation sites may be acquired that would achieve comparable mitigation ratios. All off-site mitigation must be in accordance with the terms of the PCCP, once it is approved, or as permitted by the U.S. Army Corps of Engineers (USACE), U.S. Fish and Wildlife Service (USFWS), and DFG.

The property owners' proposal also requires that a combination of one or more mitigation sites establish a core preserve area of approximately 1,000 acres set aside and protected by permanent conservation easement before the initiation of any ground-disturbing activities. The remaining mitigation requirements, beyond the 1,000 acres of open space required to be set aside, are to be addressed on an incremental basis as the development of individual projects proceed. At the time of establishment of the preserves, at the time of additions to preserves, and/or at the time of incremental additions to the geographic scope of a preserve, a management plan for the operation of the preserve lands must be approved by the County.



## ENVIRONMENTAL RESOURCES

Mitigation areas may be discontinuous with one another as a result of land additions to preserve holdings so long as the land holdings meet the minimum acreage and location requirements of mitigation measure 4.4-1 in the Specific Plan EIR. Those requirements include a minimum area of 200 acres and location within the General Plan's Agricultural and Open Space categories. Subsequent Specific Plan projects shall mitigate (for agriculture, wetlands, and/or habitat) through the establishment of preserve areas that, to the extent feasible, are located adjacent to the core preserve or other existing preserve sites.

### **Policy 4.1 Open Space Mitigation and Management Plans.**

*One acre of open space will be preserved within Placer County for each acre of open space affected within the Specific Plan area. The process through which this will be accomplished will be the approval and implementation of a series of Open Space Mitigation and Management Plans that address the management of a specific property to be preserved for mitigation of lost open space, agricultural land, and habitat. An Open Space Mitigation and Management Plan will be required for each individual development project or grouping of projects within the Specific Plan area.*

## 4.2 WETLAND RESOURCES

Wetland resources include various types of water features such as creeks, drainage basins, lakes, ponds, marshes, and seasonal wetlands. Wetlands can also be perennial and seasonal water features. Wetlands are an important part of the environmental condition because wetlands support a variety of sensitive wildlife and plant species.

Two broad categories of wetlands are identified in the Plan Area: Depressional wetlands and Riverine wetlands. Depressional wetlands include vernal pools, seasonal wetlands, seasonal marshes, seasonal drainage, and ponds. Riverine wetlands include a perennial drainage (Dry Creek) and ephemeral/intermittent drainage.

Other waters also located in the Plan Area are stock ponds, channels, and ditches. Refer to Figure 4.1 for an illustration of the location and extent of existing wetlands. See the Placer Vineyards EIR for a more detailed description and representation of the existing wetlands. Additional detailed analysis will be required for individual project sites with the submittal of development proposals.

### 4.2.1 WETLAND TYPES

Descriptions of wetlands found in the Plan Area are provided below.

#### Depressional Wetlands

##### Vernal Pools:

Vernal pools are shallow depressions with an underlying hardpan layer that causes them to inundate. Vernal pools typically support plant species found in the Sacramento Valley.

##### Seasonal Wetland:

Seasonally inundated basins are scattered throughout the Plan Area. These areas may be categorized as seasonal wetlands depending on their floristic composition and hydrology. Seasonal wetlands can be found within swales, drainages, or depressions and typically support wetland plants, including grasses, native plants, and nonnative forbs.

##### Seasonal Marsh:

Seasonal marshes occur when water remains in an intermittent drainage long enough that marsh vegetation becomes established. Riparian habitat occurs along the southeastern edge of the project adjacent to Dry Creek.

##### Seasonal Drainage:

Seasonal drainage conveys water during the rainy season and becomes dry during the remainder of the year. Seasonal drainage is typically inundated during storm events and remains saturated into the vegetation growing season.

##### Ponds:

Several large hydrologic features are present in the Plan Area, consisting of bermed swales, natural basins, and artificial, excavated stock ponds. Ponds support little, or a fringe of, perennial vegetation, dominated by cattail, tule, and common rush.

#### Riverine Wetlands

##### Perennial Drainage:

Dry Creek runs along the Plan Area's southeastern boundary and has water flowing year round. Vegetation such as Goodding's Willow, Fremont's cottonwood, Valley Oak, and herbaceous species grow along the banks of Dry Creek.

##### Ephemeral/Intermittent Drainage:

Two types of ephemeral drainage features are found in the Plan Area: intermittent drainage and drainage swales. Intermittent drainages can be several feet wide and are typically devoid of vegetation because of fast-moving water. Drainage swales are hydrologic linear features and have floristic characteristics similar to seasonal wetlands, including non-native grasses and forbs.

## 4.2.2 WETLANDS AVOIDANCE AND PRESERVATION

One of the first steps in designing the Land Use Diagram was to identify existing environmentally sensitive areas, including wetlands. Responding to the mapping of sensitive areas, the Land Use Diagram designates sites along natural drainage corridors as open space areas, including wetlands and other environmentally sensitive areas.

The intent of this approach is to preserve the highest quality and most contiguous wetlands within open space areas and at sites along natural drainage corridors. Long-term wetland preservation may also better be achieved in large off-site preserves, which avoid the urban edge effects associated with preserved wetlands in the midst of an urban area.

The final design of wetland corridors and open space areas will be determined during site-specific development proposals and associated site-specific wetland delineations. Although not reflected within the Land Use Diagram, it is anticipated that lands containing off-site wetlands and open space will also be preserved in perpetuity as an element of plan implementation.

**Goal 4.2** Avoid and minimize adverse impacts on wetlands to the extent feasible.

**Policy 4.2** *Individual development projects shall, to the extent feasible, develop plans that will preserve and protect existing wetland areas.*

**Goal 4.3** Develop a plan for mitigation of disturbance of on-site wetlands.

**Policy 4.3** *Where wetland avoidance is not feasible, a wetland mitigation plan will be developed before site disturbance to mitigate all wetland impacts. Mitigation plans will be prepared in accordance with all state and federal regulations and in conjunction with the request for permits from regulatory agencies.*

**Policy 4.4** *Wetland feasibility studies will be prepared to ensure successful establishment of the compensation wetlands in conjunction with the request for permits from regulatory agencies.*

**Policy 4.5** *Compensation wetlands will be constructed within designated open space areas of the Plan Area, or wetland mitigation credits will be purchased from the USFWS approved mitigation bank, or land at an off-site location will be purchased to preserve and or construct mitigation wetlands, or a combination of these alternatives will be implemented.*

**Goal 4.4** Establish a maintenance and monitoring program to ensure that wetland compensation areas are protected and mitigation measures are successful.

**Policy 4.6** *Maintenance and monitoring of wetland compensation areas will be conducted in accordance with requirements of USACE and pursuant to the issuance of a Section 404 permit.*

**Policy 4.7** *Maintenance of wetland compensation areas will be the responsibility of a Community Facilities District (CFD) or other funding mechanism satisfactory to USACE and Placer County.*

**Policy 4.8** *Monitoring of the efficacy of the mitigation program will comply with federal agency requirements and the California Clean Water Act and DFG Code, Streambed Alteration Agreements, and all DFG provisions.*

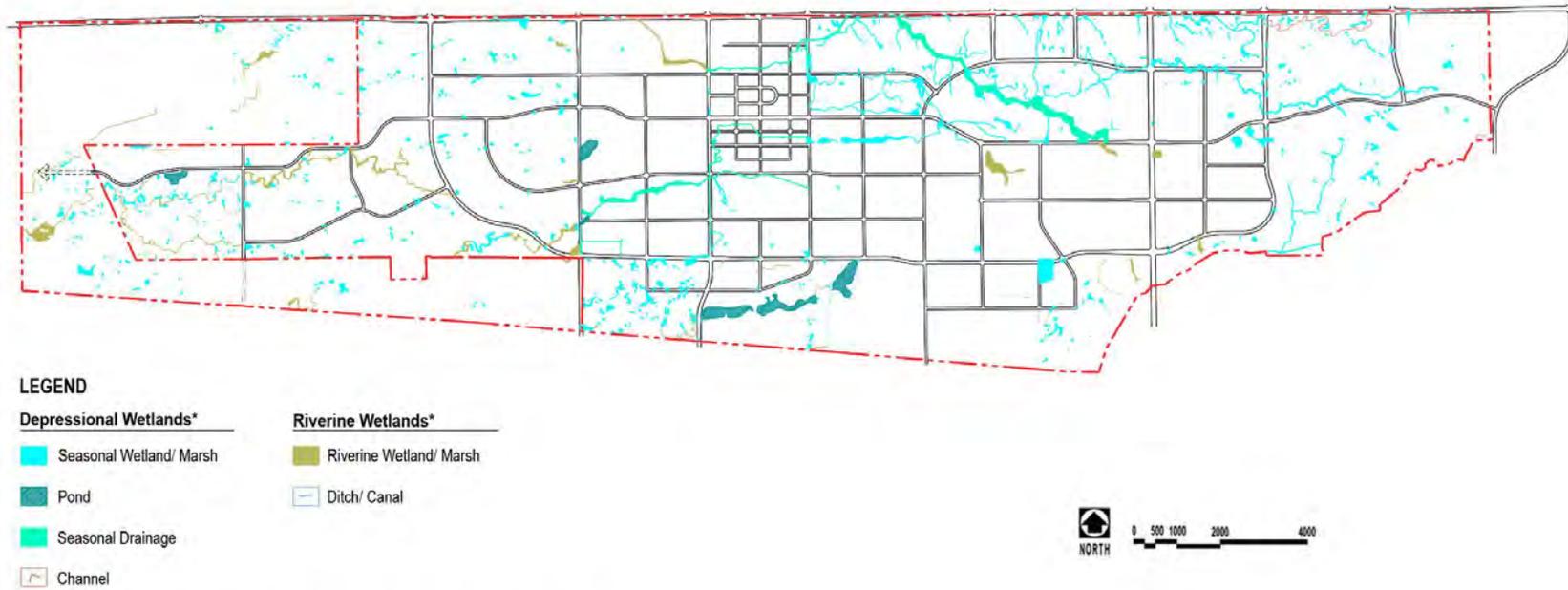
**Policy 4.9** *Maintenance and monitoring programs will be required for compensation wetlands purchased in mitigation banks.*

**Goal 4.5** Provide development plan features that will ensure the long-term health of wetland areas.

**Policy 4.10** *Only passive recreation activities compatible with natural communities will be allowed in wetland preserve areas.*

**Policy 4.11** *Hunting, dumping, operation of motorcycles, or any other activities that could be detrimental to the wetland ecosystems are strictly prohibited.*

Figure 4.1: Wetland Diagram



\* Note: Refer to wetland study by ECORP for further delineation of wetland areas.

Source: ECORP 2005

### 4.3 SPECIAL-STATUS SPECIES

The Plan Area incorporates habitat that support special-status species. The Land Use Plan designed for the Plan Area attempts to avoid potential development impacts on sensitive species by preserving habitat areas, where feasible. Site-specific biological surveys, conducted as part of individual project proposals, will verify existing habitat and species types.

#### 4.3.1 SPECIAL-STATUS SPECIES HABITAT AVOIDANCE

Existing environmentally sensitive areas were identified during the design of the Land Use Plan for the Plan Area. As a result, the Land Use Plan designates numerous open space areas and sites around environmentally sensitive areas.

The overall intent of this approach is to preserve existing on-site sensitive habitat areas, to the extent feasible. However, impacts of development on existing sensitive habitats may be unavoidable, in which case off-site mitigation lands will be provided. The final configuration and design of open space areas to preserve sensitive habitats and the extent of off-site mitigation measures will be determined during site-specific development proposals and associated site-specific biological surveys.

**Goal 4.6** Identify potential special-status species habitat areas and mitigate impacts on these areas.

**Policy 4.12** *Biological surveys will be conducted to identify potentially occurring special-status species before disturbance of habitat areas and in conjunction with requests for permits from regulatory agencies.*

**Policy 4.13** *Where special-status species habitats are indicated, project-specific mitigation measures will be developed in consultation with Placer County, DFG, and/or USFWS.*

**Policy 4.14** *Where state or federally listed special-status species may be adversely affected, required consultation will be conducted and/or appropriate permits obtained before disturbance of habitat areas.*

### 4.4 OAK AND RIPARIAN WOODLANDS

The Plan Area historically involved grazing and dry land farming. Two large stands of valley oaks still remain in the east and central portions of the Plan Area. Oak trees grow along Dyer Lane and riparian woodlands are found along Dry Creek. The Land Use Plan for the Plan Area designates open space uses to preserve the oaks and riparian woodlands.

#### 4.4.1 TREE PRESERVATION

All oaks and riparian woodlands located in the Plan Area are subject to the Placer County Tree Preservation Ordinance and the policies of this Specific Plan. Measures provided in this Specific Plan apply to both private developments and public projects.

**Goal 4.7** Preserve oak trees and riparian woodlands.

**Policy 4.15** *Oaks and other native trees with trunk diameters of 6 inches or greater, measured at breast height, will be preserved wherever feasible.*

**Policy 4.16** *Location and preservation of oaks and other native trees will be indicated on site-specific, tentative maps.*

**Policy 4.17** *Mitigation for trees removed from existing riparian or improved drainage corridors will be accomplished according to the following procedures:*

- ◆ *For each riparian tree removed, one 15-gallon tree, one 2-inch by 10-inch tube container (Deepot 40) seedling for each inch of diameter of the removed tree, and three 1-gallon shrubs will be planted in the riparian or improved drainage corridors.*
- ◆ *For each oak tree greater than 6 inches diameter at breast height that is removed, one 15-gallon tree, one 2-inch by 10-inch tube container (Deepot 40) seedling for each inch of diameter of the removed tree, and three 1-gallon shrubs will be planted.*

**Policy 4.18** *Site-specific design and tree preservation, removal, and mitigation will be identified on an individual project basis and shall conform to the requirements developed in the Mitigation, Monitoring, and Reporting Program.*

**Policy 4.19** *During construction, brightly colored, temporary plastic fencing that is at least 4 feet tall will be erected 1 foot outside the outermost edge of the tree's dripline or around the combined dripline of groves or lines of trees for protection. Signs will be erected in accordance with the Placer County Tree Preservation Ordinance.*

## ENVIRONMENTAL RESOURCES

**Policy 4.20** *Soil disruptions within driplines of existing oaks and other native trees identified for preservation will be avoided where feasible. Paving shall not be placed in the driplines of trees to be preserved.*

**Policy 4.21** *Grading and landscaping will be designed to prevent overspray or runoff within tree driplines.*

**Policy 4.22** *Irrigation will not occur within the driplines of indigenous oaks, except as prescribed by the project arborist or landscape architect.*

**Policy 4.23** *Plantings within driplines of indigenous oaks will be limited to species that require no irrigation and are tolerant of the natural semi-arid habitat of the oaks.*

### 4.5 WATER QUALITY AND CONSERVATION

Development within the Plan Area could potentially have adverse impacts on water quality, including short-term impacts from construction activities and long-term impacts from urban runoff. The Specific Plan minimizes potential water quality impacts through the preservation of existing drainage ways and establishing specific locations for detention and water quality basins and best management practices (BMPs) to contain, retain, and filter storm water runoff.

**Goal 4.8** Reduce water quality impacts within the Plan Area to the maximum extent practicable.

**Goal 4.9** Site-specific development projects should incorporate low-impact development design principles into the site layout.

**Policy 4.24** ***Storm water Quality Improvements.***

*Storm water management improvements disbursed through the Plan Area provide treatment to runoff before it enters the natural drainage conveyance systems in open space areas. In addition, by integrating the storm water management system throughout the Plan Area, individual parcels can provide specific storm water management elements that respond to the particular site conditions. This will promote the removal of various potential pollutants from each parcel before they are discharged into the drainage system. The following standards will apply to development projects.*

1. *During construction, BMPs shall be provided to stabilize soils in place and minimize the amount of sediment entering the storm drain system and drainageways. BMPs shall generally consist of a combination of the following measures: minimizing soil disturbance, hydroseeding, fiber rolls, inlet protection, and stabilized construction access.*

2. *After construction, regional water quality facilities identified in the Master Project Drainage Report shall be constructed concurrently with the backbone drainage infrastructure for permanent water quality treatment.*

3. *Development projects shall provide on-site, site-specific post-construction water quality treatment facilities to capture and remove the pollutants before they are discharged from the site. Water quality treatment facilities shall generally consist of a combination of the following measures: vegetated swales, infiltration trenches/basins, filterstrips, sand/oil separators, trenchdrains, and porous pavement.*

**Policy 4.25** ***Low-Impact Development Design.***

*To the extent feasible, site-specific development projects shall incorporate low-impact development design strategies that may include:*

1. *Minimizing and reducing impervious surfaces of site development (eg, roadways, sidewalks, driveways, parking areas, and rooftops);*
2. *Breaking up large areas of impervious surface and directing flows from these areas to stabilized vegetated areas.*

#### 4.5.1 GROUNDWATER RECHARGE

Groundwater supplies are recharged by rainwater that percolates through to the saturated zone of the soil. The principal means of groundwater recharge in the Sacramento Valley is accomplished within the streambeds that cross the valley. The rate and quality of rainwater reaching the saturation zone depends on a number of factors, such as the amount and duration of precipitation, soil type, moisture content of the soil, and vertical permeability of the unsaturated zone. Urban land uses also affect groundwater recharge by reducing the amount of permeable surface, which limits the amount of rainwater able to percolate into the soil. In addition, groundwater levels can be affected by pumping for agriculture and urban uses.

The entirety of the creek system will remain in place and will continue to serve as the principal means of groundwater recharge in this area. The land use design prepared for the Plan Area aims to reduce the effects of urbanization on groundwater recharge by designating 692.8 acres (13.2% of the Plan Area) as open space and preserving existing drainage ways.

**Goal 4.10** Conserve and preserve water quality within the Placer Vineyards Plan Area.

**Policy 4.26** *Site grading will be under taken and controlled so that sediment runoff is minimized. In locations approved by the County, detention basins may be located in open space areas so as to minimize increases in peak flows from the site. The basins may facilitate ground water recharge, but to a limited degree because of the predominance of clay soils in the area. To minimize runoff, paved parking areas will be designed to provide the minimum amount of paving area necessary to meet required parking and circulation standards. The following standards apply to site-specific development projects.*

1. *Storm water management plans will be prepared that comply with all standards and requirements of the National Pollutant Discharge Elimination System (NPDES) and the grading, erosion, and improvement standards in the Placer County Stormwater Management Plan.*
2. *Grading plans submitted for Placer County review and approval will contain an erosion and sediment control plan that includes erosion control measures to protect waterways from erosion and debris during and after construction activities.*
3. *Grading plans will be designed to minimize run-off and the area of disturbance during construction.*
4. *A Storm Water Pollution Prevention Plan (SWPPP) will be prepared and implemented for site-specific projects.*
5. *Concurrent with construction of site improvements, stormwater BMPs will be constructed and maintained in accordance with the SWPPP as approved by the Central Valley Regional Water Quality Control Board.*
6. *Drainage reports will be prepared for site-specific projects and shall comply with the Placer County Storm Water Management Manual, Land Development Manual, and Placer Vineyards Master Project Drainage Report.*

**Policy 4.27** **Impacts on Streams and Riparian Areas.**

*Impacts on Curry Creek and Dry Creek shall be avoided through the conveyance of open space buffers and the location of adjacent land uses. A 100-foot setback shall be provided for perennial streams (CurryCreek). The majority of the active floodplain of Dry Creek lies on the opposite bank and not within the reach of Dry Creek adjacent to the Plan Area. Adequate open space protection has been designated in the Specific Plan to mitigate impacts on Dry Creek. Implementation of the Specific Plan will also ensure that there would be no net loss of riparian habitat (seePolicy4.17).*

**Policy 4.28** *Construction of stream crossings or other improvements in the Dry Creek and Curry Creek corridors will be kept to the absolute minimum necessary and will meet the following standards:*

1. *A Streambed Alteration Agreement will be obtained from the DFG before commencement of construction of stream crossings.*
2. *Areas adjacent to finished improvements in Curry Creek and Dry Creek that are disturbed during construction activities will be hydroseeded and revegetated.*
3. *Disturbed areas in Curry Creek and Dry Creek not actively being developed will be planted, mulched, or otherwise protected by an acceptable means for the duration of the winter.*
4. *Construction activities will not be permitted within Curry Creek or Dry Creek that would result in disturbed areas being left unprotected between October 15 and March 15.*
5. *Construction roads crossing creek systems will be used only when necessary and other access routes are not feasible.*
6. *Construction roads crossing creek systems will incorporate culverts If roads remain in place during the winter season.*
7. *Erosion control measures will be in place before the onset of the rainy season, but no later than October 15 during construction.*
8. *Permanent roadway stream crossings will be designed for a 100-year flood event.*
9. *Stream crossings will be designed with approaches as close to a right angle wherever feasible.*
10. *Stream crossings will be designed to reduce erosion and stream degradation by the placement of structures.*
11. *Rock energy dissipaters or other Placer County approved methods will be used at outflow points of all culverts.*

## ENVIRONMENTAL RESOURCES

**Policy 4.29** *Developers shall provide a schedule of construction projects to Placer County. Placer County will provide inspection of construction sites to ensure that erosion control measures are operative through the winter period.*

**Policy 4.30** *Maintenance access easements will be dedicated to Placer County on all portions of open space and stream corridors as identified on the tentative maps of development projects and approved by the County.*

**Policy 4.31** *Maintenance access easements will include areas required for preserving trees, special-status species habitat, and wetland resources.*

**Policy 4.32** *Use of low-water-consumption plant materials and irrigation systems will be encouraged by Placer County and the following standards will be met and implemented by site-specific development projects.*

1. *Where available and feasible, recycled water will be used to irrigate all parks, schools, and public rights-of-way. Irrigation equipment shall be compatible with the use of reclaimed water.*
2. *Low-volume spray irrigation systems shall be utilized for turf and ground cover areas and drip irrigation systems for shrubs and trees.*
3. *Where recycled water is available, water-intensive landscaping may be used.*
4. *Landscaping in improved common areas will incorporate drought-resistant varieties where practical and consistent with Placer County design guidelines.*
5. *Landscaping within medians should be by subsurface drip irrigation systems.*

**Policy 4.33** *Use of currently available water conservation devices will be encouraged by Placer County in all existing development. To accomplish this, Placer County will meter the use of potable water, and new construction must meet the following standards.*

1. *Water-conserving design and equipment will be required in all new construction.*
2. *Recycled water will be used for irrigation where feasible.*

## 4.6 SOILS

Soils in the Plan Area generally consist of clays, loams, and sandy loams. For additional information regarding soil types and characteristics within the Plan Area, refer to the Placer Vineyards Specific Plan EIR Appendix L.

Soils are categorized by the Natural Resources Conservation Service (NRCS) for their potential use as agricultural land. The California Department of Conservation (CDC) Farmland Mapping and Monitoring program uses NRCS categories to prepare “Important Farmland Maps.” Most of the Plan Area, approximately 3,657 acres of farmland, are characterized as of “Local Importance” (3,401 acres) or “State-wide Importance” (256 acres) by the CDC. Specific Plan policies will preserve open space land elsewhere in Placer County to offset development of these lands.

**Goal 4.11** *Minimize impacts on important farmland through the dedication of other open space land.*

**Policy 4.34** *Require the preservation of other open space / agricultural lands elsewhere in Placer County at a ratio of 1 acre of land for each acre of land affected within the Plan Area.*

**Policy 4.35** *No additional agricultural land dedications shall be required beyond the 1:1 open space dedication required under Policy 4.34 so long as a substantial portion, as determined by County staff, of the open space lands acquired are:*

1. *In agricultural production or have the potential to support agriculture;*
2. *Undeveloped and have an NRCS soils classification of the same or greater value than lands being impacted within the Specific Plan project; or*
3. *Undeveloped and have the same or higher value CDC categorization as lands being impacted within the Specific Plan project property at issue.*

*In-kind mitigation is not required for agricultural land developed within the project area.*

## 4.7 ENERGY CONSERVATION

The Specific Plan's land use patterns and transportation systems are designed to encourage efficient energy use through non-motorized transportation and the close proximity of residential uses to jobs and services.

**Goal 4.12** Encourage efficient energy use and conservation.

**Policy 4.36** *All residential units will be developed in compliance with State of California Title 24 energy conservation measures.*

**Policy 4.37** *Use of passive and active solar devices such as solar collectors, solar cells, and solar heating systems, integrated into the building designs, are encouraged.*

**Policy 4.38** *Building and site design should take into account the solar orientation of buildings during design and construction.*

## 4.8 AIR QUALITY

The Plan Area is located in the jurisdiction of the Placer County Air Pollution Control District (PCAPCD), which is the agency responsible for monitoring and regulating air pollutant emissions from stationary, mobile, and indirect sources within Placer County. Development within the Plan Area could affect air quality in Placer County and the greater Sacramento Valley. Air quality in the Sacramento Valley affects the quality of life for all residents living in the Sacramento region. The primary factors contributing to air quality in the vicinity of the Plan Area include:

- ◆ Climatic variances, temperature inversions, and low wind speeds;
- ◆ Vehicle emissions; and
- ◆ Mobile and stationary pollutants generated by localized urban development.

Vehicle trips are a major contributor to air pollution, and the number of vehicle trips is affected by the spatial design of land uses and destinations within the community. The Specific Plan includes three major features that help reduce or minimize impacts on air quality.

First, the Plan Area includes three centralized mixed-use village cores that provide neighborhood commercial uses to encourage pedestrian/bicycle use between surrounding residential areas and the village core land uses.

Second, the Specific Plan improves the regional balance of housing and jobs. Housing opportunities made available closer to employment encourage fewer long-distance commutes, consistent with the objectives of the SACOG Blueprint Plan. To this end, SACOG has designated the Placer Vineyards Plan Area as a major regional development opportunity to improve the jobs/housing balance.

Third, the land use pattern and transportation system also facilitates the use of alternative transportation choices throughout the Plan Area. The plan provides for a future bus rapid transit route and transit node within the village center along Watt Avenue. The Plan also uses an extensive bike and pedestrian system along roadways and major open space corridors, linking residences to the bus rapid transit system, the village centers and town center, and public facilities. A street pattern of multiple and parallel routes between destinations minimizes traffic congestion and facilitates residents to combine vehicle trips into one route.

**Goal 4.13** Minimize air quality impacts on the Placer Vineyards area and the region.

**Policy 4.39** *Local area source emissions shall be minimized through a variety of strategies:*

1. *Promote low-emission energy use by requiring building design features that accommodate and encourage use of alternative energy sources.*
2. *Promote low-emission energy use by incorporating landscaping conducive to passive solar energy uses:*
  - a. *Buildings that are oriented in a south-to-southwest direction, where feasible;*
  - b. *Deciduous trees that are planted on the west and south sides of structures;*
  - c. *Landscapes with drought-resistant species and ground covers rather than pavement to reduce heat reflection; and*
  - d. *Maximum parking lot shading at all non-residential developments.*

**Policy 4.40** *Provide, on a project-specific basis, adequate buffers designed to separate emission and nuisance sources from residential uses, consistent with the Placer County General Plan.*

**Policy 4.41** *Construction activities will comply with all requirements of grading permits and PCAPCD.*

**Policy 4.42** *PCAPCD may replace or supplement air pollution control measures for individual projects as new technology and feasible measures become available over the course of the Plan buildout.*

## ENVIRONMENTAL RESOURCES

### 4.9 VECTOR CONTROL AND MOSQUITO ABATEMENT

The purpose of this section is to provide guidelines that prevent new mosquito breeding sources and to apply the necessary measures that support the agencies involved in the surveillance, management, and elimination of mosquito breeding sources. Vector sources such as creeks, wetlands, vernal pools, and other naturally occurring habitat along with artificial sources—agricultural, industrial, and residential sources—will be routinely checked and monitored for mosquito production.

**Goal 4.14** Protect public health and safety by preventing the creation of mosquito breeding areas through proper drainage and routine surveillance of standing water sources for mosquito production.

**Policy 4.43** *Grading shall be performed in a manner to prevent the occurrence of standing water or other are as suitable for the breeding of mosquitoes or other vectors. Water detention and related surface water conveyance features shall also be designed to prevent the breeding of mosquitoes.*

**Policy 4.44** *The Placer Mosquito Abatement Districts shall be granted access in perpetuity to perform vector control in all common areas, including drainage, open space corridors, and park areas. Such access shall be a condition of approval of all tentative maps approved within the Plan Area.*

### 4.10 NOISE

A major source of noise affecting the Plan Area is vehicular traffic along arterial roadways. Other local roadways also contribute to noise in the environment. The Specific Plan will allow for development of residential uses (which are considered to be noise-sensitive users) adjacent to highly traveled roadways. As development of residential land uses occurs in the Plan Area, individual projects will be analyzed for potential noise impacts.

In most cases the edge treatments and orientation of buildings along arterial streets will reduce outdoor noise levels to an acceptable level (60 decibel day-night average noise level, community noise equivalent level [60 dB DNL] or less). In those instances where the noise level is in excess of the 60 dB DNL standards, design practices shall be implemented to reduce noise levels in outdoor use areas to less than 60 dB DNL.

To reduce noise levels, sound walls, berms, and other devices need to be constructed to buffer noise created by vehicles on adjacent residential land uses. However, the Placer County General Plan discourages the use of sound walls for noise mitigation. The General Plan encourages the use of setbacks, building orientation, and other alternatives to sound walls. The General Plan further provides that, where noise mitigation measures are required to achieve adopted standards, the emphasis shall be placed upon site planning and project design. The use of noise barriers shall be considered only after all other practical design-related noise mitigation measures have been integrated into the project. The policies of this chapter and of Chapter V, “Transportation and Circulation,” and Chapter VI, “Community Design,” of this Specific Plan provide the means and methods to satisfy these General Plan requirements.

**Goal 4.15** Minimize noise impacts on residential land uses.

**Policy 4.45** *Edge treatments and building orientations along arterial streets will reduce outdoor noise levels to 60 dB DNL or less for residential uses and 70 dB DNL for commercial uses such as offices. In those instances where the noise level is in excess of the standard, design practices shall be implemented to reduce noise levels in outdoor use areas.*

1. *Future residential or sensitive development along arterials and collectors shall not exceed County noise standards. Creative site planning shall be the primary means to achieve a 60 dB DNL noise level at the outdoor use area (i.e., side and rear yards, patios, balconies, decks, or enclosed courtyards). When necessary, building facades and noise barriers may be placed between the arterial roadway and the outdoor use areas (see Policy 6.44, “Edge Treatments for use in Low-and Medium-Density Residential Area.”).*

2. *Commercial uses along Baseline Road and Watt Avenue and some residential uses along interior arterial streets, may be exposed to excessive noise levels. Where it is not possible to reduce noise in outdoor activity areas to 60 dB DNL or less in residential developments using a practical application of the best available noise reduction measures, an exterior noise level of up to 65 dB DNL may be allowed, provided that available noise level reductions have been implemented and interior noise levels are not in excess of 45 dB DNL. Acoustical analyses shall be prepared for all uses exposed to levels in excess of “normally acceptable” noise levels to show how both the outdoor uses areas and indoor noise thresholds shall be met in these locations.*
3. *All residences, hotels, motels, churches, nursing homes, and theaters uses exposed to a noise level in excess of 60 dB DNL will require sound-rated windows, added wall insulation, and mechanical ventilation capable of achieving the indoor noise requirements of 45 dB DNL, as determined by an acoustical analysis.*
4. *To determine compliance with noise standards, site specific acoustical analyses shall be required as a part of the Subsequent Conformity Review process, during the submittal of tentative subdivision designs and grading maps. Acoustical analyses shall be used to determine appropriate noise attenuation measures (i.e. setbacks, berms, building orientation, noise walls and other noise mitigation measures within the Placer County General Plan Noise Element and the design guidelines found under Section 6.4.3 “Walls, Fences, and Screening” of this Specific Plan) required to reduce traffic noise to levels that meet County noise level standards.*
5. *“The Landscape Master Plan” that will be subsequently prepared for the project shall include the design of noise attenuating features within the landscape setbacks and landscape corridor lots in the Plan Area, especially along Baseline Road, Watt Avenue, and Dyer Lane, where the greatest noise impacts are anticipated to occur.*
4. *Mechanical equipment noise at commercial and residential areas must be controlled. Methods may include quiet equipment, sound-attenuating enclosures, and noise barriers.*
5. *Full disclosure shall be required for all residential uses that are adjacent to, or directly across from schools, houses of worship, neighborhood parks, playgrounds, nightclubs, bars, and restaurants with live music and entertainment venues. The disclosure should state the typical hours of operation and noises associated with the use.*
6. *Additional acoustical analysis may be required for specific noise-generating activities that have the potential to adversely affect adjacent residences or other noise-sensitive uses (e.g., hospitals, retirement homes, day-care centers, and schools). The analysis should identify the potential noise level and the means by which outdoor and indoor noise levels can be controlled to achieve the acceptable standards under the County Noise Ordinance.*

**Policy 4.47** *Construction noise shall be controlled to meet applicable County codes and minimize annoyances on surrounding land uses. Construction noise abatement is critical in later phases of Placer Vineyards development when portions of residential neighborhoods are already in place. Mitigation measures to reduce constructing noise impacts may include the following:*

1. *Construction noise emanating from any construction activities for which a grading and building permit is required is prohibited on Sundays and federal holidays and shall only occur:*
  - a. *Monday through Friday, 6 a.m. to 8 p.m. (during Daylight Savings Time);*
  - b. *Monday through Friday, 7a.m. to 8p.m. (during Standard Time); and*
  - c. *Saturday, 8 a.m. to 6 p.m.*
2. *Truck traffic shall be routed through less noise-sensitive areas.*

**Policy 4.46** *Impacts of noise-generating uses will be minimized. Noise attenuation strategies shall be incorporated into all potential noise generating uses, and may include the following:*

1. *Outdoor use spaces shall be located behind buildings so that the building mass shields noise-sensitive uses from the noise sources.*
2. *Noise barriers shall be constructed between commercial uses and residences.*
3. *Limitations on hours of operation, maximum sound levels, and types of uses may be placed on the proposed uses of amplified sound at schools, parks, bars, restaurants, clubs, and other events.*

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CHAPTER V: TRANSPORTATION AND CIRCULATION



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## 5.1 TRANSPORTATION/CIRCULATION CONCEPTS

### Context

This chapter sets forth the policies for the transportation and circulation systems within the Placer Vineyards Specific Plan Area (Plan Area) with the goal of providing a variety of circulation choices in a safe and efficient manner. Placer Vineyards is designed to accommodate a diverse range of transportation modes—roadways, street corridors, and open space trails—for automobiles, emergency vehicles, buses, and other transit services, and bikeways and paths that accommodate pedestrians, bikers, and equestrians.

Roadway layouts for the Plan Area and their cross section designs are also presented in this chapter. A summary of projected roadway volumes of streets affected by the project and their level of service conditions at major intersections is analyzed in Section 4.7, “Transportation,” in the Placer Vineyards EIR. The proposed intersection designs for Placer Vineyard’s roadways are provided in Appendix C of this document. Pedestrian and bike circulation policies and development standards for travel via on-and off-street trails and their corresponding cross sections are also contained in this chapter. For streetscape corridor and landscape design guidelines, refer to Chapter VI, “Community Design.”

## 5.2 EXISTING TRANSPORTATION CONTEXT

### Regional Highways and Roadways

Three major regional highways and two main arterial roadways provide access into the Plan Area as indicated in Figure 5.1. Major regional highways include State Routes (SRs) 99 and 65 and Interstate 80 (I-80). The thoroughfare streets (identified as major arterial streets in the Placer County General Plan) include Base Line Road, which connects to Sutter County and the City of Roseville and Watt Avenue, which connects south to Sacramento County.

Located approximately 4 miles west of the Plan Area, SR 99, a 4-lane highway that runs north-south from the Central Valley (i.e., Chico and Bakersfield) into Sutter County. SR 99 is heavily traveled by trucks between these destinations.

4 miles east of the Plan Area is I-80, an 8-lane freeway that extends from the San Francisco Bay Area to Nevada and on to the East Coast. I-80 is the primary east-west freeway running through the Sacramento Valley region. SR 99 and I-80 both provide access south to downtown Sacramento, a regional employment center for area residents.

SR 65, generally a north-south road, is located about 5 miles northeast of the Plan Area and provides access from I-80 to SR 70. The highway begins as a 4-lane road in Rocklin and narrows to 2 lanes just north of Twelve Bridges Drive in Lincoln.

Base Line Road is a thoroughfare street running east-west on the northern border of the Plan Area connecting SR 99 and I80. Base Line Road originates in Roseville as a commercial main street, becomes Riego Road as it crosses into Sutter County, and intersects with SR 99.

Watt Avenue is a thoroughfare street running north-south through the Plan Area. Watt Avenue terminates at Base Line Road on the northern border of the Plan Area, extends south into Sacramento County, and provides access to I-80.

### Local Roadways

Local roadways providing access in and around the Plan Area include Fiddymment Road and Walerga Road.

Fiddymment Road is a 2-lane undivided rural roadway that runs north from Base Line Road to Moore Road. It is the primary access road serving the surrounding residential developments.

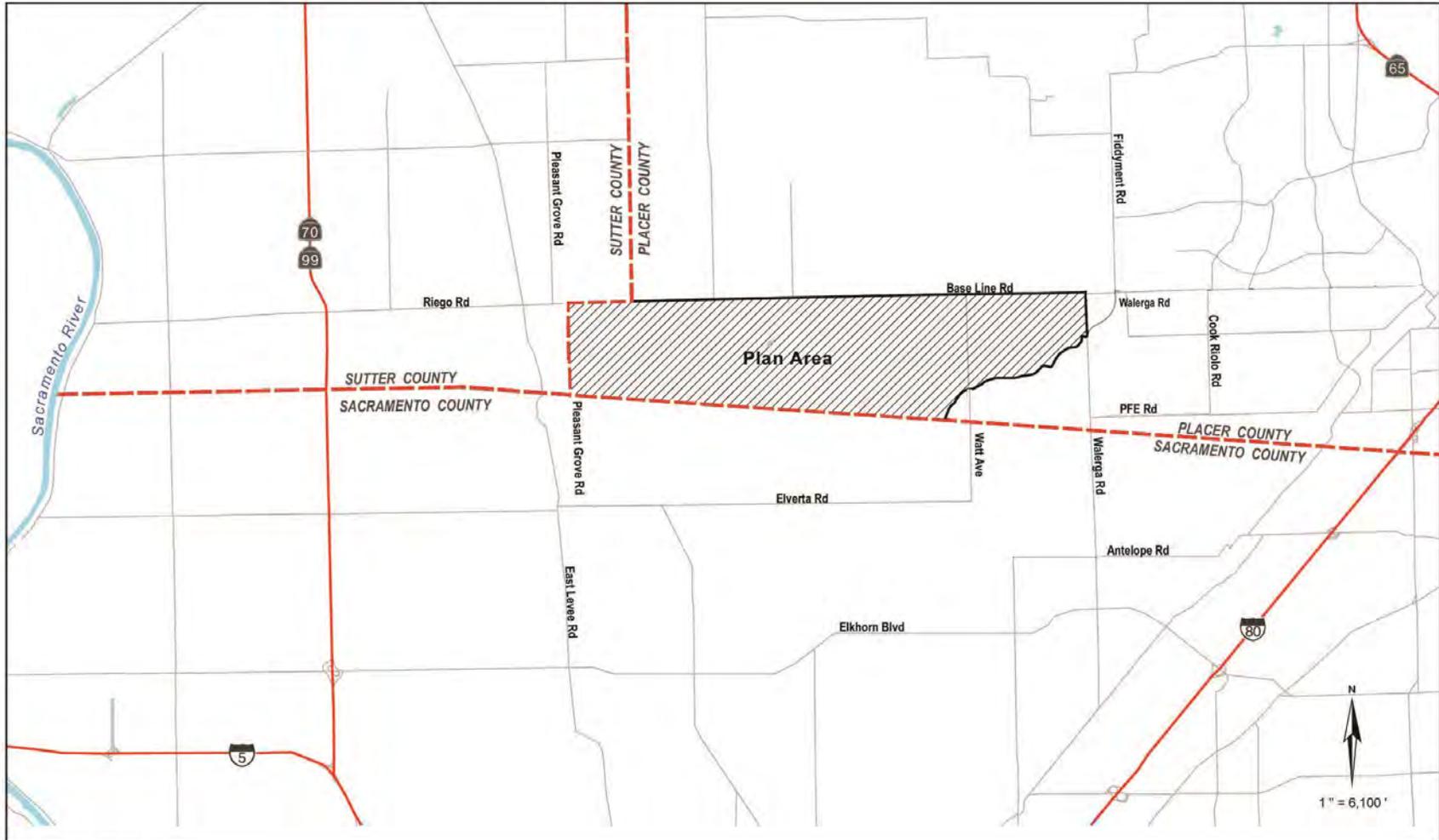
Walerga Road is a 2-lane undivided rural roadway that extends from Roseville Road in Sacramento County and terminates at Base Line Road. Walerga Road provides access between the Antelope area and Roseville.

Several minor rural roadways also provide access to the Plan Area. Locust and Pleasant Grove Roads are 2-lane rural roadways of varying width and shoulder distances. They provide access within the Plan Area from Elverta Road to the south in Sacramento County up to Base Line Road. Palladay Road, Tanwood Road, Dyer Lane, and Colburn Street are minor rural roads that currently provide private access to properties within the Plan Area.



TRANSPORTATION AND CIRCULATION

Figure 5.1 - Local and Regional Roadways



Source: Quad Knopf, 2005

**5.3 TRANSPORTATION AND CIRCULATION ANALYSIS**

Placer County’s traffic model was used to determine the ability of the proposed circulation system to accommodate the anticipated traffic from the Plan Area. This model includes anticipated build-out of the Specific Plan plus development outside of the Plan Area.

**Goal 5.1** Create and maintain a balanced, multi-modal transportation system that provides for the efficient and safe movement of people, goods, and services.

**Policy 5.1 Level of Service Standards.**

*Within the boundaries of the Specific Plan Area and on its boundaries, the Placer Vineyards roadway system will be developed and managed to accommodate a Level of Service (LOS) D. Outside the Specific Plan Area, roadways shall conform to General Plan Standards that require the County to develop and maintain a minimum LOS “C” for rural and urban/suburban roadways, except within 1/2 mile of state highways where the standard shall be LOS “D,” or as provided in Policy 5.2.*

**Policy 5.2 Exceptions to General Plan Level of Service Standards.**

*The County will allow exceptions to these LOS standards where it finds that the improvements or other measures required to achieve the LOS standards are unacceptable based on established criteria. In allowing any exception to the standards, the County shall consider the following factors:*

1. *The number of hours per day that the intersection or roadway segment would operate at conditions worse than the standard*
2. *The ability of the required improvement to significantly reduce peak hour delay and improve traffic operations*
3. *The right-of-way needs and the physical impacts on the surrounding properties*
4. *The visual aesthetics of the required improvement and its impact on community identity and characters*
5. *Environmental impacts, including air quality and noise impacts*
6. *Construction and right-of-way acquisition costs*
7. *The impacts on general safety*
8. *The impacts of the required construction phasing and traffic maintenance*
9. *The impacts on quality of life as perceived by residents*

10. *Consideration of other environmental, social, or economic factors on which the County may base findings to allow an exceedance of the standards*

*Exceptions to the standards will be allowed only after all feasible measures and options are explored, including alternative forms of transportation.*

**5.3.1 PLAN AREA ROADWAY CIRCULATION**

The new roadway circulation system for Placer Vineyards is based on a grid pattern of streets that organizes and provides access into the Plan Area. The Plan Area will be served by a network of public streets organized as a system of thoroughfares, arterials, major collectors, collectors, and local streets, as indicated in Figure 5.2.

**Goal 5.2** Provide a complete network of transportation improvements including thoroughfares, arterials, collector, and local roadways.

**Goal 5.3** Locate roadways, wherever possible, adjacent to open space, public facilities, and multi-family residential and commercial uses to minimize the need for sound walls.

**Goal 5.4** Minimize street widths, orient homes to front on low-volume collector streets, and provide landscape corridors that improve the streetscape environment.

**Goal 5.5** Minimize traffic congestion in Placer Vineyards by discouraging regional thru-traffic on collector and local residential streets.

**Policy 5.3 Roadway System**

*The roadway system shall comply with Figure 5.2, the street sections in Figure 5.3, and the policies and design guidelines presented in this chapter and in Chapter VI, “Community Design.” Figure 5.2 is intended to be a guide to internal roadway traffic needs. As each area is developed, additional roundabouts or traffic signals may be added in the future, as determined necessary by the County for traffic flow and traffic calming. Local streets providing property access are not indicated in Figure 5.2.*

**Policy 5.4 Street Section Design**

*The project shall dedicate rights-of-way of sufficient width to accommodate all future anticipated lanes, including auxiliary lanes, and intersection widening for dual left-turn lanes and free right-turn lanes. Refer also to Chapter VI, “Community Design,” for more specific landscape and streetscape design guidelines and to Appendix C for traffic intersection designs.*

## TRANSPORTATION AND CIRCULATION

1. *The landscape rights-of-way and parkways of all streets shall be dedicated at the same time as street rights-of-way.*
2. *The roadway measurements indicated in Figure 5.3 are measured to the back of the curb.*
3. *Thoroughfares: Base Line Road and Watt Avenue.*  
*The General Plan describes thoroughfares as major arterial streets designed to carry high volumes of thru-traffic with limited travel delay. Base Line Road and Watt Avenue shall be designed as thoroughfares with on-street bike lanes adjacent to 50-foot landscape corridors and meandering bike and pedestrian trails. They shall also include a 20 foot-wide landscape median on Watt Avenue and a 14 foot wide landscape median on Base Line Road. Drive-through breaks shall be provided at intervals along the roadway with medians at a spacing to be determined by the Department of Public Works in consultation with the California Department of Forestry. The County will maintain all median landscaping with funding provided by a County Services CFD.*

*Base Line Road is projected to become 6 lanes divided by a raised median. Figure 5.3, Section A, illustrates the proposed street sections for Base Line Road. At ultimate build-out, Watt Avenue is anticipated to be 6 to 8 lanes with 2 lanes dedicated for bus rapid transit (BRT) right-of-way. Figure 5.3, Section B, illustrates the core and ultimate build-out street sections for Watt Avenue with and without the BRT system.*

*Thoroughfares will provide limited access at the locations indicated in Figure 5.2. No new connections shall be allowed on thoroughfare roads in addition to those shown in Figure 5.2, unless it can be shown that the new connection will benefit overall traffic flows. Access points shall be coordinated with the County to prevent driveways with parking along thoroughfare streets.*

4. *Arterials: Dyer Lane and 16<sup>th</sup> Street.*  
*Arterial streets are high-volume streets with limited, controlled intersections. Their proposed street sections are illustrated in Figure 5.3, Sections D and E. Local and collector streets feed arterial streets to provide linkages between neighborhoods and major retail and employment centers. Arterial streets shall be designed to be 4-lane divided streets with 14-foot medians, on-street bike lanes, and 35-foot landscape corridors with a separated, multi-use trail. Drive-through breaks shall be provided at intervals along the roadway with medians at a spacing to be determined by the Department of Public Works in consultation with the California Department of Forestry. The County will maintain all median landscaping with funding provided by a County Services CFD.*

*Arterials will provide limited access with minimum intersection spacing at approximately every 1/4 mile (1,200 feet). Right-in and right-out access points without median breaks may be provided at a minimum spacing of 600 feet. To the extent possible, the street alignment for Dyer Lane shall be designed to avoid removing and/or disturbing the existing oak trees on Dyer Lane.*

5. *Major Collector Streets: Palladay Road, Tamwood Avenue, and 14<sup>th</sup> Street.*  
*Major collector streets carry moderate traffic volumes. Major collector street sections are illustrated in Figure 5.3, Section F. Major collector streets provide access to individual development areas, neighborhoods, schools, parks, and other community amenities. Major collector streets are generally characterized as 2-lane roadways with on-street bike lanes, parallel parking, and separated tree lined sidewalks. The minimum distance from intersections to driveways shall be 600 feet or a distance determined appropriate by the County for safe access and traffic flow.*
6. *Collector Streets*  
*Collector streets carry light to moderate traffic volumes. Collector street sections are illustrated in Figure 5.3, Sections G, H, TC1, and TC2. Collector streets provide access to individual development areas, neighborhoods, schools, parks, and other community amenities. Collector streets are generally characterized as 2-lane roadways with on-street bike lanes, parallel parking, and separated tree-lined sidewalks. On collector streets located adjacent to a neighborhood commercial development, the minimum distance from intersections to driveways shall be 300 feet or a distance determined appropriate by the County for safe access and traffic flow. The minimum distance from intersections to driveways to residential developments on a collector street may be less than 300 feet if determined appropriate by the County for safe access and traffic flow.*
7. *Commercial Streets*  
*Commercial streets serve parcels within the commercial, business park, power center, and town center areas. They typically do not include Class II bike lanes. Standards for commercial streets not included in the town center are shown in Figure 5.3, Section C. Standards and street sections for the town center commercial streets are provided in Figures 6.9 through 6.11 of Chapter VI, "Community Design."*
8. *Local Streets*  
*Local streets are not located or shown in the circulation diagram. They provide access to neighborhoods within the Plan Area and include non-residential and residential streets. Local streets are low traffic volume, 2-lane roadways with parallel parking, separated sidewalks, and tree-lined landscape parkways. Local streets will be determined in conjunction with specific site development at*

*the time of tentative map submittal. Section for residential streets and cul-de-sacs are provided in Figure 5.3, Sections R1, R2, R3, R5, and R6.*

9. Residential Alleys

*Residential alleys, as shown in Figure 5.3, Section R4 shall be designed to the following standards:*

- a. *Residential alleys are encouraged to be continuous through a block. However, if dead end alleys are allowed they shall be no more than 150 feet long and shall provide visibility from one end of the alley to the other.*
- b. *Landscaping shall be provided, consistent with Policy 6.6-4.*

**Policy 5.5 Private Local Streets.**

*Private local streets will be allowed within a development site subject to the review and approval of the County. Where access is to be provided by private local streets, the private street easement width can be less than 40 feet but cannot be less than the roadway dimensions (from back of curb to back of curb) shown on the street sections in Figure 5.3. Setbacks for private streets shall be measured from the back of the sidewalk. 12.5 feet wide public utilities easements (PUE) shall be provided adjacent to the private road for the benefit of utility companies serving the development. Exceptions to the 12.5 feet wide PUE may be granted upon approval by utility providers, if such easements are not needed.*

*Private streets shall be created as either a private roadway, public utility and emergency access easement, or as a separate parcel to be owned and maintained by a private entity, such as a Homeowner’s Association. Placer County or a CSA will not accept the dedication or maintenance of private streets.*

**Policy 5.6 Locust Road Circulation Study.**

*Prior to approval of improvement plans for any Phased Improvement to be constructed as part of the first Development Phase approved by the County, the Developer and/ or Development Group shall fund a study to be undertaken by the County to identify and review the feasibility of alternatives to retaining Locust Road as a through roadway between Base Line Road and West Town Center Drive. The study shall be funded by developers as provided in the Development Agreement. Any such study shall:*

- 1. *Review the impacts upon the roadway systems in the Specific Plan and adjacent jurisdictions and identify the need for new or additional infrastructure, if any;*
- 2. *Examine the application of strategies contained in the County’s Neighborhood Traffic Management Program and evaluate their effectiveness at addressing residents’ concerns;*

- 3. *Include an analysis of the necessary amendments to the Specific Plan, the County General Plan, and/ or the Dry Creek West Placer Community Plan to implement any such alternatives;*
- 4. *Identify the costs associated with any such alternatives; and*
- 5. *Specify compliance with the California Environmental Quality Act and any other applicable legal requirements.*

*The County shall utilize the study to determine whether modifications to Locust Road are in the best interests of the County. The developers acknowledge that modifications to this Plan, the Plan Area roadway system, and required infrastructure, including the Backbone infrastructure, as the County may determine is necessary to effectuate the intent of this policy, may be required.*

## TRANSPORTATION AND CIRCULATION

### Roadway Design Guidelines

Roadways shall be designed according to the following guidelines:

1. Roads shall be designed for their dual roles as vehicular and non-vehicular transportation corridors with landscape berms or open space parkways, containing bicycle and pedestrian trails.
2. Local roadways shall be located to facilitate local circulation and shall discourage regional thru-traffic. Regional thru-traffic shall be concentrated on Base Line Road and Watt Avenue.
3. Thoroughfares, Base Line Road and Watt Avenue, shall be located at the perimeter of major development areas.
4. A finer grain network of connector streets shall be located to provide convenient access to all land use parcels.
5. East-west connector streets shall generally provide through connections between and through land use areas while north-south connector streets may be more discontinuous, terminating at parks, open space, and neighborhood entries.
6. Multiple points of access to development areas are encouraged, to maximize the number of streets that carry traffic and the distribution of traffic loads from each development area.
7. Neighborhoods should be designed with internal connecting streets to encourage a more open and accessible network for residents and improve the distribution of traffic throughout the roadway network. However, cul-de-sac roads are not excluded within residential areas as long as they are not excessively used.



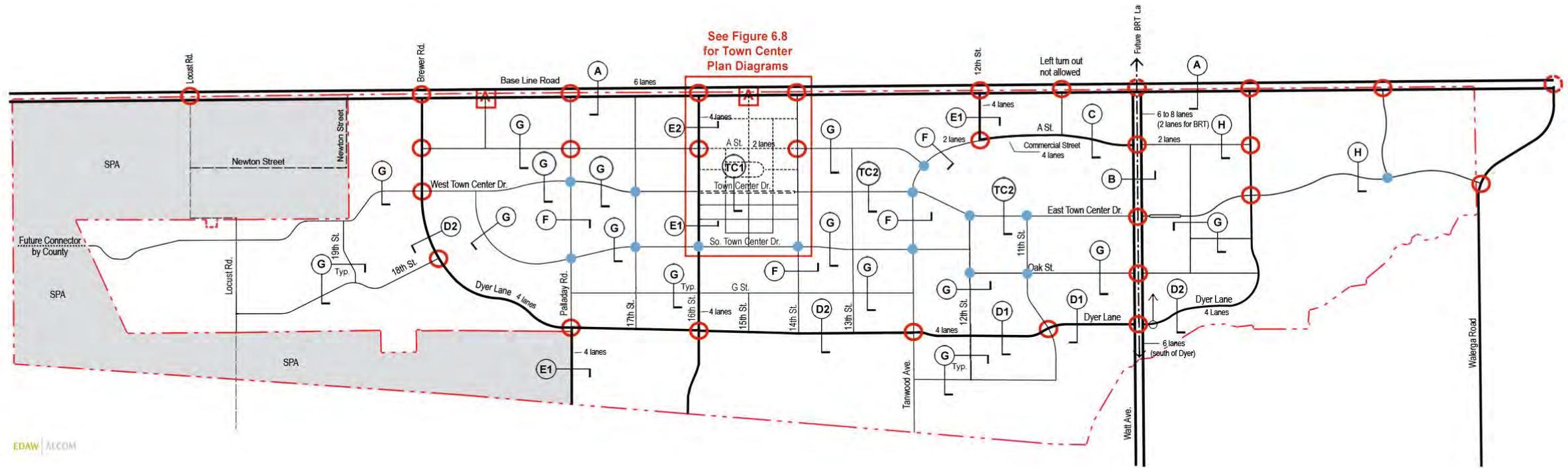
*A street designed for both vehicular and non-vehicular transportation modes*



*A continuous arterial street designed to provide access to neighborhood development*

8. Development areas and commercial sites shall be interconnected to allow for internal circulation and minimize impacts on adjacent arterial roadways.
9. Cul-de-sac roads should be no greater than 800 feet in length.
10. Streetscapes shall be designed in accordance with the design guidelines found in Chapter VI, "Community Design."

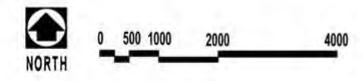
Figure 5.2 – Roadway Circulation Diagram



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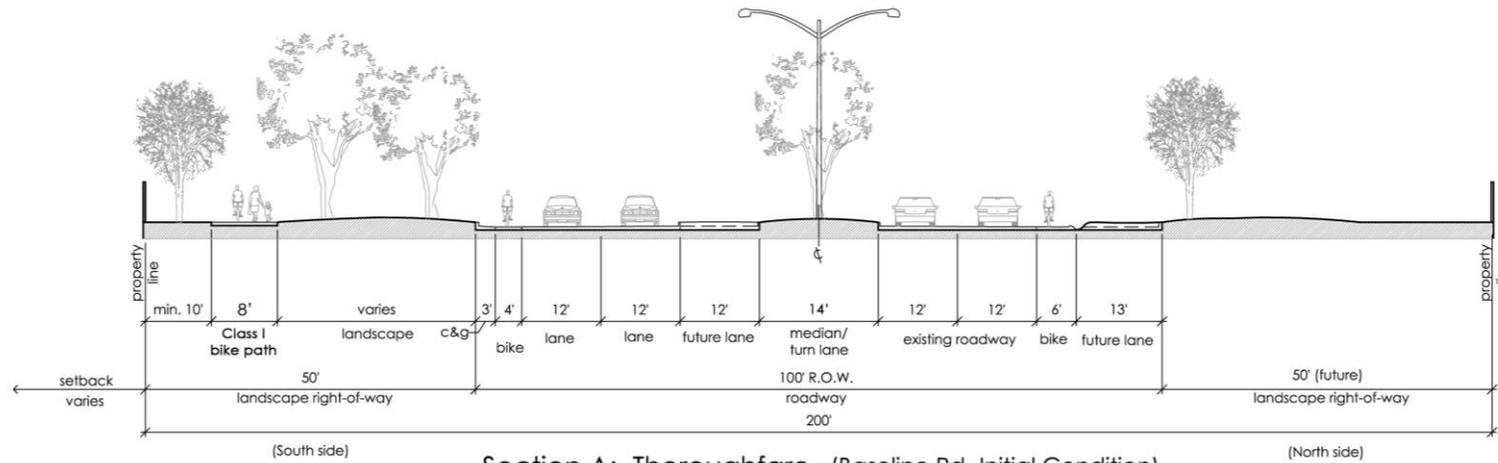
**LEGEND**

- |                          |                    |                          |                         |
|--------------------------|--------------------|--------------------------|-------------------------|
| Thoroughfare             | Town Center Drive  | Future BRT Lanes         | Left In, Right Out Only |
| Major Arterial (4 lanes) | Town Center Street | Roundabouts              |                         |
| Collector (2 lanes)      |                    | Traffic Signals          |                         |
| Existing Road            |                    | Existing Traffic Signals |                         |
| Future Road              |                    | Section Cuts             |                         |



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Figure 5.3 - Roadway Sections – Thoroughfare (Base Line Road)



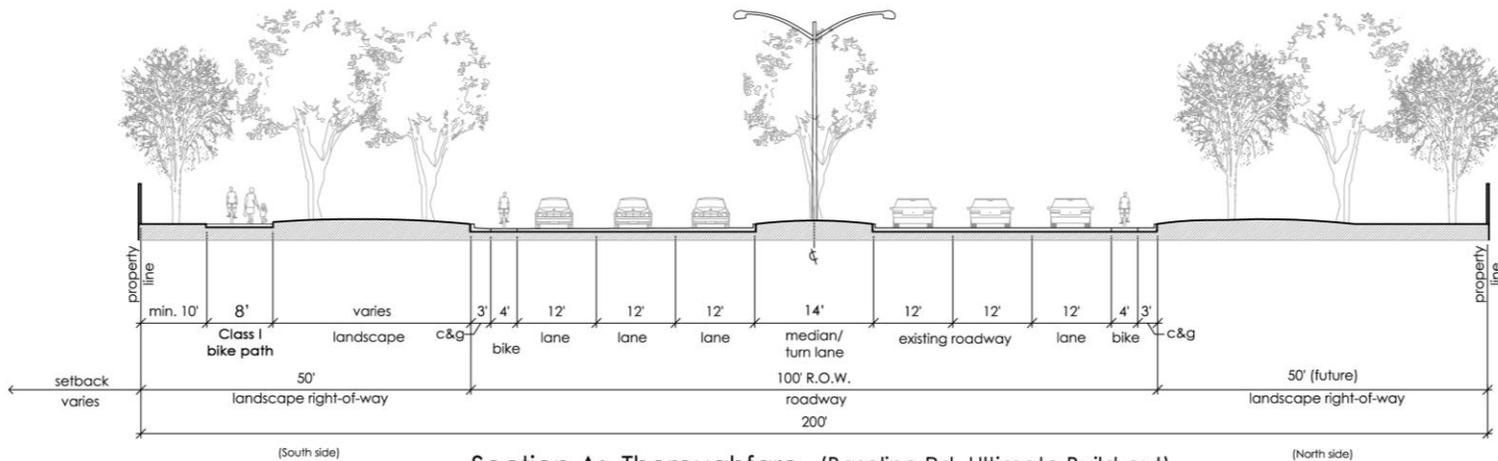
Section A: Thoroughfare (Baseline Rd.-Initial Condition)

LEGEND

landscape	landscaping	bike	bike lane
lane	driving lane	c&g	curb and gutter
R.O.W.	right-of-way	min.	minimum

Notes:

- (1) Backbone infrastructure system



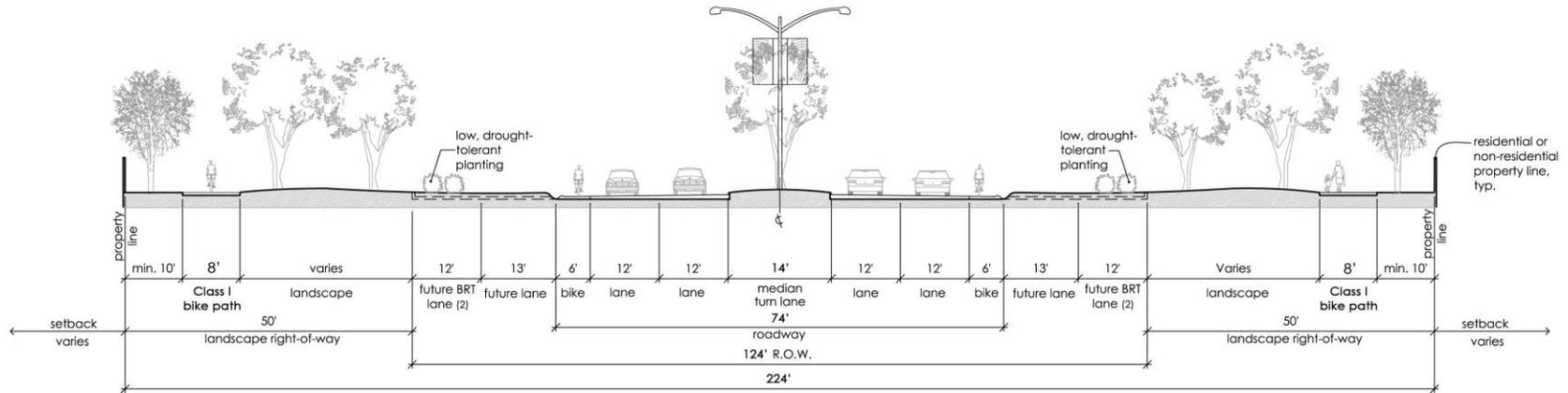
Section A: Thoroughfare (Baseline Rd.-Ultimate Build-out)

Notes:

- (1) Plan Area build-out condition

# TRANSPORTATION AND CIRCULATION

Figure 5.3 - Roadway Sections – Thoroughfare (Watt Avenue Phase 1 and 2)



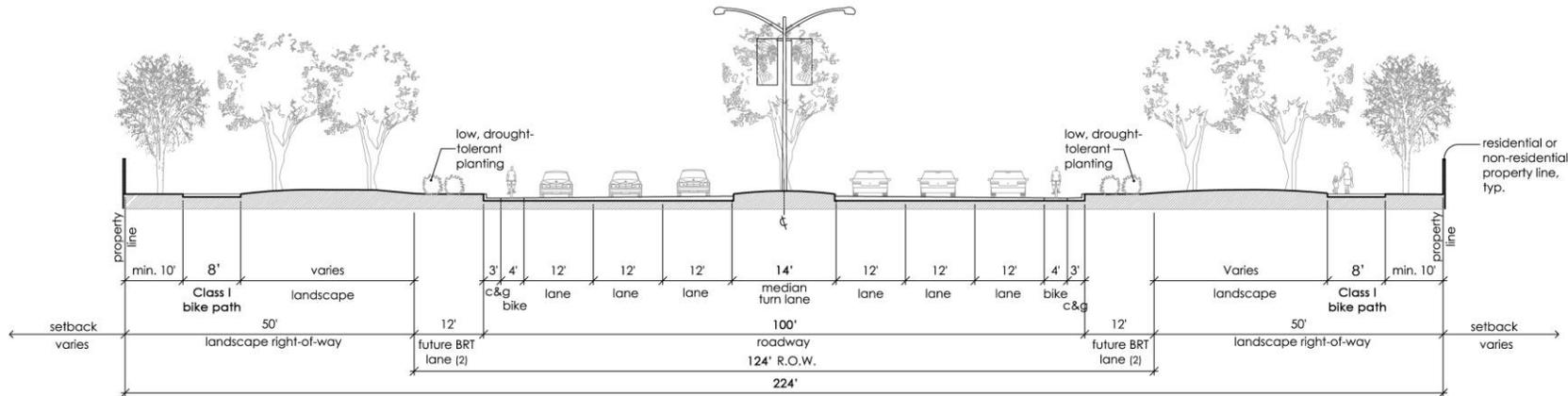
Section B: Thoroughfare (Watt Ave.-Phase 1)

**LEGEND**

landscape	landscaping	bike	bike lane	c&g	curb and gutter
lane	driving lane	BRT	bus rapid transit		
R.O.W.	right-of-way	min.	minimum		

**Notes:**

- (1) Backbone infrastructure system
- (2) Location of transit to be determined in the future

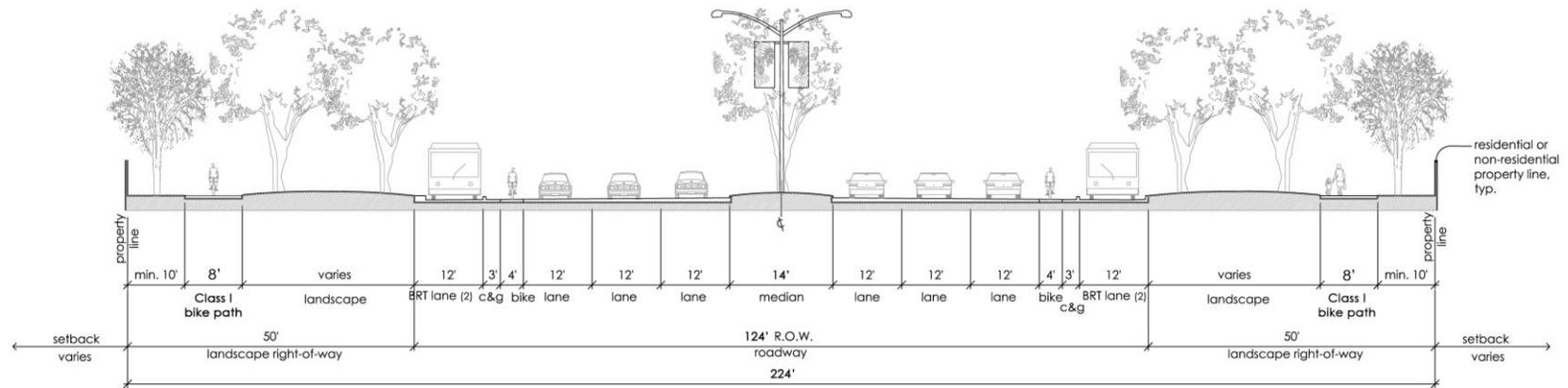


Section B: Thoroughfare (Watt Ave.-Phase 2)

**Notes:**

- (1) Plan Area build-out condition with transit R.O.W. reserved
- (2) Location of transit to be determined in the future

Figure 5.3 - Roadway Sections – Thoroughfare (Watt Avenue Phase 3)



Section B: Thoroughfare (Watt Ave.-Phase 3)

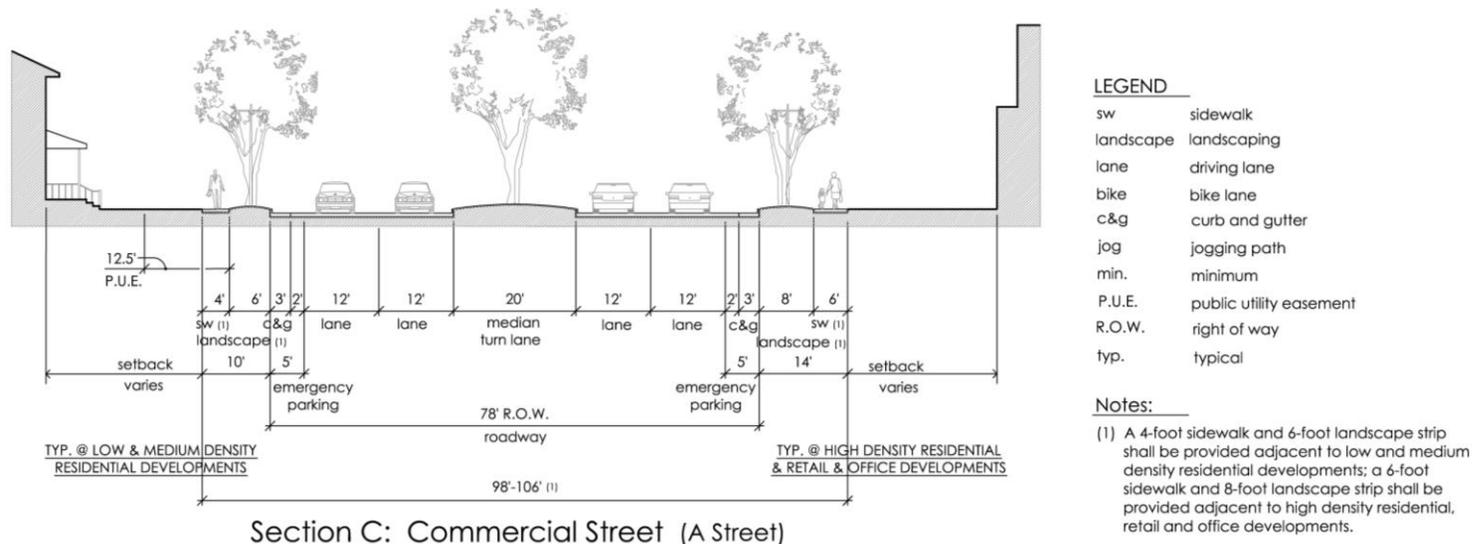
**LEGEND**

landscape	landscaping	bike	bike lane	c&g	curb and gutter
lane	driving lane	BRT	bus rapid transit		
R.O.W.	right-of-way	min.	minimum		

**Notes:**

- (1) Ultimate build-out condition with BRT transit
- (2) Location of transit to be determined in the future

Figure 5.3 - Roadway Sections – Commercial Street



**LEGEND**

sw	sidewalk
landscape	landscaping
lane	driving lane
bike	bike lane
c&g	curb and gutter
jog	jogging path
min.	minimum
P.U.E.	public utility easement
R.O.W.	right of way
typ.	typical

**Notes:**

(1) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.

Figure 5.3 - Roadway Sections – Major Arterial Street (Dyer Lane)

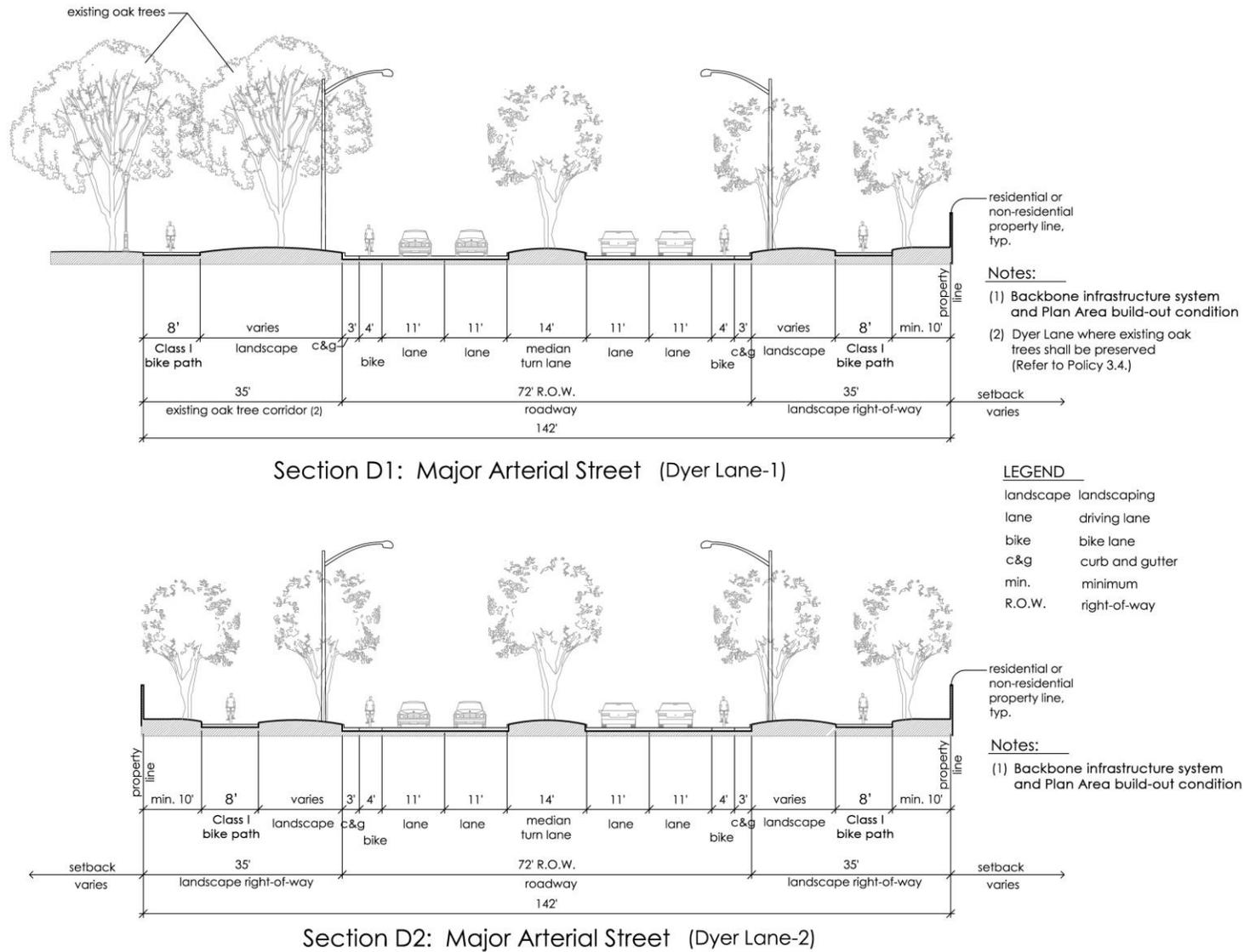
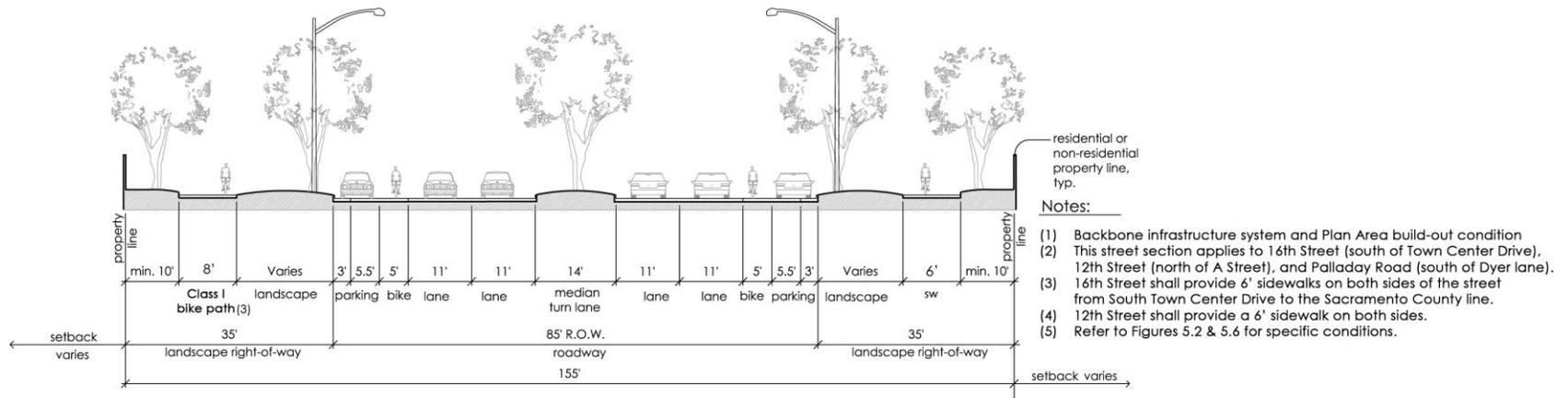


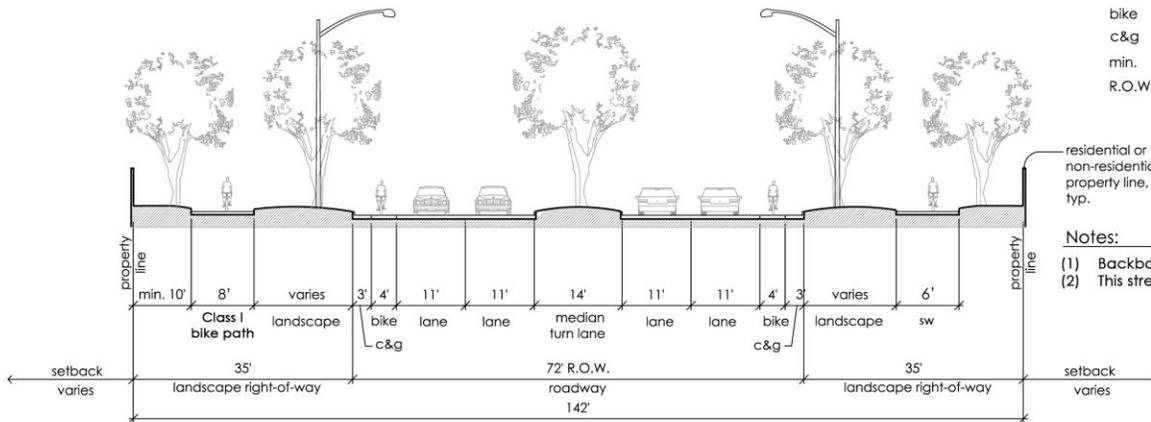
Figure 5.3 - Roadway Sections – Major Arterial Street (16<sup>th</sup> Street)



Section E1: Major Arterial Streets  
(16th Street-South, 12th Street-North, Palladay Road-South)

LEGEND

landscape	landscaping
lane	driving lane
bike	bike lane
c&g	curb and gutter
min.	minimum
R.O.W.	right-of-way



Section E2: Major Arterial Street (16th Street-North)

Figure 5.3 - Roadway Sections – Major Collector Street

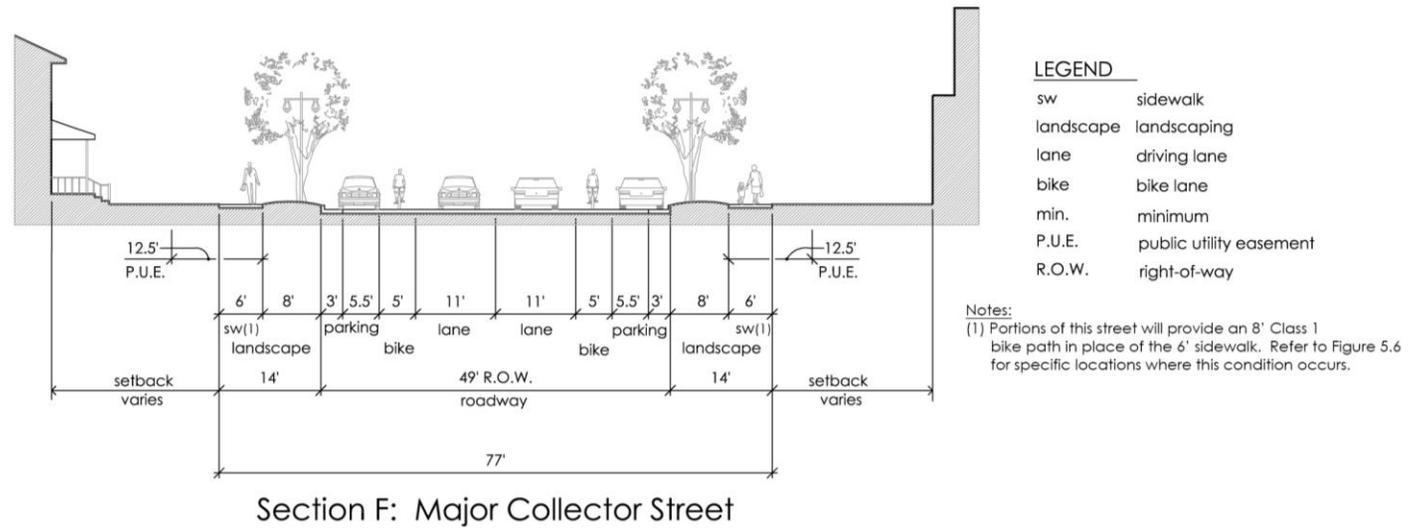
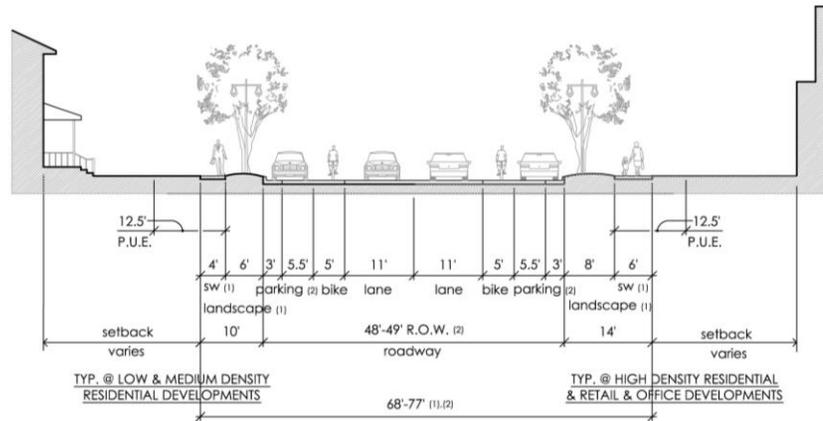
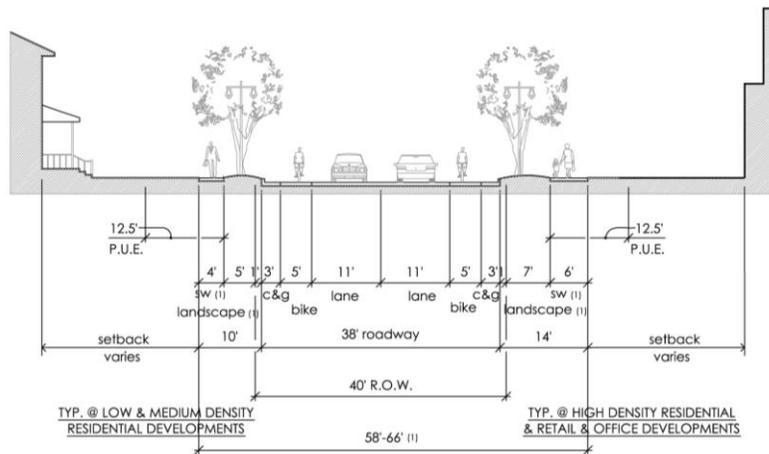


Figure 5.3 - Roadway Sections – Collector Street



Section G: Collector Street



Section H: Collector Street

**Notes:**

- (1) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.  
Portions of this street will provide an 8' Class 1 bike path in place of the sidewalk. Refer to Figure 5.6 for specific locations where this condition occurs.
- (2) Rolled curbs may be allowed subject to County review. Parallel parking lanes shall be 8-feet wide to back of curb at rolled curbs and shall be 8.5-feet wide to back of curb at vertical curbs.

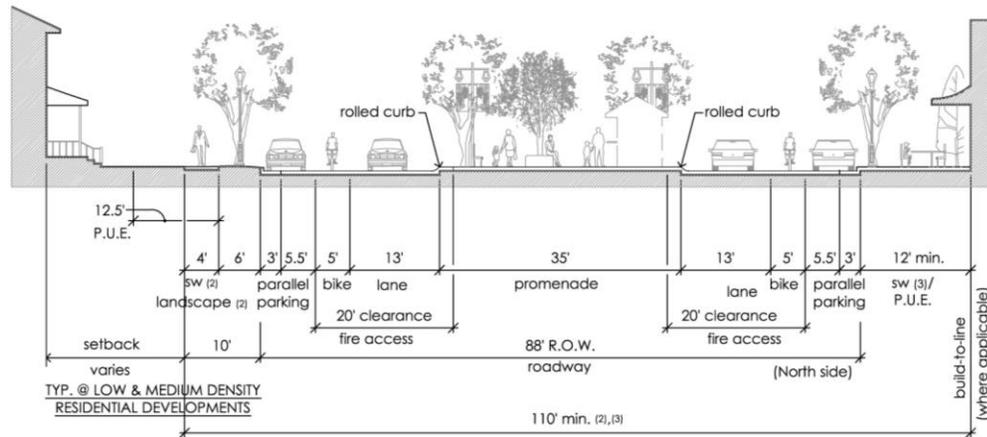
**LEGEND**

sw	sidewalk
landscape	landscaping
lane	driving lane
bike	bike lane
c&g	curb and gutter
min.	minimum
P.U.E.	public utility easement
R.O.W.	right-of-way
typ.	typical

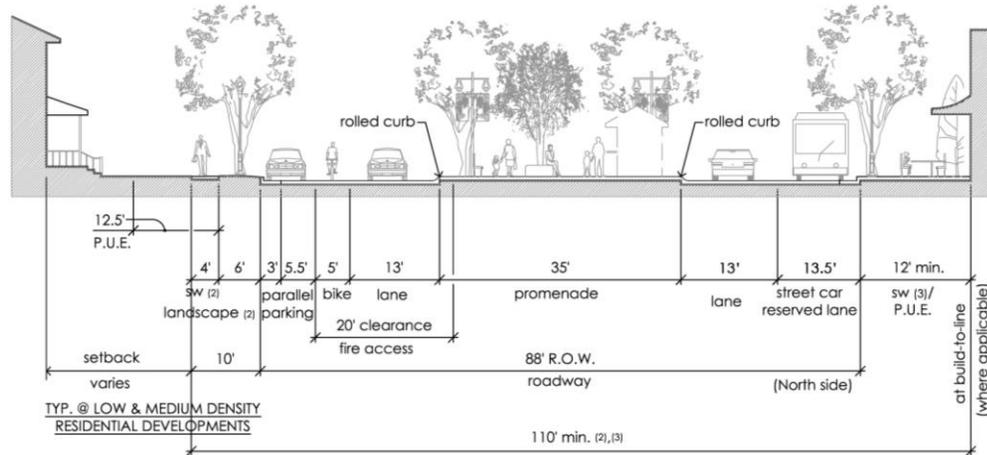
**Notes:**

- (1) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.  
Portions of this street will provide an 8' Class 1 bike path in place of the sidewalk. Refer to Figure 5.6 for specific locations where this condition occurs.

Figure 5.3 - Roadway Sections – Town Center Drive



Section TC1: Town Center Drive (Initial Condition)



Section TC1: Town Center Drive (Build-out)  
Elevation

**Notes:**

- (1) Plan Area initial condition or condition without streetcar
- (2) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.
- (3) The minimum overall sidewalk width in the Town Center shall be 12 feet with a preferred width of 16 feet. Sidewalks fronting retail and office developments shall maintain a minimum 6-foot wide clearance for pedestrian traffic. Also refer to Policy 6.29, "Town Center Sidewalks."

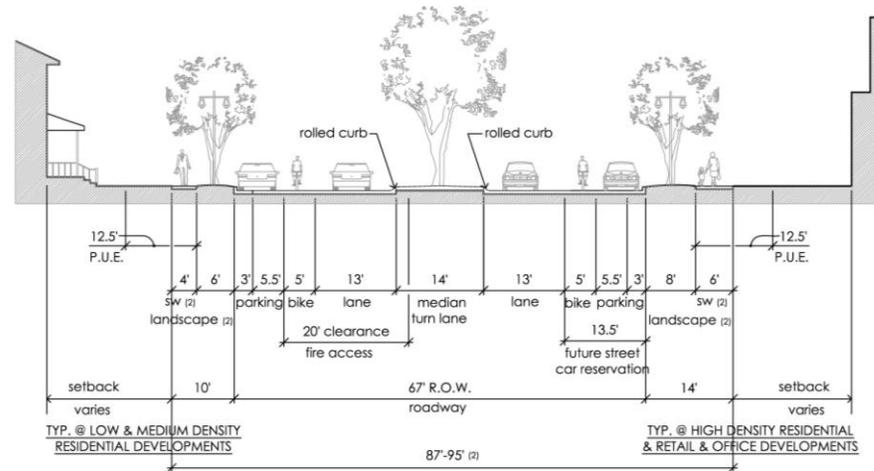
**LEGEND**

sw	sidewalk
landscape	landscaping
lane	driving lane
bike	bike lane
min.	minimum
P.U.E.	public utility easement
R.O.W.	right-of-way
typ.	typical

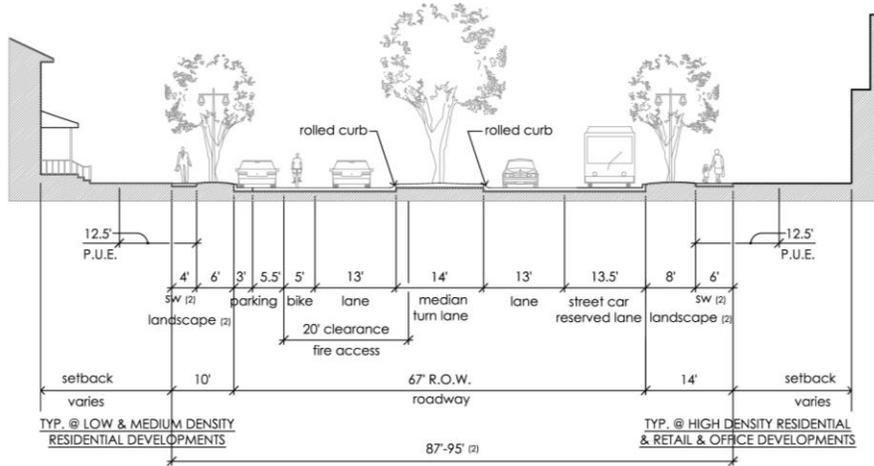
**Notes:**

- (1) Plan Area build out condition with streetcar
- (2) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.
- (3) Sidewalks fronting retail and office developments shall maintain a minimum 6-foot wide clearance for pedestrian traffic. Also refer to Policy 6.29, "Town Center Sidewalks."

Figure 5.3 - Roadway Sections – East Town Center Drive



Section TC2: Town Center Drive (Initial Condition)



Section TC2: Town Center Drive (Build-out)

**Notes:**

- (1) Plan Area initial condition or condition without streetcar
  - (2) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.
- Portions of this street will provide an 8' Class 1 bike path in place of the sidewalk. Refer to Figure 5.6 for specific locations where this condition occurs.

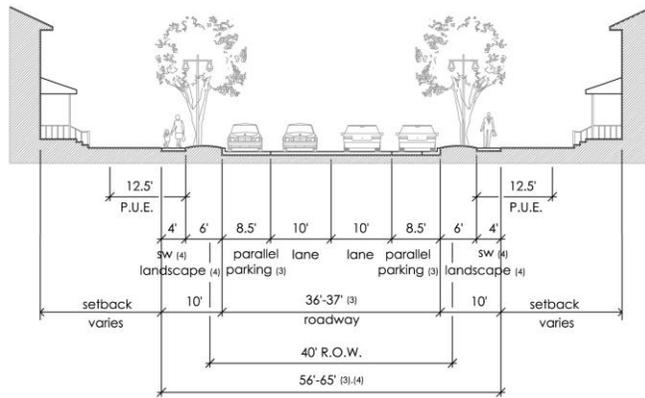
**LEGEND**

sw	sidewalk
landscape	landscaping
lane	driving lane
bike	bike lane
min.	minimum
P.U.E.	public utility easement
R.O.W.	right-of-way
typ.	typical

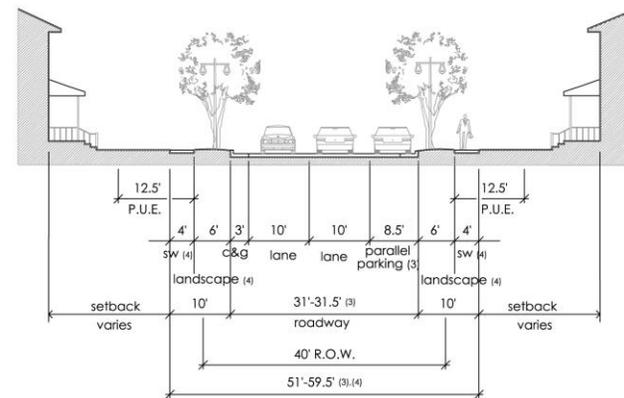
**Notes:**

- (1) Plan Area initial condition or condition with streetcar
  - (2) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.
- Portions of this street will provide an 8' Class 1 bike path in place of the sidewalk. Refer to Figure 5.6 for specific locations where this condition occurs.

Figure 5.3 - Roadway Sections – Residential Streets



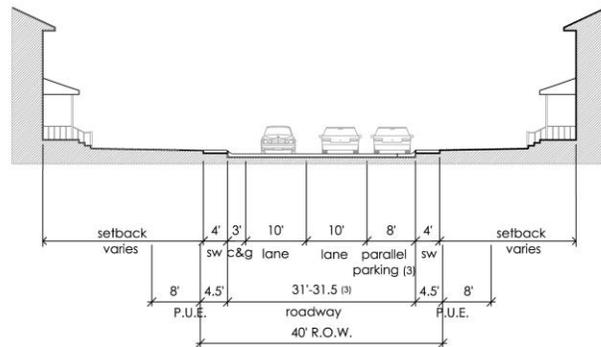
Section R1: Residential Street-Type A<sup>(5)</sup>  
Parking on Both Sides



Section R2: Residential Street-Type B<sup>(2),(5)</sup>  
Parking on One Side

**LEGEND**

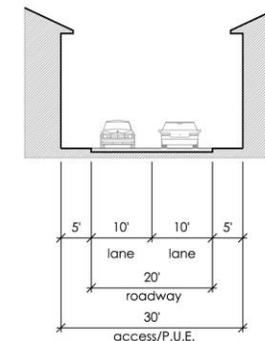
sw	sidewalk	bike	bike lane	c&g	curb and gutter
landscape	landscaping	P.U.E.	public utility easement		
lane	driving lane	R.O.W.	right-of-way		



Section R3: Residential Cul-de-Sac Street<sup>(5)</sup>  
(Ten Units or Less)

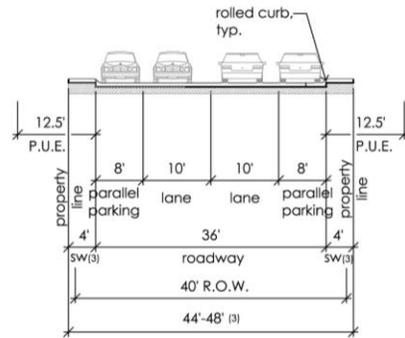
**Notes:**

- (1) A min. 12.5-foot P.U.E. is required adjacent to R.O.W. for all residential streets with exceptions provided upon approval by utility providers.
- (2) Parallel parking shall be provided only on one side of the street for single loaded street conditions, streets adjacent to open space and loop roads. (Refer to Section R2).
- (3) Rolled curbs may be allowed subject to County review. Parallel parking lanes shall be 8-feet wide to back of curb at rolled curbs and shall be 8.5-feet wide to back of curb at vertical curbs.
- (4) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.
- (5) Private residential streets may have a road easement width that is less than 40' but shall not have a roadway width (back of curb to back of curb) that is less than the roadway widths for residential sections R1-R4.

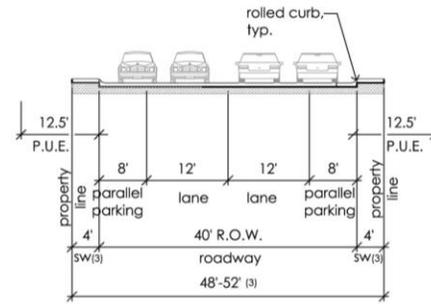


Section R4: Residential Alley

Figure 5.3 - Roadway Sections – Residential Street



Section R5: Internal Residential Street  
(In Age-Restricted Community)



Section R6: Internal Residential Street  
(In Age-Restricted Community)  
(At High Volume Locations)

**LEGEND**

sw	sidewalk	c&g	curb and gutter
landscape	landscaping	P.U.E.	public utility easement
lane	driving lane	R.O.W.	right-of-way

**Notes:**

- (1) A min. 12.5-foot P.U.E. is required adjacent to R.O.W. for all residential streets with exceptions provided upon approval by utility providers.
- (2) Parallel parking shall be provided only on one side of the street for single loaded street conditions, streets adjacent to open space and loop roads. (Refer to Section R2).
- (3) A 4-foot sidewalk shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk shall be provided adjacent to high density residential, retail and office developments.

**5.3.2 TRANSPORTATION IMPROVEMENTS**

Regional circulation improvements are identified by the Placer, Sacramento, and Sutter County General Plans and the latest version of the *Metropolitan Transportation Plan/ Sustainable Communities Strategies*, prepared by SACOG. Planned improvements include expansions to regional roadways, local roadways, regional transit systems, and trail systems needed to accommodate growth anticipated to occur in this region. These facilities will provide capacity to the surrounding local and regional developments. A number of local and off-site intersection improvements will be constructed as part of the Placer Vineyards development to accommodate cumulative traffic levels.

***Policy 5.7 Regional Transportation Improvements.***

*Relative to the traffic impacts generated by the project: The Placer Vineyards Development Agreement ensures that the project will pay for its fair share of transportation improvements.*

***Policy 5.8 Off-site Transportation Improvements.***

*Placer Vineyards shall provide traffic signals and off-site intersection improvements, in conjunction with development in the Plan Area at the following locations:*

1. Riego Road and East Natomas Road
2. Riego Road and Pleasant Grove Road
3. Base Line Road and Pleasant Grove Road
4. Watt Avenue and PFE Road

***Policy 5.9 On-site Transportation Improvements.***

*The Placer Vineyards development shall fund and construct all transportation network improvements, including roadway design, traffic signalization, and traffic calming, necessary to support the new development when and as they are needed.*

***Policy 5.10 Concurrency.***

*Roadway improvements shall be constructed to coincide with the demands of new development as required to satisfy minimum level of service standards, asset by this Specific Plan.*

***Policy 5.11 Local Intersection Improvements.***

*Placer Vineyards shall provide local intersection improvements as guided by Figure 5.2.*

1. *Signalized intersections for the thoroughfares, Base Line Road and Watt Avenue, are provided in Figure 5.2 at the following locations:*
  - a. *On Base Line Road, signalized intersections shall be located on Locust Road, Dyer Lane (west), Palladay Road, 16<sup>th</sup> Street, 14<sup>th</sup> Street, 12th Street, Tanwood Avenue, Watt Avenue, Dyer Lane (east), and Park Street.*
  - b. *On Watt Avenue, signalized intersections shall be provided at ‘A’ Street, Town Center Drive (east), Oak Street, and Dyer Lane.*
  - c. *On Dyer Lane, signalized intersections shall be provided at ‘A’ Street (east and west), Town Center Drive (east and west), 18<sup>th</sup> Street, Palladay Road, 16<sup>th</sup> Street, Tanwood Avenue, and 11<sup>th</sup> Street.*
  - d. *For commercial developments on ‘A’ Street, signalized intersections shall be provided at Palladay Road, 16<sup>th</sup> Street, 14<sup>th</sup> Street, and 12th Street.*
2. *Base Line Road and Watt Avenue intersections shall be planned and designed to accommodate the needs of thru-traffic. This will include traffic synchronization and intersection designs that favor through movements and minimize conflict points. This may also include additional turning lanes or other special features, such as pedestrian amenities that highlight intersection crossings.*
3. *Roundabouts shall be located along the major east-west collector streets and lower volume traffic streets, focused at the intersections of residential neighborhoods.*
4. *Roundabouts shall be considered as an alternative, where all-way stops or traffic signals are indicated in the future (i.e., project build-out).*
5. *The County shall also reserve the right during the large lot or small lot tentative map process to require additional traffic signals or roundabouts, as determined to be necessary for traffic flow or safety.*
6. *The County shall also reserve the right to modify the minimum distance from a street intersection to a development driveway, as determined to be necessary for the traffic flow or safety of a specific site condition.*

## TRANSPORTATION AND CIRCULATION

### ***Policy 5.12 Access within the Development Site.***

*Primary access to development will be avoided on high-volume arterial and thoroughfare roadways, and instead will be provided on collector or neighborhood streets and shall comply with the following standards:*

- 1. Thoroughfares shall provide limited access. No driveways shall be permitted on Base Line Road or Watt Avenue. Access to properties fronting on Base Line Road shall be provided mainly from A Street and to a lesser extent from the roads that connect A Street to Base Line Road. Access to parcels from these connector roads shall be located at sufficient distance from Base Line Roads so as not to impede the flow of traffic or create safety issues.*
- 2. Access to development sites from thoroughfares and arterials allowing for left turns into and out of the sites shall be limited to the identified signalized intersections in the Placer Vineyards Specific Plan, unless otherwise required under future development patterns.*
- 3. Minor right-turn-in and right-turn-out access points may be permitted by the County upon further detailed review and analysis of potential traffic and circulation impacts.*
- 4. See Figure 6.17 in Chapter 6, "Community Design," for conceptual site access designs for neighborhood commercial sites on high-volume roadways.*

### ***Policy 5.13 Minimizing Barriers to Access.***

*The circulation and site plans for individual developments proposed within the Plan Area shall minimize barriers to access by pedestrians, the disabled, and cyclists. Handicap ramps shall be incorporated into the design of all intersections and bicycle racks shall be located convenient to all retail, office, and civic sites.*

### ***Policy 5.14 Fire and Emergency Access.***

*All new development shall be coordinated with the local fire department to ensure that adequate emergency access is provided to all development areas and that emergency access routes are designed to the specification of the Placer County Fire Department.*

### ***Policy 5.15 Sound Walls.***

*Use of sound walls is discouraged. Where sound walls are required because of noise levels and traffic volumes on major streets, screen landscaping and mounding should be provided to minimize their visual impact and create a more attractive streetscape. Refer to Section 4.10 for additional policies related to noise levels and to Section 6.4.3 for wall, fence, and screening techniques and design guidelines.*

**5.3.3 TRAFFIC CALMING DESIGNS**

***Policy 5.16 Traffic Calming Roadway Design.***

*Use of traffic calming roadway design techniques in the design of residential streets and intersections is required. Techniques may include corner bulb-outs at intersections, traffic circles and rotaries, chokers, chicanes, etc. See the chart below and Figure 5.4 for recommended traffic calming designs. In all cases, traffic calming devices shall not restrict access by emergency vehicles or limit emergency response time below the required level of service standard.*

**A. Narrowing and Horizontal Devices**

Traffic Calming Device	Description	Application
<p style="text-align: center;"><b>Neckdown/Bulb-out</b></p>  <p>Source: ITE pedestrian bicycle council</p>	<p>Neckdowns/bulb-outs shorten the crossing distance of intersections and decrease the curb radii, reducing turning vehicle speeds.</p>	<p>Neckdowns/bulb-outs can be used at intersections or midblock locations on most roadway types where the expected average daily traffic is less than 20,000 vehicles per day and the posted speed limit is 35 mph or less.</p>
<p style="text-align: center;"><b>Two-lane choker</b></p>  <p>Source: ITE pedestrian bicycle council</p>	<p>2-lane chokers are midblock curb extensions that narrow a street. Chokers leave the street cross section with 2 lanes that are narrower than the normal cross section.</p>	<p>2-lane chokers should be used at midblock locations only on most roadway types where the expected average daily traffic is less than 20,000 vehicles per day and the posted speed limit is 35 mph or less.</p>

## TRANSPORTATION AND CIRCULATION

### A. Narrowing and Horizontal Devices

Traffic Calming Device	Description	Application
<p><b>One-lane choker</b></p>  <p>Source: <a href="http://www.pedbikeimages.org/">www.pedbikeimages.org/</a> City of Portland Office of Transportation</p>	<p>One-lane chokers narrow the roadway width so that there is only enough width to allow travel in one direction at a time. They operate similarly to one-lane bridges, where cars approaching on one side must wait until all traffic in the other direction has cleared before proceeding.</p>	<p>The example provided is the entryway into a residential development. One-lane chokers should be used at midblock locations only on lower order residential streets where the expected average daily traffic is less than 3,000 vehicles per day and the posted speed limit is 30 mph or less.</p>
<p><b>Center Island Narrowing</b></p>  <p>Source: <a href="http://www.pedbikeimages.org/">www.pedbikeimages.org/</a> City of Portland Office of Transportation</p>	<p>These raised islands located along the centerline of a street narrow the travel lanes at that location. They can also be used at intersections to provide refuge for pedestrians when fitted with a gap for pedestrians to walk through.</p>	<p>Center island narrowings can be used on most roadway types where the expected average daily traffic is less than 20,000 vehicles per day and the posted speed limit is 35 mph or less.</p>
<p><b>Chicane</b></p>  <p>Source: <a href="http://www.pedbikeimages.org/">www.pedbikeimages.org/</a> City of Portland Office of Transportation</p>	<p>These curb extensions alternate from one side of the street to the other, forming S-shaped curves. Chicanes can also be created by alternating on-street parking between one side of the road and the other</p>	<p>Chicanes should be used at midblock locations only. Application where the expected average daily traffic is less than 5,000 vehicles per day and the posted speed limit is 35 mph or less is appropriate.</p>

A. Narrowing and Horizontal Devices

Traffic Calming Device	Description	Application
<p><b>Lateral Shift</b></p>  <p>Source: <a href="http://www.pedbikeimages.org/">www.pedbikeimages.org/</a></p>	<p>These are curb extensions or other physical shifts of the roadway on otherwise straight streets that cause deflection.</p>	<p>Lateral shifts should be used at midblock locations only. Application where the expected average daily traffic is less than 5,000 vehicles per day and the posted speed limit is 35 mph or less is appropriate.</p>
<p><b>Traffic Circle</b></p>  <p>Source: <a href="http://www.pedbikeimages.org/">www.pedbikeimages.org/</a></p>	<p>Traffic circles are raised islands, placed in intersections, around which traffic circulates. Stop signs or yield signs can be used as traffic controls at the approaches of the traffic circle. Circles prevent drivers from speeding through intersections by impeding the straight-through movement and forcing drivers to slow down to yield.</p>	<p>Traffic circles should be used at low volume residential intersections. The combined intersection volume should not exceed 10,000 vehicles per day.</p>
<p><b>Roundabout</b></p>  <p>Source: <a href="http://www.pedbikeimages.org/">www.pedbikeimages.org/</a></p>	<p>Roundabouts are typically larger than neighborhood traffic circles and are used on higher volume streets to allocate right-of-way among competing movements. They have splitter islands to channel approaching traffic to the right, and do not have stop signs.</p>	<p>Single-lane roundabouts can be used at intersections where collector streets intersect. The combined intersection volume should not exceed 16,000 vehicles per day. A single lane roundabout may also be used in place of a traffic signal.</p> <p>Refer to Figure 5.4 for a typical roundabout design prototype for Placer Vineyards.</p>

# TRANSPORTATION AND CIRCULATION

## B. Vertical Devices

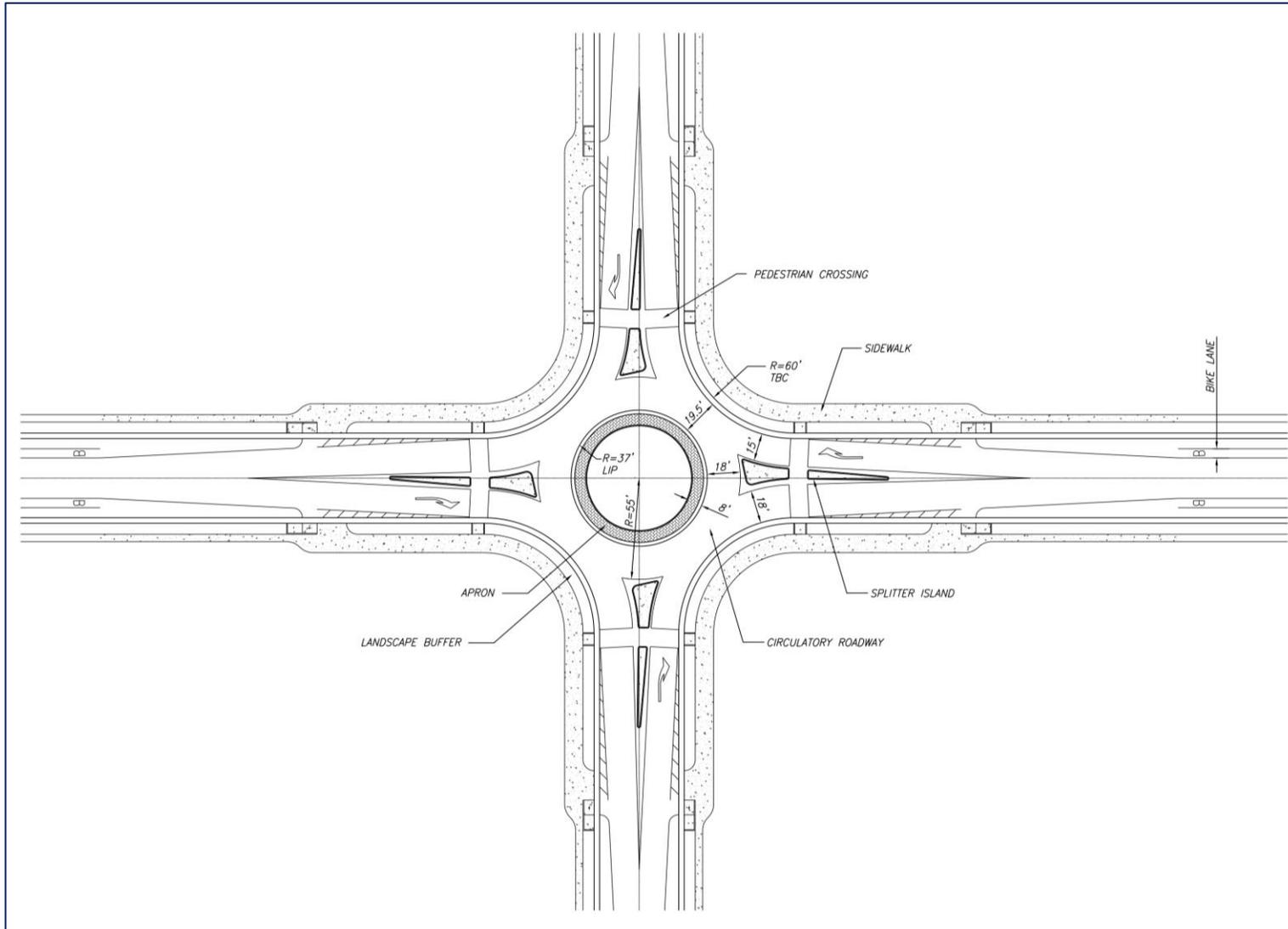
Traffic Calming Device	Description	Application
<p>Textured Pavement</p>  <p>Source: <a href="http://www.pedbikeimages.org/">www.pedbikeimages.org/</a> Dan Burden</p>	<p>Textured colored pavement includes the use of stamped pavement (asphalt) or alternate paving materials to create an uneven surface for vehicles to traverse. It alerts drivers to a change in surroundings or emphasizes other traffic calming devices.</p>	<p>Textured pavement can be used at intersections, midblock locations, or driveways. This treatment can be applied to most roadway types, but should be limited in residential areas due to the noise created.</p>

C. Volume Control Devices

Traffic Calming Device	Description	Application
<p>Median Barrier</p> 	<p>These raised islands are located along the centerline of a street that continues through an intersection to block through movement at a cross street.</p>	<p>Median barriers are similar to center island narrowing but are used at intersection locations to prohibit certain turning movements. They can be used on most roadway types where the expected average daily traffic is less than 5,000 vehicles per day.</p>
<p>Forced Turn Island</p> 	<p>These raised islands prohibit certain movements on approaches to an intersection.</p>	<p>Forced turn islands channelize and/or restrict certain movements at an intersection. This treatment can be applied to most roadway types where the expected average daily traffic is less than 5,000 vehicles per day.</p>

Source: Fehr and Peers 2005

Figure 5.4 - Typical Roundabout Design



### 5.3.4 TRANSIT SYSTEM

The Plan Area will include systems and facilities to promote public transit use, including dedicated bus rapid transit lanes on Watt Avenue from Base Line Road to the southern limits of the Plan Area, a site for a transit center with bus turnouts, and provisions for future, local bus service on local roadways with bus turnouts and stops appropriately spaced. The transit system is more fully described in the approved Transit Master Plan.

An internal transit system shall be planned and implemented as the project is constructed. An Americans with Disabilities Act (ADA) dial-a-ride service will be provided. A fixed-route internal service will be provided that connects the village centers with the town center and other areas as deemed appropriate. Figure 5.5 shows the potential location of bus stops and the potential circulation routes for the public transit systems serving Placer Vineyards. Commuter service will be provided to downtown Sacramento. The transit center, proposed in the east village center along Watt Avenue, will serve as a major transfer point between regional and local transit service.

**Goal 5.6** Promote public transit systems as an alternative means of transportation to reduce traffic congestion.

***Policy 5.17 Bus Rapid Transit System.***

*A public transit system and dedication of right-of-way corridors for future bus rapid transit with a feeder bus network shall be provided along Watt Avenue from Base Line Road to the Dyer Lane intersection just north of Dry Creek.*

***Policy 5.18 Streetcar Right-of-Way.***

*Dedication of rights-of-way for a future street car system shall be provided along the north side of Town Center Drive, extending from the transit center on Watt Avenue to the town center, ending at 16<sup>th</sup> Street.*

***Policy 5.19 Multi-modal Transit Center.***

*A transit center site is located on Town Center Drive to serve as a transfer point for regional and local transit services. The transit center site shall be of sufficient size to accommodate all future anticipated uses. It will include covered shelters, bus staging areas, park-and-ride lots, and bicycle storage facilities.*

***Policy 5.20 Transit Service and Facilities***

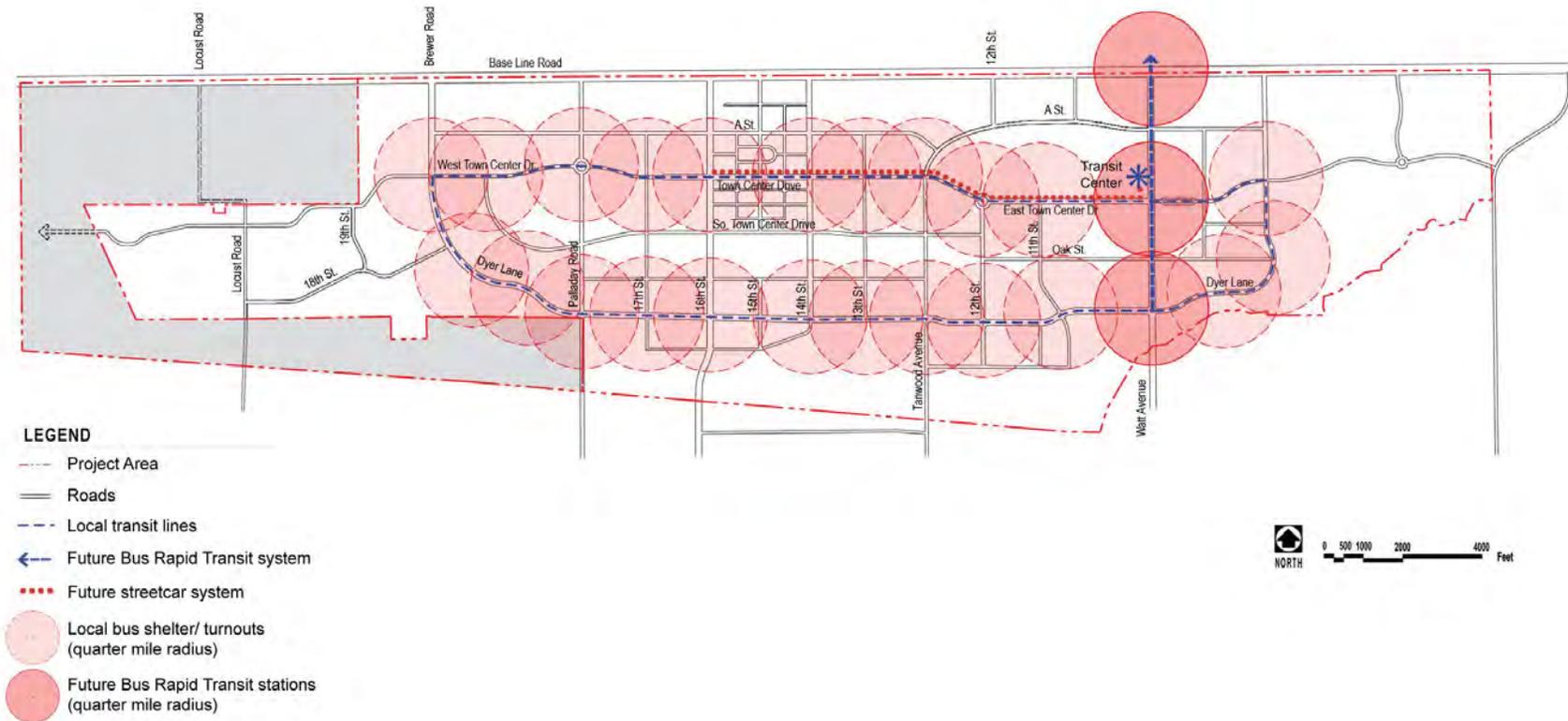
*Placer Vineyards shall participate in regional service with connection to light rail transit on Watt Avenue in Sacramento County, Regional University, Galleria Mall, and other regional centers. As each parcel is developed, provisions for bus stops, turnouts, shelters, park-and-ride lots, bike lockers, lighting, and other transit-support facilities will be examined and constructed.*

***Policy 5.21 Provision of Park-and-Ride Lots.***

*Park-and-ride lots shall be established and maintained at the town center and transit center at the east village center. The majority of the park-and-ride spaces shall be accommodated in the transit center where a majority of local and regional commute trips will be concentrated. A minimum of 50 spaces shall be provided in the town center, established as shared parking. Other smaller park-and-ride lots are encouraged to be established as a shared parking use incorporated into the overall parking design of other commercial and office centers or adjacent to public transit.*

*In total, a minimum of 193 parking spaces shall be distributed between the park-and-ride lots. More park-and-ride lots should be provided, especially adjacent to neighborhood activity centers, transit routes, and major transit corridors to encourage ride sharing, promote use of public transit, and reduce air pollution.*

Figure 5.5 - Public Transit Concept



**5.3.5 BIKE AND PEDESTRIAN CIRCULATION**

The Specific Plan includes three basic types of bikeways:<sup>1</sup>

Class I Bike Paths:

Off-street bike paths are completely separated from traveled roadways and are for the exclusive use of cyclists and pedestrians

Class II Bike Lanes:

Signed and delineated on-street lanes designed for one-way use of bicycles. Class II bike lanes are typically located along the shoulder or gutter in a widened portion of the street.

Class III Bike Routes:

Non-designated, on-street routes along local public streets where cyclists do not have a delineated lane and must share the roadway with motorists.

**Class I Bike Paths:**

The proposed off-street bikeway system for Placer Vineyards provides more than 35.1 miles of Class I bike paths, located within open space and landscape corridors along thoroughfares and arterial streets. Class I bike paths adjacent to roadways with Class II bike lanes shall be hard paved with a minimum width of 8 feet. The Class I bike path along the Dry Creek corridor shall be 12 feet in width with a 2-foot-wide decomposed granite jogging path on one side of the hard surface paving. All other Class I bike paths shall be 10 feet in width. The locations of Class I bike paths are indicated in Figure 5.6, “Class I Bike Paths and Multi-Purpose Trails Diagram,” and the roadway sections in Figure 5.3.

Off-site trail dedications may be requested in

association with tentative subdivision maps if it is determined by County staff that such linkages are crucial in the ability to complete major trail segments.

**Class II Bike Lanes**

Class II bike lanes will be located within the right-of-way of arterial, major collector, and collector streets. Bike lanes shall be designated with a white, painted stripe on the roadway. Street signs shall indicate the location of bike lanes and major destination points.

**Class III Bike Routes:**

Class III bike routes will be located on existing traffic lanes with low traffic volumes. These streets will connect to Class II bike lanes and Class I bike paths.

**Goal 5.7** Provide a system of on-and off-street bikeway and multi-purpose trails that connect to destinations within the Plan Area and to the regional trail network.

**Policy 5.22 Class I Bike Paths & Multi-Purpose Trails System.**

*Class I Bike Paths and Multi-Purpose Trails shall be provided as identified by Figure 5.6, “Class I Bike Paths and Multi-Purpose Trails Diagram.”*

**Policy 5.23 Class I Bike Paths and Multi-Purpose Trails.**

*Class I bike paths and multi-purpose trails shall offer a variety of experiences, including connections within and between parks and other public open space lands or to schools, and connections to regional trails and transit facilities within and outside of the Plan Area.*



*Class I Bike Path*



*Class II Bike Lane*



*Class III Bike Route*

<sup>1</sup> Specific Plan bikeway types are consistent with the Placer County Bikeway/Trails Master Plan, Section E of the Dry Creek Community Plan and the CalTrans Highway Design Manual.

## TRANSPORTATION AND CIRCULATION

### **Policy 5.24 Provision of Class I Bike Paths and Multi-Purpose Trails.**

Private developers shall incorporate Class I bike paths and multi-purpose trail routes that are within their proposed tentative maps as identified in the Class I Bike Paths and Multi-Purpose Trails diagram (see Figure 5.6). Placer Vineyards Class I bike paths and multi-purpose trails shall conform to the following standards:

1. In the Dry Creek corridor only, Class I bicycle paths shall be 12-foot wide hard surface paving with a 2-foot-wide decomposed granite path on one side of the paving. Class I bike paths adjacent to roadways with Class II bike lanes shall be hard paved with a minimum width of 8 feet. In all other areas, Class I bike paths shall be 10 feet wide with hard surface paving.
2. In open space areas, natural surface (gravel, earth) multi-purpose trails may be set a minimum of 10 feet off the hard surface paved trail (for activities such as equestrian riding and mountain biking).
3. Informational signs will be placed throughout the trail system (e.g., “2.4 miles to town center”).
4. Class I Bike Paths and Multi-Purpose Trails will be setback a minimum of 10 feet from residences.
5. Class I Bike Paths and Multi-Purpose Trails will be setback a minimum of 25 feet from preserved or reconstructed wetlands, whenever possible.
6. Collapsible bollards or other similar devices approved by the County will be placed at entries to restrict vehicular access where trails and streets intersect.
7. Class I Bike Paths and Multi-Purpose Trail crossings of drainageways will occur at appropriate intervals.
8. Traffic calming methods and signage shall be used to enhance the safety of the trail systems where they cross major or collector streets.
9. A Class I bike path crossing shall be provided under the Watt Avenue bridge within the Dry Creek corridor.
10. As depicted on Figure 5.6, a Class I bike path shall also be provided on the east side of the Dry Creek bridge and along Watt Avenue, extending to the Placer/Sacramento County line. The Class I bike path on the east side of the bridge will be separated from traffic by a railing.

### **Policy 5.25 Construction of Class I Bike Path & Multi-Purpose Trail Improvements.**

Class I Bike path and multi-purpose trail improvements are planned to connect Morgan Creek to Gibson Ranch Park. Land owners shall design and construct

Class I bike path and multi-purpose trail improvements within the open space portions of their property, according to the following standards and provisions of the Development Agreement.

1. In conjunction with the construction of a backbone roadway system, a set of backbone trails adjacent to these roadways, as described in Section 9.3 and the Public Facilities Financing Plan, shall be constructed at the same time that the backbone roadways are constructed.
2. Landowners shall install sections of the trail when they install subdivision improvements within the parcels adjacent to the open space. Trail connections to the backbone trails shall be included as part of the subdivision improvements.
3. Class I bike path sections shall be constructed and improved according to Figure 5.6, “Class I Bike Paths & Multi-Purpose Trails Diagram.” Class I Bike paths shall be designed in accordance with the County’s design standards for off-street bike paths and the guidelines provided in the Specific Plan.
4. Landowners shall proceed to complete the construction of Class I bike path improvements at the same time that they install and complete the balance of the subdivision improvements for the parcel(s) adjacent to the open space.
5. Landowners shall be responsible for all costs associated with the design and construction of Class I bike path and multi-purpose trail improvements, including the costs of preparing required plans and drawings and obtaining all required permits.
6. Upon completion of Class I bike path and multi-purpose trail improvements by the landowner, the County and/or the Park District<sup>2</sup> shall accept the dedication of the Class I bike paths and multi-purpose trails and applicable open space area and assume ownership and maintenance of these facilities, provided that the cost of maintenance shall be funded by a Park Services CFD.

### **Policy 5.26 Fire Trails/Access through Open Space.**

Fire access routes shall be integrated into the open space trails system and shall comply with Placer County Fire Department standards.

1. Class I bike paths and multi-purpose trails shall include design features that minimize barriers to emergency response, such as knock-down bollards for emergency access at trailheads.

<sup>2</sup> The Development Agreement specifies the required steps for the formation of the Park District.

2. *Rolled curb access points shall be provided in open space areas. Open space access points shall be provided at each cul-de-sac that abuts an open space and spaced every 1,000 feet along streets adjacent to open space areas. These access points shall be identified with signage and painted red curbs. Emergency access easements shall be provided for each emergency access area.*

***Policy 5.27 Roadway Crossings in Utility Corridors.***

*Roadway crossings shall be minimized through utility corridors to reduce the fragmentation of trails and open space.*

***Policy 5.28 Provision for Multi-Purpose Trails.***

*Multi-purpose trails are located in the open space buffer areas adjacent to the SPA and next to the Placer/ Sacramento County line, as indicated in Figure 5.6, "Class I Bike Paths and Multi-Purpose Trails Diagram." The multi-purpose trails planned adjacent to the SPA will be separated from the Placer Vineyards development area with landscape berms (refer to Figures 7.10-7.14 for plans and sections of the berms and buffers next to the SPA). The multi-purpose trails planned in the open space buffer areas along the Placer/ Sacramento County line, shall connect to the facilities at Gibson Ranch Park and link into the multi-purpose trail system proposed along the south side of Dry Creek, as directed by the Dry Creek/ West Placer Community Plan.*

Figure 5.6 - Class I Bike Paths and Multi-Purpose Trails Diagram



CHAPTER VI: COMMUNITY DESIGN



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## 6.1 COMMUNITY DESIGN CONCEPT

### Context

This chapter is intended to define the type of community envisioned for Placer Vineyards. This chapter sets the goals, policies, and design guidelines that ensure the creation of a safe, high quality, and livable environment for the existing and new residents of Placer Vineyards. The “Community Design” chapter along with those standards and guidelines in other chapters of the Specific Plan are intended to ensure a cohesive and unique character for Placer Vineyards, while allowing flexibility for individual property owners to develop their own unique brand of housing types and distinct neighborhoods.

The contents of this chapter shall be used in concert with the standards and guidelines found elsewhere in the Specific Plan. Refer also to the following chapters of the Specific Plan for information regarding the design of the Placer Vineyards community:

- ◆ Chapter III, “Land Use,” introduces the land use plan and describes the land uses in the community;
- ◆ Chapter V, “Transportation and Circulation,” provides the design standards and guidelines related to the design of the streets and trails within the Placer Vineyards site;
- ◆ Chapter VII, “Parks and Open Space,” describes the parks and open space network for Placer Vineyards;
- ◆ Chapter VIII, “Public Utilities and Services,” provides information on the design of the infrastructure system serving the Plan Area as well as the schools, sheriff, fire protection, government services and other community serving facilities;
- ◆ Chapter IX, “Implementation,” discusses the administration and implementation steps required to achieve the goals and regulations set forth in this Specific Plan;
- ◆ Appendix A, “Land Use and Development Standards,” further defines the intensity and density of development and permitted uses allowed in each of the land use districts; and
- ◆ Appendix B, “Recommended Plant List,” provides a list of the recommended trees, shrubs, vines, and ground covers for Placer Vineyards.

### Purpose

In general, the standards and guidelines of the Specific Plan are intended to be consistent with adopted County goals and policies. This chapter of the Specific Plan establishes specific standards for the Plan Area that may be unique or different from the *Placer County Design Guidelines* and *Placer County Landscape Design Guidelines*. This chapter addresses the overall vision for Placer Vineyards and will serve to implement and direct development specific to the Placer Vineyards site.



Application of these standards and guidelines ensures that development that may occur over an extended period of time is high quality. The Placer Vineyards development will result in distinct neighborhoods and development areas with unique site and landscape characteristics. Different areas of the site will be united by a common network of streets, trails, and greenways; connected by common access to an extensive system of parks, open space, schools, employment, public facilities, and urban centers; and united by a townscape vocabulary achieved through the consistent treatment of common site and building design elements.

## COMMUNITY DESIGN

The standards and guidelines in this chapter address the common design elements, the centers, and unique residential neighborhoods in Placer Vineyards. It is organized into the following sections:

### 6.2 Community-Wide Design Elements

- 6.2.1 Landscape Design
- 6.2.2 Streetscapes
- 6.2.3 Community Gateways
- 6.2.4 Signage Systems
- 6.2.5 Lighting

### 6.3 Centers of Activity

- 6.3.1 Design of Activity Centers
- 6.3.2 Town Center
- 6.3.3 Village Centers
- 6.3.4 Commercial Centers
- 6.3.5 Base Line Road Regional Commercial Corridor
- 6.3.6 Neighborhood Commercial Centers

### 6.4 Residential Neighborhoods

- 6.4.1 General Lot Design Standards
- 6.4.2 Residential Design
- 6.4.3 Walls, Fences and Screening

The standards and guidelines in this chapter will be used to assist Planning staff, the Planning Commission, and Board of Supervisors in evaluating the merit of development proposals. To aid in the interpretation of policies and guidelines in this chapter, the following rules apply:

- ◆ Policy statements and design standards express a County requirement and use the terms, “shall” or “must.”
- ◆ Design guidelines are qualitative and express a design intent through descriptions and illustrations, with flexibility for interpretation so long as the intent of the guidelines is upheld. Guideline statements use the words “should,” “may,” “encouraged,” and “discouraged.”

- ◆ Guidelines that employ the word “should” are intended to express the County’s desire and expectation. An alternative measure or approach may be considered, however, if it meets or exceeds the intent of the design guideline. Guidelines that include “may” allow for flexibility and expresses that alternative measures or approaches can be used to satisfy the intent.
- ◆ Guideline statements using the words “encouraged” or “discouraged” are meant to express, respectively, a more or less desirable design solution.

Should individual projects differ in some respects but stay substantially consistent with the intent of these design standards, minor deviations may be allowed. These determinations will be made at the discretion of the Planning Director or the Planning Commission. For design items that are specified in exhibits, text, and/or adopted goals and policies of the Specific Plan, modifications may require an amendment to the Specific Plan. Minor changes to an approved project that are consistent with original approvals may be approved by the Planning Director. Refer to Chapter IX, “Implementation,” for the administrative procedures to follow.

## 6.2 COMMUNITY-WIDE DESIGN ELEMENTS

This section outlines the goals and policies applicable to plan elements that are to be consistent across the entire Plan Area. For certain elements (i.e. parks, open space, streets) the text may also refer to design standards contained in other associated chapters of this Specific Plan.

### 6.2.1 LANDSCAPE DESIGN

The designed landscape creates the framework for common elements that knit the community together. These elements are intended to bring cohesiveness to the community, to engage the natural vegetation and topography of the site, to enhance the livability and identity of the community, and to provide transitions to adjacent sites and land uses.

- Goal 6.1** Provide a consistent and visually cohesive landscape throughout the Plan Area as well as provide identity to individual neighborhoods.
- Goal 6.2** Define and maintain a landscape and architectural style for the Plan Area.
- Goal 6.3** Provide for views from the road to community features such as creeks, wetlands, major tree groves, and other open space on the site

**Goal 6.4** Put in place a designed landscape that utilizes a plant palette that is visually attractive, varied and either uses recycled or reclaimed water or minimizes water use.

**Policy 6.1 Landscape Design and Landscape Master Plan.**

*In order to ensure the implementation of a cohesive image for the community, a detailed Landscape Master Plan shall be developed and approved by the County. This Master Plan shall address the design of streetscapes, entries, and other image features. The Landscape Master Plan shall define the public landscape areas of the community. The Landscape Master Plan shall be guided by the following features:*

1. *The landscape design treatment for Placer Vineyards shall consider the context, historical land use, and ecology of the region. Design elements consistent with the historic rural image of the area are encouraged.*
2. *Use landscape materials, trees, shrubs, and ground cover that are native to the area, have low water use, and fit into the local environmental conditions. Refer to Appendix B, "Recommend Plan List."*
3. *The streetscape designs described in the following sections of this chapter and the conceptual design figures that accompany them will be further refined and modified during the preparation of the Landscape Master Plan.*

**Policy 6.2 View Protection.**

*Landscape elements shall be designed to create vistas and frame views of important community features, such as natural site features, significant buildings, or landmarks.*

**Policy 6.3 Natural Resource Preservation**

*Where possible, open space areas shall interconnect with the oak woodlands, grasslands, wetlands, and other natural resources in the Plan Area.*

**Policy 6.4 Recommended Plant Palette.**

*Appendix B of this Specific Plan contains a list of plants recommended for use in Placer Vineyards. This list, as well as the plants listed in the Placer County Landscape Design Guidelines, should be used when designing open space, landscape buffer corridors, streetscapes, gateways and parks. Plant similar to those listed in the table may also be used, subject to County review and approval.*

## 6.2.2 STREETSCAPES

The streetscape is one of the major visual elements that can help tie the community together in a consistent theme and quality of experience. Streetscape design is concerned with the design of the "public realm" or the area that stretches from the public streets to the building face. Streetscape design includes the selection of landscaping along the street and front yards and includes pedestrian amenities along the public right-of-way such as seating, lighting, and pedestrian furniture. Streetscape design guidelines apply to street corridors and along landscape buffers, medians, and gateways.

Trees are a major component of the streetscape design and livability of the Placer Vineyards community. Trees provide an attractive setting for walking and bicycle use, shade the sidewalks, street surface, and adjoining buildings; and improve the air quality and overall environment of the neighborhood. The streetscape design within the Plan Area is intended to provide a unified design and character for the project.

The guidelines presented in this section complement the roadway design standards found in Chapter V.

**Goal 6.5** Create an attractive and inviting setting for the "public realm" that supports an active and vital mixed-use community. Features will include trees, shrubs, and ground cover planting, as well as trails, paths, monument signs, and other architectural amenities.

**Goal 6.6** Design streetscapes along thoroughfares and arterials with a more spacious character to the public realm, consistent with the size and width of the street corridor, the volume and speed of traffic, and the amount and type of pedestrian activity on the street. Consider safety, access and traffic control regulations for maintenance personnel in the design of median landscaping along thoroughfares and arterials.

**Goal 6.7** Create a canopy of tree coverage throughout the Placer Vineyard that established a strong and attractive community identity and provides shade.

**Policy 6.5 Landscape Corridors on Major Streets**

*Street landscape corridors will be developed along thoroughfares and arterials (i.e., Watt Avenue, Dyer Lane, and 16<sup>th</sup> Street; see Figures 6.1 and 6.2) and along major collector streets.*

## COMMUNITY DESIGN

1. *These parkways are wide and heavily landscaped with double rows of street trees, ground cover, and flowering accent vegetation.*
2. *Walkways shall be designed as joint-use pedestrian and bicycle paths running through the parkway.*
3. *The streetscapes shall also provide linkages to neighboring open spaces, parks, schools, and major activity nodes, allowing for pedestrian and bicycle circulation as well as for other passive recreational and educational opportunities.*
4. *The corridors will have walks and trails separated from the streets. Landscaping will be expanded at entries to the development and blended in with the adjoining park sites and open space areas.*

### **Policy 6.6 Street Landscape Corridor Design.**

*Street landscape corridors shall be designed with different plant palettes to give a unique character to the streets. Landscaping along roadway corridors shall provide a unifying element to a neighborhood. Refer to Appendix B for a list of recommended streetscape trees, shrubs, vines, and ground covers, and Figures 6.1 through 6.2 for sample street corridor designs for Base Line Road and Watt Avenue.*

1. *Thoroughfares: Base Line Road, Watt Avenue and Arterial Streets: Dyer Lane and 16th Street. Thoroughfares (see Figures 5.3-A, B) will have a 50-foot landscaped setback buffer and 14-foot planted median throughout the project area. Arterial streets (see Figure 5.3-D and E) will include a 35-foot landscape setback on both sides of the street and a 14-foot planted median. Landscape buffers will include earthen berms and plants, deciduous trees, evergreen trees, and drought-tolerant shrubs and ground covers. Berms and plants, such as 3-4-foot tall hedges, should be used to screen parking areas and/ or sound walls.*

*For thoroughfares and arterial streets, a row of trees for shade with drought-tolerant shrubs and ground covers will be planted in the road medians. Low drought-tolerant plants shall be installed as temporary landscaping in the future BRT right-of-way on Watt Avenue. Deciduous and evergreen canopy trees will be planted between curbs and sidewalks, with evergreen screen trees placed between sidewalks and residential walls. Landscaping shall provide for a consistent use of plant materials for the thoroughfares and each arterial street.*

*Unless recycled water is used for irrigation, irrigated turf will be limited to 25% of the total landscape corridor area. Turf areas will be limited to accent areas near intersections, project signage, buildings, and areas between the back of the curb and sidewalks.*

2. *Major Collector Streets: Palladay Road, Tanwood Avenue, 14th Street. Select collector streets, as shown in Figure 5.3, Section F, will be landscaped with a 20-foot landscape setback area. A single, large deciduous canopy tree or other thematic landscape combination should be established per street. The same tree or repetition of trees will be planted within landscaped areas on both sides of the street at regular intervals for the entire road segment. In addition, other planting will consist of drought-tolerant ground covers, vines, and shrubs to screen walls or fences that abut the collector street landscape areas.*
3. *Minor Collector Streets and Local Streets. Local streets will be landscaped with a single, large deciduous canopy tree planted at regular intervals (at approximately 25-30-foot intervals or at a distance that provides appropriate spacing for the type of tree selected) on both sides of the street for the entire length of the road. In addition, other planting will consist of ground covers, vines, and shrubs to screen walls that “side-on” to local streets (see Figure 5.3, Sections G, H, R1 through R3, R5, and R6 for typical street cross sections).*
4. *Residential Alleys. All unpaved alley setback areas shall be landscaped.*

### **Policy 6.7 Street Tree Design.**

*Street tree planting throughout Placer Vineyards will vary depending on the type and size of each street and the desired character of the individual neighborhoods. Street tree conditions will serve as visual screens and buffers from development.*

1. *In general, street trees shall be planted at regular intervals, or in groupings with spacing dependent on tree size at full growth, to create a shaded canopy along the pedestrian travel way (see Appendix B for a recommended plant palette).*
2. *Street tree planting shall also be modified to incorporate existing trees and native oaks, when possible.*
3. *Street trees along major roadways shall be larger, scaled in relation to the street corridor and automobile travelers. Along thoroughfare and arterial streets, the dominant tree specified will be used 70% of the time.*
4. *Neighborhood streets shall be designed to include one row of large deciduous canopy trees.*

Figure 6.1 - Conceptual Base Line Road Landscape Corridor

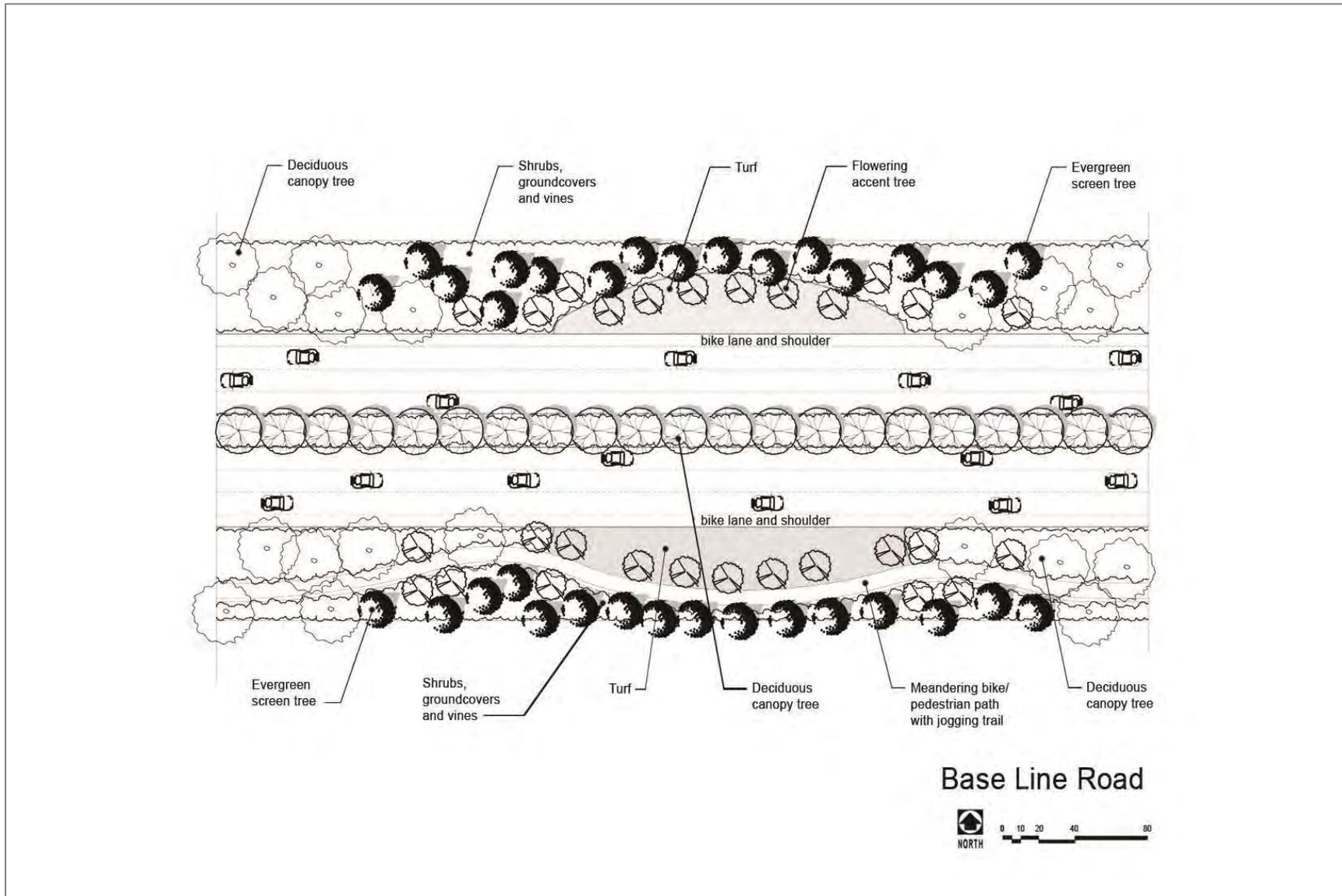
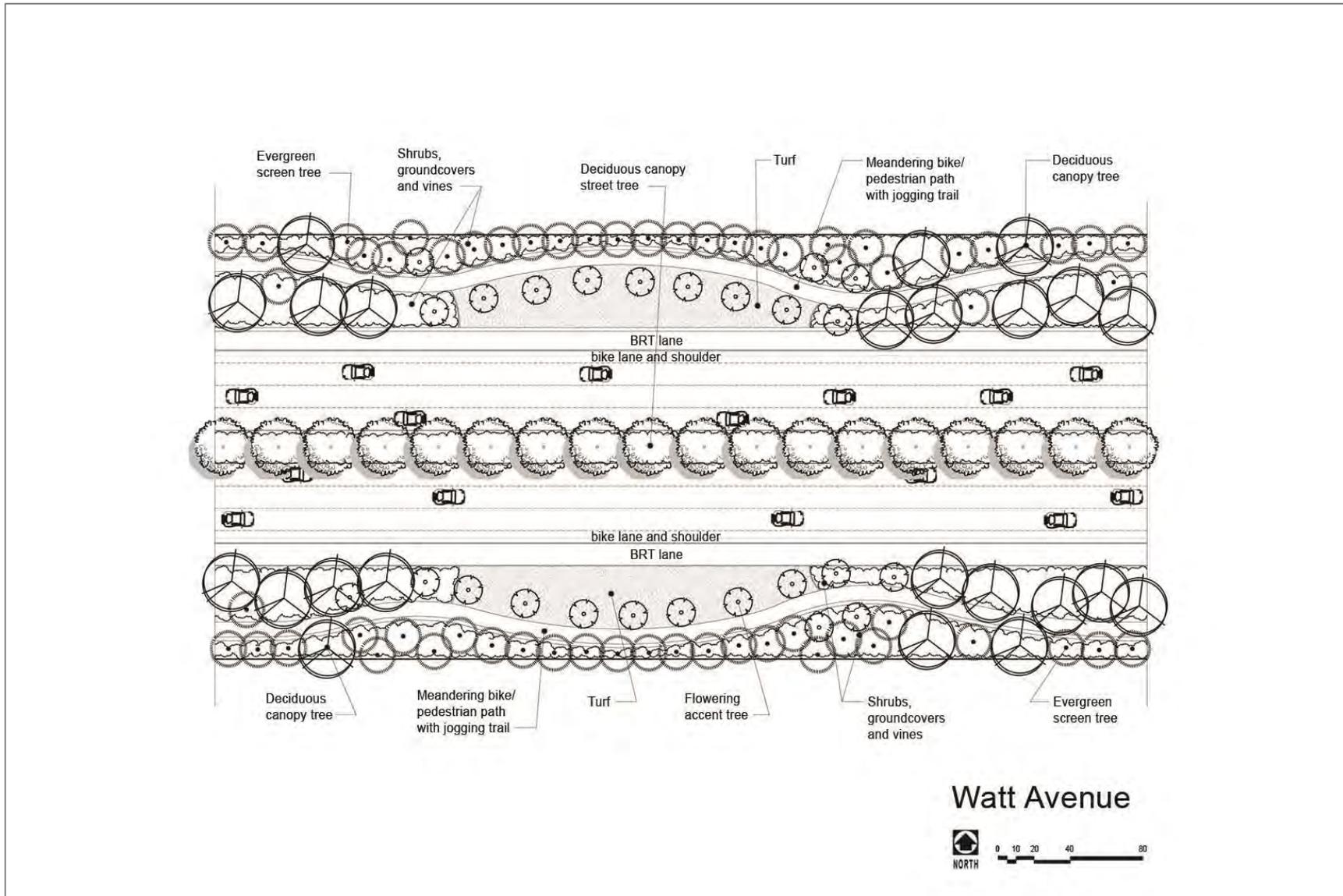


Figure 6.2 - Conceptual Watt Avenue Landscape Corridor



### Street Tree Design Guidelines

1. The size and spacing of street trees can help identify the street classification and function. In general, thoroughfares and arterials should be landscaped with larger canopy street trees and spaced accordingly. Collectors should be planted with different, smaller tree species, and local neighborhood street trees should be planted with yet another tree type.
2. Deciduous canopy trees and low ground cover should be placed in landscaped areas between the curb and sidewalk, while evergreen trees, shrubs, and vines should be located in landscaped areas between the sidewalk and walls or buildings. Trees should be planted with sufficient spacing to create full coverage and to allow for mature growth.
3. Street tree selection and plant locations should vary along the major thoroughfares, such as Watt Avenue and Dyer Lane, to create a naturalistic feel to the street corridors.
4. Tree selection should reflect the unique character of each neighborhood or street. Trees should be planted to ensure the spatial continuity of the street. Street trees should provide a large canopy with shape as well as color, variety, and accent.
5. Accent trees with color and seasonal flowering should be planted in clusters that exhibit interest and highlight intersections, entry points, or important destinations.
6. Street tree selection should reflect the environmental characteristics of the region. Low maintenance and drought-tolerant species are recommended.
7. Selected street trees species should have the ability to thrive in urban conditions where tree roots are often impacted by sidewalks and other obstacles such as underground utility lines and vaults.

### Sidewalk and Street-Side Trail Design Guidelines

General guidelines for sidewalks and Class I bike paths along major streets include:

1. Highly visible and lighted crosswalks should be located at all major intersections. Alternative surface materials are encouraged at crosswalks;
2. Sidewalks on major streets should have a minimum clear path of 5 feet in Placer Vineyards for pedestrian and wheelchair access;
3. Streetscape elements such as seating, benches, bike racks, signage, fire hydrants, news racks, bus shelters, lighting, planters, trees, and utility poles and boxes should be located in landscape lots or sidewalks. It is desirable to combine these elements when feasible;
4. Privately maintained areas adjacent to sidewalks should enhance the public space. Materials used for walls, fences, and pavement should be coordinated with and complement the streetscape design; and
5. Streetscape elements and paving materials should be of high quality to minimize maintenance. The quality of material and surface pattern contributes to the pedestrian experience.

6.2.3 COMMUNITY GATEWAYS

The gateways into the community help define separate and distinct areas of the project, while providing for a sense of arrival. Placer Vineyards is designed with a series of community and neighborhood entry points. The entryway concept provides a hierarchy of entryway experiences and, when coordinated with the street system, will help orient visitors and community residents to the site. Entryway designs should complement the overall image and style of the Placer Vineyards community, and will be further defined in the *Landscape Master Plan*. Placer Vineyards will incorporate primary entries at major arterials throughout the project and minor entry points at other streets (see Figure 6.4).

- Goal 6.8** Create attractive gateway features that help establish a distinct identity for the Placer Vineyards community and its neighborhoods and helps orient visitors to the site.
- Goal 6.9** Use a consistent application of elements such as landscaping, signage, fencing, and lighting at key entrances to the community.

**Policy 6.8 Community Gateway Signage.**  
*Community gateway signage will consist of monument signs set in median or on one or both sides of the street. These monument signs shall be located outside the right-of-way when they are proposed along the sides of streets and may be incorporated into walls or other architectural features to emphasize the transition. A complete signage program design for Placer Vineyards will be developed as a part of the Landscape Master Plan (see Policy 6.1).*

**Policy 6.9 Minor Entries.**  
*Minor entries shall be located outside the right-of-way at intersections to the internal street system to identify and mark the transition into residential neighborhoods in Placer Vineyards. Entries are small-scale landscape or architectural features, signs, or monuments at the street corners that may include special lighting, flowering plants, or public art. These features are smaller in scale than community entries and serve as landmarks that give special identity and character to each residential neighborhood or block.*

**Policy 6.10 Community Gateway Theme.**  
*All entries shall include thematic architectural or landscape design elements that incorporate a special landmark feature or public art to identify destinations in the community, representative of the Placer Vineyards character. Public art shall be subject to Design/ Site Review approval.*

**Policy 6.11 Northern Community Gateways and Entries.**  
*The primary northern entries will be provided on Base Line Road at Dyer Lane, 16th Street, Watt Avenue, and East Dyer Lane. Minor entries will be located at other additional collector roadways intersecting Base Line Road, as indicated in Figure 6.3.*

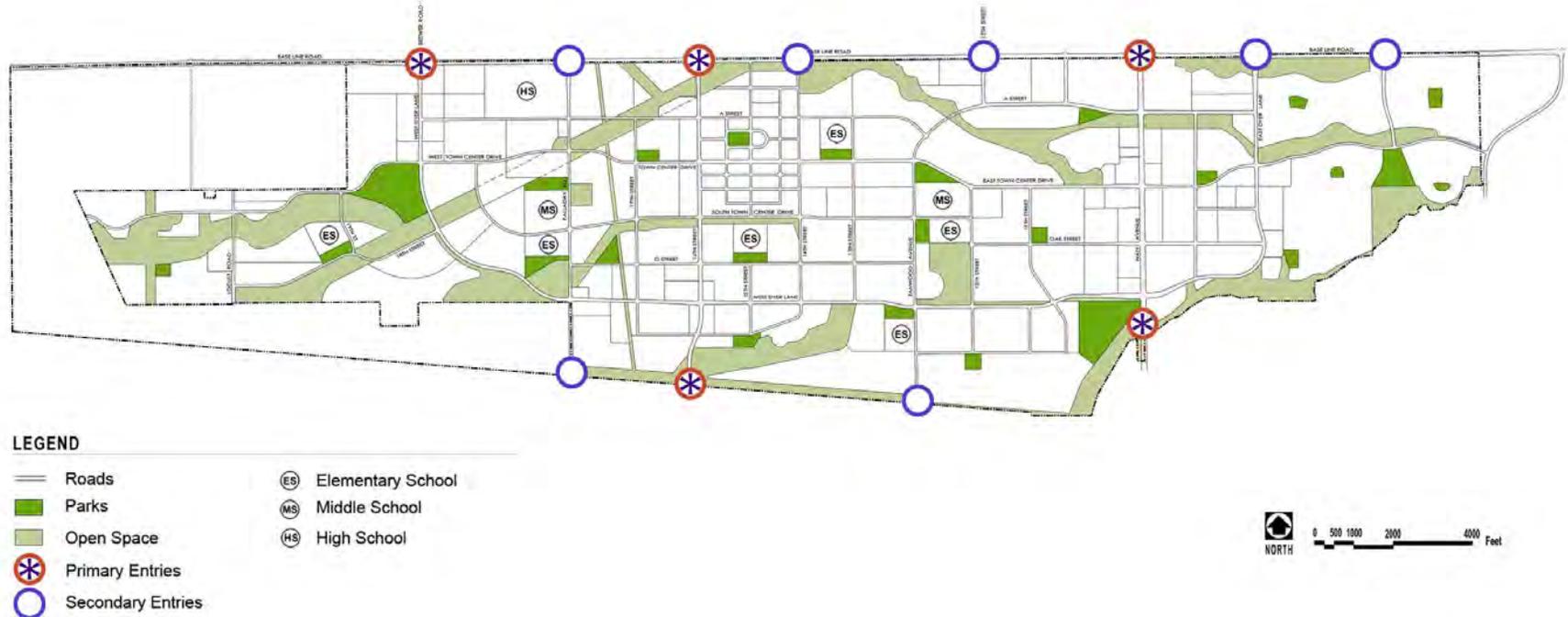
**Policy 6.12 Southern Community Gateways and Entries.**  
*South of the project site, primary entries will be located along the Sacramento/ Placer County Line at 16th Street, Tamwood Avenue, and at Watt Avenue where it intersects with Dry Creek (see Figure 6.3).*

**Policy 6.13 Lighting of Entryway Signs.**  
*Lighting of entry signs shall consist of either indirect lighting with the light source at ground level or internal halo-type lighting. In both cases, the lighting source shall not be visible from traveled roadways.*



*Entries may be identified using landmark features or public art*

Figure 6.3 - Community Gateway Site Location Diagram



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### Design Guidelines for Community Gateways and Entries

1. Entries should be designed to represent the character of Placer Vineyards.
2. Entries should be treated with similar materials, colors, and forms.
3. Entries should be designed to highlight the transition into Placer Vineyards with taller vertical elements.
4. Entry walls should be designed as low-profile monument signs.
5. The ground plane entering into the Placer Vineyards neighborhood core should be highlighted from the street with colored and textured paving.
6. Landscape materials, oak trees, and other planting should be formal groupings, to reinforce the signage and entryway features.
7. Landscaping should be carefully placed to avoid obscuring signage.
8. Up-lights and halo-lit signs and monuments should be integrated into entryway features.
9. Primary entry signs are not intended to be used for individual business advertisement.
10. Entry signs will be designed not to interfere with vehicle sight distances.



*Community Gateway monument sign design*



*Use ground cover, shrubs and other plants to integrate monument signs into the overall site landscape*



*Minor entryway sign used to identify a residential neighborhood or district*

### Design Guidelines for Minor Entries

1. Minor entries should be designed with a consistent visual theme using similar materials, landscaping, lighting, and architectural forms.
2. The minor entries should be incorporated into the landscape areas outside the street rights-of-way. Features may include landscaping, signage, lighting, decorative walls, and other architectural forms that provide transition.
3. Designs should incorporate pedestrian streetscape elements and landscape materials that represent the area's urban, mixed-use character.
4. Surface textures and colored paving materials may be incorporated into the ground surface.
5. At the entry into mixed-use areas, land uses and building corners should exhibit a more active and pedestrian retail environment. Within the town center and commercial areas, entries should incorporate larger building setbacks, corner entry plazas, and storefront windows and doorways.
6. Minor entry signs should be designed to be low-profile, monument-type signs that do not interfere with vehicle sight distance.
7. Minor entry signs are not intended to be used for individual business advertisement.

**6.2.4 SIGNAGE SYSTEMS**

Signs and landmark features located throughout the Plan Area will help to establish a coordinated and consistent quality, character, and image for Placer Vineyards and aid residents and visitors in navigating through the community.

The intent of creating a system of permanent signs is to convey information regarding the names of streets, neighborhoods, particular locations, and community facilities. Permanent signs include community entry signs and features, neighborhood signs and entry landmarks, street signs, directional signs, and business identification signs. The complete signage program design for Placer Vineyards is not addressed in this Specific Plan but will be developed for review and approval by Placer County separately in more detail at a later date.

**Goal 6.10** Create a coordinated and consistent quality of signage for the Placer Vineyards community.

**Policy 6.14 General Sign Requirements.**

*All signage shall be subject to the approval of a sign permit by the Placer County Planning Department and shall conform with the sign standards of the Placer County Zoning Ordinance (see Sections 17.54.170 through 17.54.200), unless this is superseded by the adoption of a separate signage program for Placer Vineyards.*

**6.2.5 Lighting**

Lighting throughout the Plan Area is an integral part of the overall community image and character. In addition to reinforcing the character of the town center, village centers, parks, and the streetscape, lighting helps to increase the safety and security of residents and users. The lighting design for Placer Vineyards is guided by the following goals and policies.

**Goal 6.11** Provide sufficient lighting to increase the safety and security of residents and visitors to Placer Vineyards on its streets, parks, and pedestrian pathways. Provide lighting that is attractive and compatible with other design features in the community.

**Goal 6.12** Maintain the spirit and intent of the Dark Skies standards (the ability to view the night sky) by protecting against glare and excess lighting; providing safe roadways for motorists, cyclists, and pedestrians; and promoting efficient and cost-effective lighting.

**Policy 6.15 General Lighting Requirements.**

*Lighting shall be designed and located to meet minimum, necessary ambient light levels for any given location consistent with public safety standards.*

1. *Lighting shall occur at intersections, areas of major pedestrian activity, and building entries, and generally shall be minimized elsewhere.*
2. *Street lighting shall be directed and regularly spaced, and shall project light down to the road surface and away from the building.*
3. *Decorative pedestrian-scaled lighting fixtures shall be used in the town center, village centers, commercial centers, parks, and other major pedestrian activity areas.*
4. *The selection and design of lighting fixtures shall minimize glare and excess spillage onto neighboring properties. All*

5. *street lights shall utilize cut-off fixtures to minimize visibility from adjacent areas of the community and public use areas.*
6. *No lighting shall blink, flash, or be of unusually high intensity or brightness.*
7. *Lighting fixtures shall be energy efficient and provide a balance between energy efficiency and pleasing light color and coverage. Avoid low-pressure sodium fixtures. Acceptable light sources include mercury vapor, metal halide, and similar sources.*

**Policy 6.16 Street Lights.**

*Street lights on Base Line Road and Watt Avenue shall be simple cobra-head light fixtures, located particularly at major intersections. Street lights for collectors and local streets should be decorative fixtures that are compatible with the character of the Placer Vineyards streetscape. The selection of street lights for Placer Vineyards will be addressed in the Landscape Master Plan.*



*Cobra-head street lights shown in the background*

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1. **Color:** The color of street light poles and fixtures shall be consistent throughout the community. All specified metals shall utilize anodized coatings, rather than painted colors.
2. **Materials:** Street light poles shall be either metal or wood fixtures.
3. **Town Center:** Street lighting in the town center shall consist of ornamental street lights flanking the roadways. Street lights shall be located in parallel pairs along the length of the street, spaced to provide illumination levels of 0.5 footcandle, and shall not exceed 14 feet in height.
4. **Village Centers:** Village centers shall use a design in the same design family as the ornamental street lights in the town center, however, each village center shall utilize a light fixture designed to give the center a unique character and identity.
5. **Base Line Road and Watt Avenue:** Street lights on Base Line Road and Watt Avenue shall be simple, single-or double-arm, cobra-head lights, spaced as required to maintain illumination levels of 1.75 footcandle, and shall not exceed 40 feet in height.
6. **Arterial Streets:** Street lights on arterial streets shall be selected decorative fixtures, spaced, as required, to maintain illumination levels of 1.0 footcandle on alternating sides of the street, and shall not exceed 25 feet in height.
7. **Collector and Local Residential Streets:** Street lighting on collector and local streets in residential neighborhoods shall be ornamental or decorative street light fixtures,



Ornamental streetlights in the town center



Banners on decorative, arterial streetlights



Residential streetlight



Streetlights and bollard lights along a pedestrian path

spaced, as required, to maintain illumination levels of 0.5 footcandle on alternating sides of the street, and shall not exceed 14 feet in height.

8. **Neighborhood Entries:** Street lights at neighborhood entries shall consist of ornamental or decorative fixtures flanking the roadway, located in pairs, and shall not exceed 14 feet in height.

### **Policy 6.17 Pedestrian Oriented Lighting.**

Pedestrian oriented lighting will be provided within the town center, village centers, Ornamental streetlights in the town center commercial centers, parks, and major pedestrian activity areas. Pedestrian lighting shall consist of smaller pole fixtures, spaced to maintain illumination levels of 0.5 footcandle, and shall not exceed 14 feet in height.

### **Design Guidelines for Pedestrian Oriented Lighting**

1. Bollard light fixtures are also recommended along pedestrian paths of travel as an alternative to pole-mounted light fixtures.
2. With the exception of sports lighting, wood poles are recommended in parks and open spaces and along major pedestrian paths, in keeping with the more rural and rustic theme for Placer Vineyards.
3. Specialized pedestrian lighting should be used to distinguish differences in individual neighborhoods and areas within Placer Vineyards.
4. Pedestrian lighting should be incorporated into the landscape design along major pedestrian paths of travel at appropriate intervals to provide lighted coverage.

5. Bus shelters, telephone kiosks, and other site furnishings should be adequately lighted for security and functionality. Light sources shall be included in the structure of the furnishings.
6. Pedestrian lighting should be durable and easily maintained and of sufficient intensity to provide lighting without spilling over onto road rights-of-ways and adjoining properties, or conflicting with street lighting.

**Policy 6.18 Recreation Areas and Athletic Facilities**

*Lighting for athletic fields and court facilities shall be designed to minimize illumination and spillage onto neighboring uses and residential areas. Athletic facility lighting shall be provided only in the two community park facilities and the town center recreation center. Mercury vapor or metal halide lamps, mounted 30-50 feet high, are recommended.*

**Policy 6.19 Public Parking Lot Lighting.**

*Parking lot lights shall be no higher than necessary to provide efficient lighting of the parking areas and shall not exceed 25 feet in height for larger parking lots, including the base. Illumination levels shall be maintained at 1.0 footcandle. Spacing between light poles shall be determined by the required level of illumination.*

1. *The fixture design style and colors shall be compatible with the architectural design of the site.*
2. *Within the town center and village centers, the design and color of parking lot light fixtures, shall be consistent with the design of street lights.*

**Policy 6.20 Landscape Lighting.**

*Landscape lighting shall be limited to important landscape areas, entryway features, signage, or pedestrian areas.*

1. *The design of landscape light fixtures shall be hidden from direct view unless designed as an integral part of the area's design.*



*Parking lot lighting design examples*



*Bollard light fixture integrated into site landscaping*

2. *The light source shall be shielded from direct view at night, with the emphasis on the object or area being lighted. Up-lighting and mood lighting in trees and landscaping can be effective design feature for special entries, signs, water features, and landmarks.*
3. *Landscape light fixtures shall be durable and easily maintained. For ease of maintenance, landscape light fixture types and lamps shall be consistent with landscape design and street lighting for each neighborhood.*

**Policy 6.21 Service Areas and Security Lighting.**

*Lighting in services areas shall be designed to avoid spillover onto adjacent properties and activity areas, and shielded from direct view of the light source at night.*

1. *Building-mounted flood light fixtures shall not be used unless the light source is completely shielded from public view.*
2. *Low-pressure sodium fixtures or other lighting types that contrast excessively with lighting in the surrounding use area shall be avoided unless completely shielded from public view. Mercury vapor, metal halide, and similar fixture types are permitted.*

**Policy 6.22 Building Identification and Street Number Lighting.**

*Lighting of building identification and street numbers on buildings shall be used to help locate buildings during evening hours.*

1. *Lighting of street identification or numbers shall be internally lit, back lit, or externally lit. External spot lighting is preferred.*
2. *Street number lighting shall be subdued and not contrast excessively with the surrounding area lighting.*

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3. Numbers and lighting shall be conveniently placed near the building entry and shall be visible from the street and adjoining parking lots.
4. Buildings placed back from and not visible from the street may use freestanding lighted monuments or small pedestrian pole lights with mounted street numbers to identify the street address of the buildings from the street.

### 6.3 ACTIVITY CENTERS

The centers of activity are the community gathering places that are specially designed to serve the Placer Vineyards community. See Figure 6.4 for the location of activity centers. Activity centers include the town center (see Section 6.3.3), the village centers (see Section 6.3.4), the regional commercial centers along Base Line Road (see Section 6.3.5), and the neighborhood commercial centers (see Section 6.3.6).

Special treatment is given to the design of these mixed-use neighborhood sites, with emphasis placed on creating a safe and pedestrian friendly setting with easy and convenient access by multiple means of transit. Commercial and other non-residential properties located within the centers of activity shall require Design/Site Review. All buildings and improvements proposed within the activity centers are subject to approval of a Design/Site Review agreement as outlined in Section 7.52.070 of the *Placer County Zoning Ordinance* and to Appendix A, “Land Use and Development Standards.”

#### 6.3.1 GENERAL GOALS AND POLICIES FOR ACTIVITY CENTERS

- Goal 6.13** Design elements that accommodate pedestrians and cyclists shall be equally treated or take precedence over elements that primarily accommodate automobiles, especially in the town center, village centers, neighborhood centers and access areas leading into parks, schools and other public facilities. Retail centers and commercial areas shall be designed to provide maximum pedestrian accessibility.
- Goal 6.14** Provide a range of housing close to commercial centers with paths that connect them.
- Goal 6.15** Create attractive and comfortable outdoor, pedestrian activity areas that are adjacent to buildings, entries, and along major commercial streets.

#### ***Policy 6.23 Pedestrian Orientation***

*Design elements that accommodate pedestrians and cyclists shall be equally treated or take precedence over elements that primarily accommodate automobiles, especially in the town center, village centers, neighborhood centers and access areas leading into parks, schools and other public facilities. Retail centers and commercial areas shall be designed to provide maximum pedestrian accessibility.*

#### ***Policy 6.24 Density within Centers of Activity.***

*The town center, village centers, and Base Line Road commercial corridor should be established at sufficient densities to support express bus transit from Placer Vineyards and other neighborhood areas in the region.*

- Goal 6.16** Design parking areas that are in scale with and are visually subordinate to the developments they serve.

#### ***Policy 6.25 Parking Lot Shading.***

*New parking lots serving non-residential developments shall include tree planting designed to result in 50% shading of parking lot surface areas within 15 years per the requirements of the Placer County Landscape Design Guidelines (latest edition). These shading requirements shall apply to all impervious surfaces on which a vehicle can drive including parking stalls, drives, and maneuvering areas within the property.*

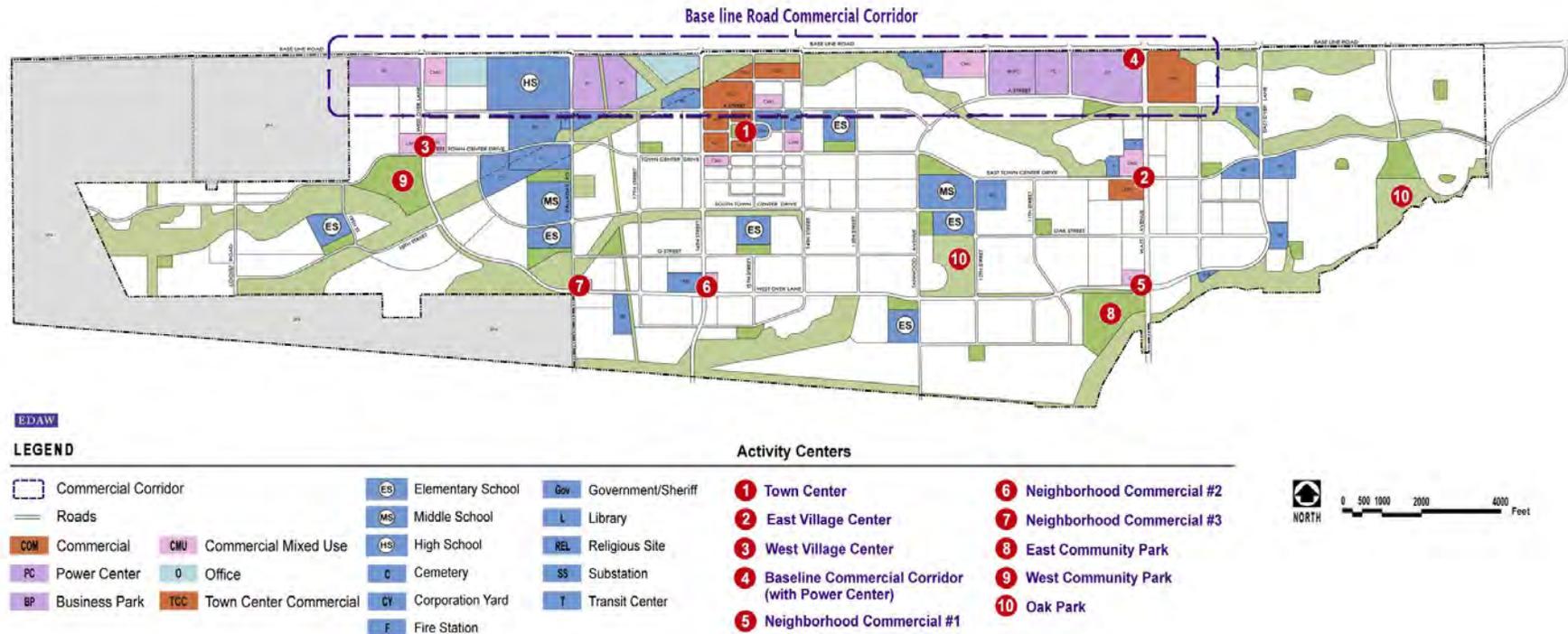
- Goal 6.17** Design buildings whose architectural character contributes to the vitality and attractiveness of the districts, neighborhoods, shopping centers, and work places in the community.

- Goal 6.18** The heights of buildings shall be related and proportional to the activities and surrounding context in which they are located.

#### ***Policy 6.26 Building Heights.***

*Taller buildings will be located along the major commercial nodes, at the town center and within the office and commercial areas along Base Line Road. Lower story buildings and less intense development will be located adjacent to existing neighborhoods in the Special Planning Area to provide appropriate transitions to more intense development at the center and along the major transportation corridors in the Plan Area*

Figure 6.4 - Activity Centers Site Diagram



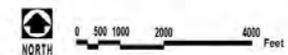
EDAW

LEGEND

Commercial Corridor	Elementary School	Government/Sheriff
Roads	Middle School	Library
Commercial	High School	Religious Site
Commercial Mixed Use	Cemetery	Substation
Power Center	Corporation Yard	Transit Center
Business Park	Fire Station	

Activity Centers

Town Center	Neighborhood Commercial #2
East Village Center	Neighborhood Commercial #3
West Village Center	East Community Park
Baseline Commercial Corridor (with Power Center)	West Community Park
Neighborhood Commercial #1	Oak Park



## COMMUNITY DESIGN

### 6.3.2 GENERAL DESIGN GUIDELINES FOR ACTIVITY CENTERS

#### PEDESTRIAN-ORIENTED SITE DESIGN GUIDELINES

The overall site design should be organized to encourage pedestrian use and activity.

1. Ground-floor commercial buildings should be oriented to plazas, parks, and pedestrian-oriented spaces and streets rather than to interior blocks or parking lots.
2. Street-level windows and numerous building entries, including arcades, porches, bays, and balconies are encouraged.
3. Walls of commercial establishments without an entry or a pedestrian route shall include windows and display areas, or should be lined with retail shops to provide visual interest to pedestrians.
4. Entries to small shops and offices should be sited to directly open onto a pedestrian-oriented street. Buildings with multiple retail tenants should have numerous street entries.
5. Commercial centers should be designed to include pedestrian connections to surrounding neighborhoods.
6. Transit stops, where provided, should be conveniently located within each center and designed as an integral part of the site. Bus transit stops should include amenities that create an attractive and comfortable



*Stores oriented around a public plaza create a setting for pedestrian activity*



*Street-level windows, architectural details and furniture provide interest along the street*



*Transit stop designed for comfort and convenient access*

## BUILDING DESIGN GUIDELINES

Development should be designed to provide varied and interesting building façades and provide variety without creating a chaotic image. Facades should vary from one building to the next to avoid an overly unified frontage. Continuous covered walkways should be provided whenever possible.

### Architectural Details

Provide architectural detailing that gives buildings human scale, visual interest, and distinctiveness through the following:

1. Use a variety of architectural elements such as arcades, recessed exterior balconies, changes in the façade treatment, window awnings, canopies, and other building elements that create visual interest through light and shadow.
2. Use special architectural features to accentuate building entries.
3. Avoid large blank walls at the building base along pedestrian walkways and entries. The building base should be articulated with changes in materials, color, and finishes, window and door patterns, and use of special building entrance features.
4. Coordinate the design of exterior elevations, roofs, and details to create a consistent overall design pattern within a buildings and development site.
5. Articulate wall and window surface places with setbacks, recesses, trim, reveals, or other design detail to add visual interest, scale, and changes in light and shadow to building façades.

6. Screen all utility boxes and rooftop equipment to provide attractive views on the street or from adjacent taller buildings. Avoid use of chain-link fences as rooftop screening.
7. Use high-quality finishes and materials that contribute to creating a unified building character and ensure a consistent design quality, particularly on surfaces adjacent to pedestrian spaces.



*Architectural treatments at the corner define the entry area*



*Walls and window surfaces articulated with recesses, trims, reveals and signage*

### Build-to-Line

The build-to-line establishes a minimum percentage of building space (retail uses, public uses, or active spaces) facing the street with a minimum percentage of windows, doorways, and entry spaces facing onto the sidewalk. Activity spaces opening onto the street, enhance the pedestrian character of the street, provide for visual surveillance into the public realm, and increase the safety and security of the area.

The “Town Center” section of this chapter establishes a set of build-to-lines and their requirements for the town center. Commercial/ Mixed-Use sites in the village centers and neighborhood centers should also establish build-to-lines.

### Materials

Materials used should reflect the style and overall character of buildings. Permanent, long-lasting, low-maintenance building materials are encouraged. These may include stone, stucco, brick, high-quality wood products, steel panel, or flange steel.

### Use of Color

Colors should be used to bring together materials used in the design of the site and can complement the building architecture. Colors should be used to give distinct character to different buildings and different tenants within large building complexes.

### Cornices

When used, cornices should have contrasting colors and materials to the wall surface areas next to them

## COMMUNITY DESIGN

### Roofs Forms:

Roof form should be used to identify and articulate different buildings and distinguish different tenants within a larger building complex.

### Towers

Tower elements may be incorporated into building designs to create identity when appropriate to the building aesthetic. They can provide a change in scale at the street corners or be sited in between buildings to provide focal points and transitions between spaces or buildings in a multi-tenant complex. Tower elements should be used sparingly and not conflict with the rhythm of vertical elements on the block.

### Windows

The shape, size, and placement of windows are major design elements that help establish the style and character of a building's design. Windows, located at the ground floor, along major pedestrian commercial streets, should be large display windows that have a transparent quality, connecting the activities within to the pedestrian experience outside. On upper floors, window design elements such as shutters, canopies, recesses, and other elements should be used to add variety to the building design fronting the street.

### Canopies, Awnings, and Arcades

Canopies, awnings, and arcades should be used along pedestrian streets to add an additional layer of variety, depth, and interest to building façades. Canopies, awnings, and arcades help identify and articulate major entries, cast shadows on the building façades, and may be used to carry additional signage.



*Varied roof heights, tower elements, and arcades help differentiate tenants within a larger building complex*



*Variations in the placement of windows, entries, arcades and awnings help articulate the building facade*

### Signage

Signs should be designed as an extension of the architecture of a building or site and should complement the building or site materials, colors and rhythm of walls, windows, parapets, and other architectural design features. Signage should provide information and identification of shops and tenants, should not be used as advertisement, and are subject to the standards in Policy 6.14 and the standards and requirements of Section 17.54.170 of the *Placer County Zoning Ordinance*. Refer to Section 6.2.4 for signage design guidelines.

Wall signs located on multi-tenant centers shall be designed to be compatible with each other through the coordination of sign type, size, placement, illumination, color, and/or font.

### Lighting

Refer to Section 6.2.5 for lighting design guidelines.



*Signage should be designed as an extension of the architecture of a building or site*

## CIRCULATION AND PARKING DESIGN GUIDELINES

Centers should be designed with internal circulation systems that allow for easy, efficient, and safe, slow speed vehicular movement with well-defined pedestrian and bicycle paths.

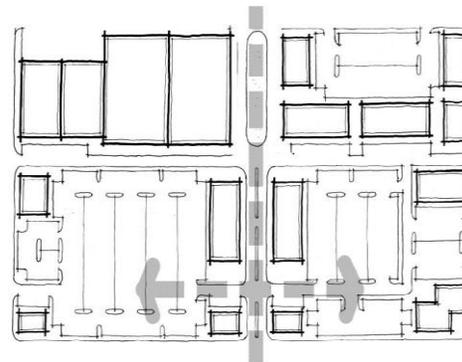
1. Parking areas should be designed with separate vehicular and pedestrian circulation paths and include traffic calming design features. Alternative surface materials are encouraged to differentiate pedestrian circulation paths.
2. On-site pedestrian amenities and bicycle parking should be provided at convenient locations to site destinations with direct, safe, and convenient access to adjoining path and trail systems.
3. Off-street parking should be located at the rear of buildings with separated walkways leading to the street and entryways.
4. Dispersion of parking into smaller units is encouraged.
5. Parking lots of commercial centers and public use areas should contain landscaped areas with large shade trees of sufficient size and shading to surrounding parking spaces (see Policy 6.25).
6. Shared parking arrangements and driveway access between adjoining commercial and office projects are encouraged. Parking standards may be relaxed to facilitate shared parking (see Policy 3.20 for parking reductions in Commercial/Mixed-use sites).



*Parking areas should provide clearly defined pedestrian access, separated from vehicular traffic*

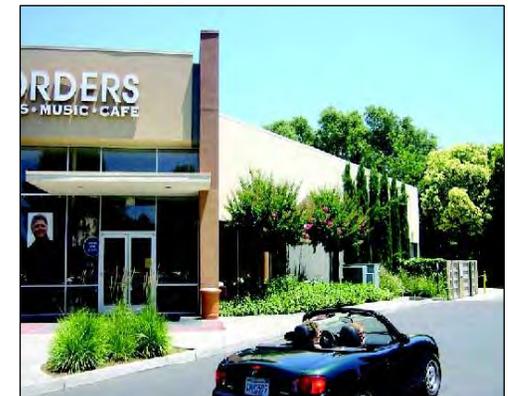


*Place bicycle facilities, seating, and other pedestrian amenities convenient to public destinations*



*Shared parking arrangements and driveways between adjoining commercial and office projects is encouraged*

7. Park and ride lots are provided primarily in the transit center and town center. Other commercial sites are also encouraged to provide smaller park and ride lots as a shared parking use incorporated into the overall parking design.
8. Service areas and loading functions should be integrated into the circulation pattern to minimize conflicts with vehicles and pedestrians. Service areas should be screened from public view with fencing, walls, and landscaping, or a combination of these elements.



*Screen service area and mechanical equipment with landscaping, walls, or a combination of these*

## COMMUNITY DESIGN

### 6.3.3 TOWN CENTER

The Placer Vineyards town center is located in the “heart” of the community, just south of Base Line Road from 16th Street, extending east to 14th Street. The town center provides a “traditional” small-town core and acts as the community focal point for Placer Vineyards. The urban design vision for the town center is focused on a traditional grid street pattern, organized around a town green. The town center is intended to be pedestrian in scale with large tree-shaded streets and retail and civic uses oriented to the street.

The town center is designed as a pedestrian-friendly streetscape environment with buildings located behind wide sidewalks. This streetscape environment allows outdoor eating, display areas, and public art exhibits and features angled street parking for easy access to local shops and stores. Shared parking areas are located within the middle of each block with buildings oriented toward the street and the town green.

**Goal 6.19** Create a mixed-use, pedestrian-friendly town center, including residential developments that are higher density and mixed-use; loft spaces; professional offices; traditional retail stores, neighborhood-serving retail, and services; entertainment uses, such as movie theaters and live entertainment venues, restaurants, and cafes; and a range of public uses, including a library, and civic buildings for government offices, community meetings, and public gatherings.

#### **Policy 6.27 Town Center Design.**

*The town center shall be designed to serve as the public, institutional, and social focal point for the community. The town center will contain:*

- ◆ *A site for a centrally located government center and sheriff station that anchors and serves as a visual landmark for the town green,*
- ◆ *A library site, and*
- ◆ *A religious site.*

#### **Policy 6.28 Town Center Design Standards.**

*Within the town center, the following standards shall apply. Refer also to Appendix A, Section 3.4 for Town Center Commercial development standards.*

1. *At least 70% of the frontage at a build-to-line shall be occupied by a building with the façade placed at the back of the sidewalk (see Figure 6.6, “Town Center Design Framework” for the location of build-to-lines for the town center).*

2. *At least 70% of the façade of the first floor of all buildings in the town center on the blocks with build-to-lines shall have transparent storefront glass, windows, entries, doorways or other active spaces fronting onto the public streets and sidewalks.*
3. *Buildings in the town center shall be a minimum of 25 feet in height.*

#### **Policy 6.29 Town Center Sidewalk Design.**

*Sidewalk designs shall be guided by Figures 6.8 through 6.11 and the following:*

1. *Any sidewalk that fronts on to commercial development shall maintain a minimum 6-foot wide clearance for pedestrian traffic.*
2. *All sidewalks shown in street section T2 shall be a minimum of 16' in width. Sidewalks shown in street section T3 shall be a minimum of 12' in width. All other sidewalks shall be guided by widths shown in the appropriate street sections.*
3. *Sidewalks at outdoor eating areas shall be a minimum of 16' in width unless buildings are designed to inset a portion of the outdoor eating area onto the commercial development site.*
4. *The width of sidewalks as part of the overall Town Center design will be reviewed during the Design/Site Review process.*

#### **Policy 6.30 Parking in the Town Center.**

*The following parking standards apply to the town center (refer also to Appendix A, Section VI: “Parking Standards”):*

1. *For mixed-use projects, parking may be shared between uses, as defined in Policy 3.20.*
2. *Within the town center, all on-street parking will be used in the calculation of parking requirements for all land uses. Calculation of on-street parking includes all parallel parking and diagonal parking along the subject parcel boundaries.*

3. *Off-street common parking areas shall be located in the middle of most blocks where feasible. Reciprocal access and use agreements shall be recorded between property owners of the businesses that use the parking facility.*
4. *Off-street parking lots shall be screened with shrubs, trees, landscaped berms, and low fences, walls, or other landscape design elements.*
5. *Parking structures, if required or desired, shall be architecturally incorporated into the design of surrounding buildings and landscaping in the town center.*
6. *Parking structures facing onto the retail streets shall incorporate ground-floor retail uses along the street.*



*Town Center Site Location Diagram*

Figure 6.5 - Town Center Illustrative Site Plan

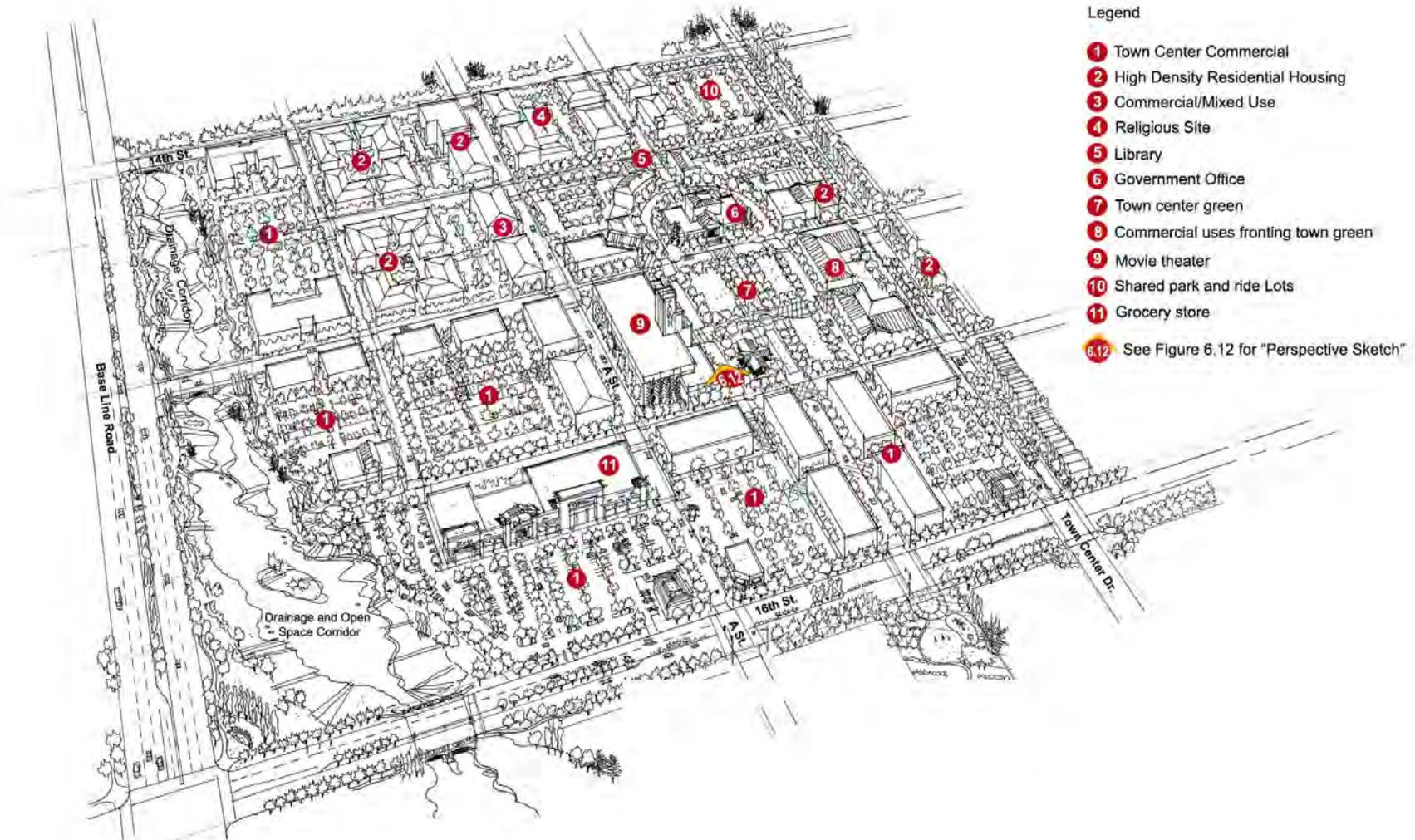
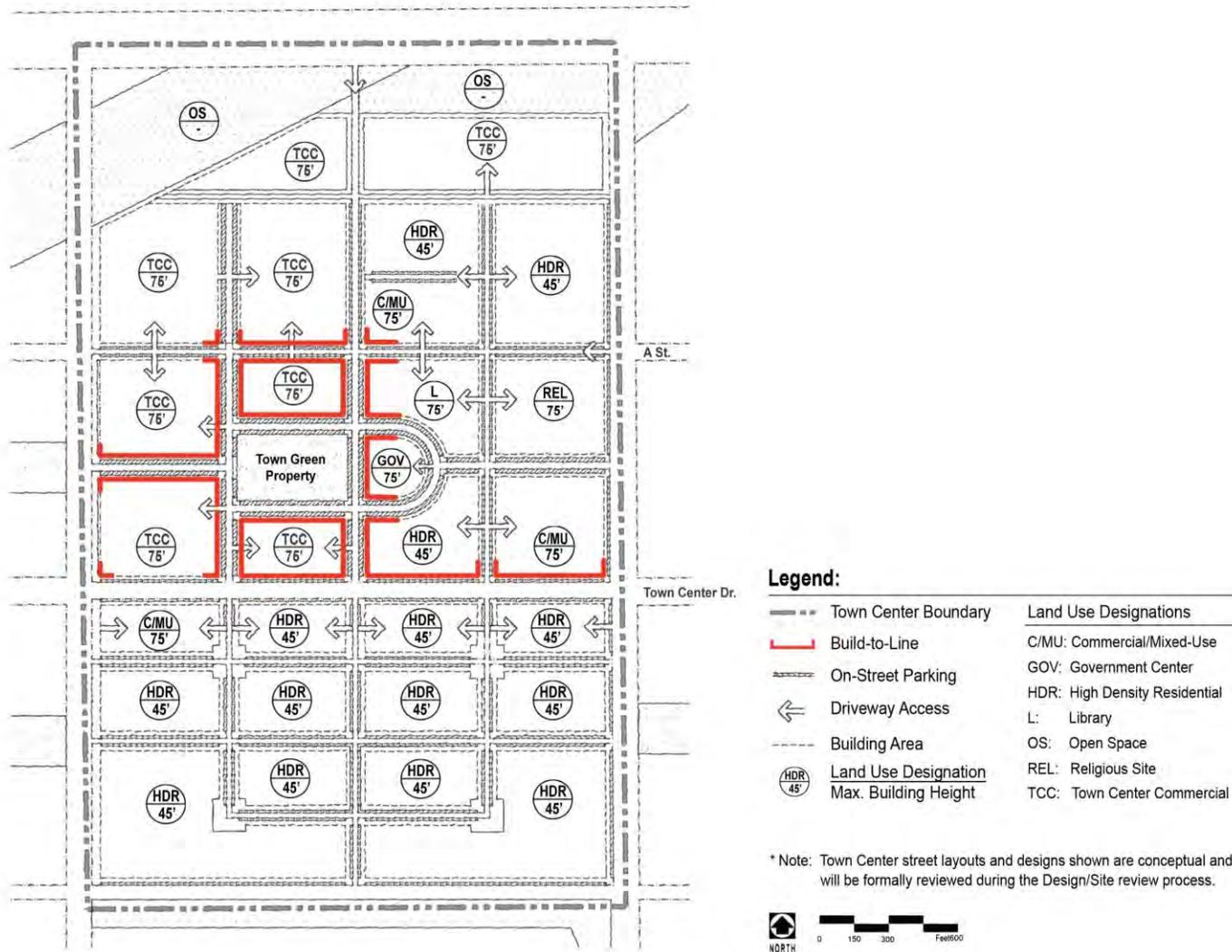


Figure 6.6 - Town Center Design Framework



COMMUNITY DESIGN

Figure 6.7 - Town Center Massing

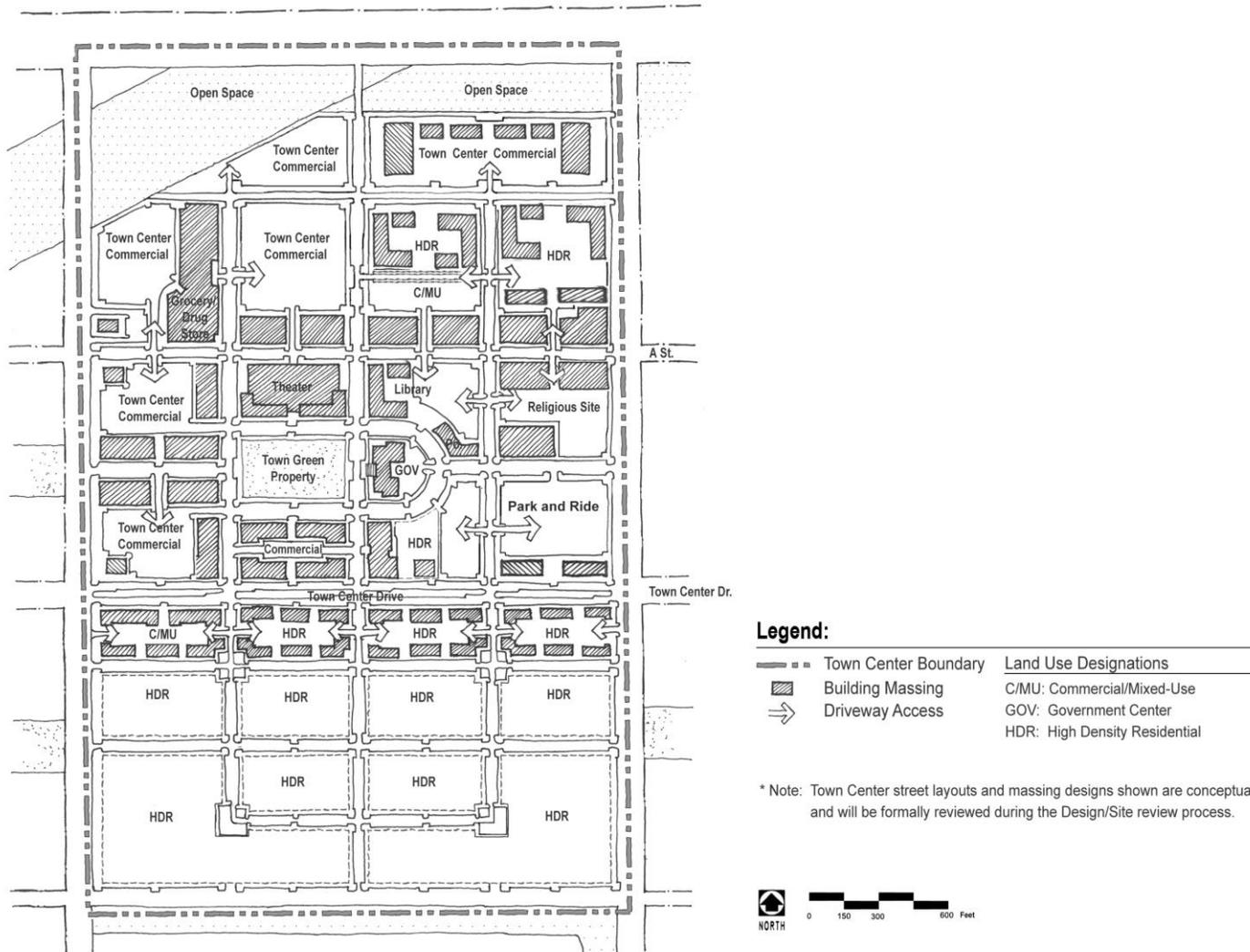


Figure 6.8 - Town Center Parking and Streetscape

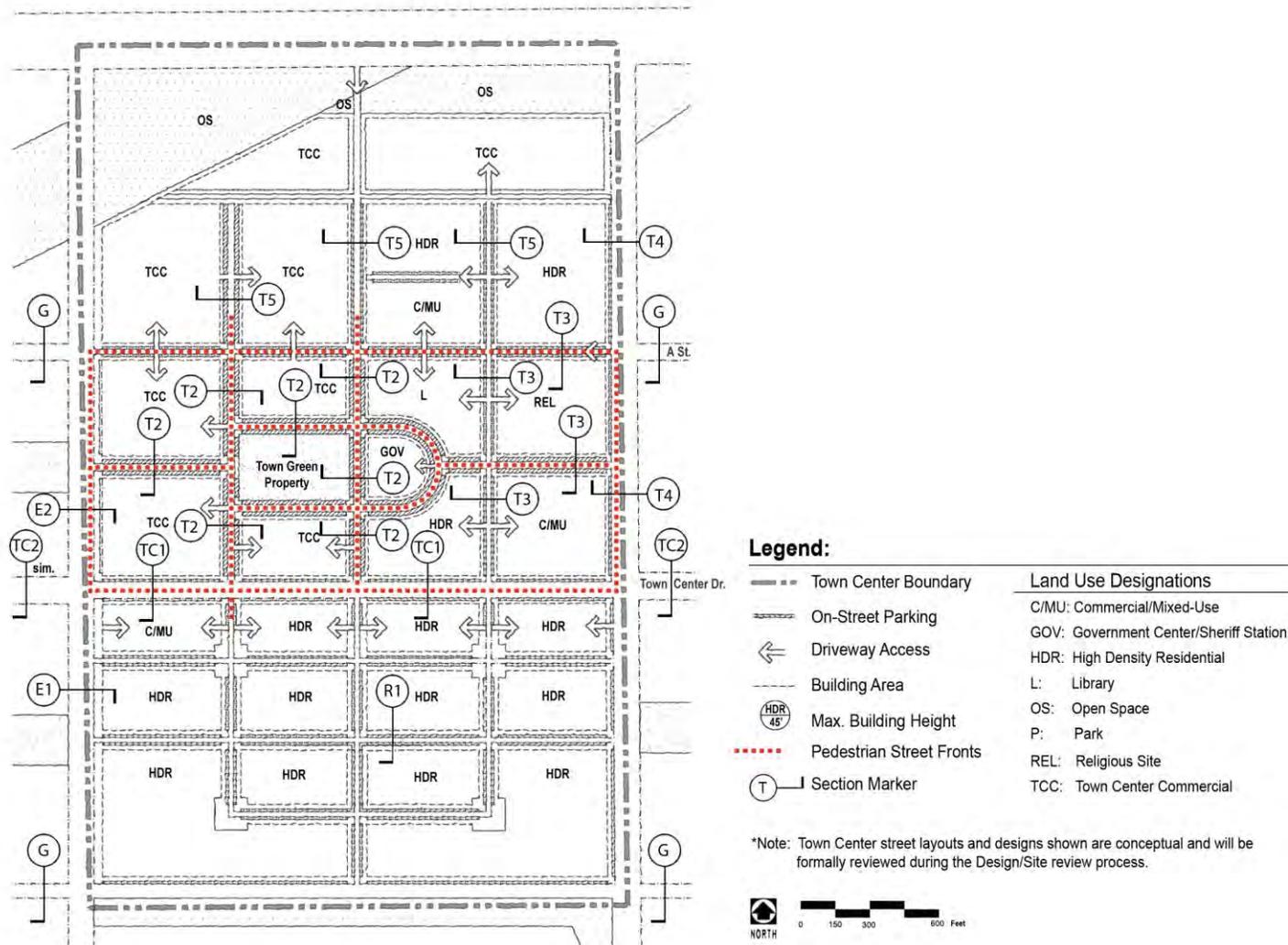


Figure 6.9 - Town Center Street Section

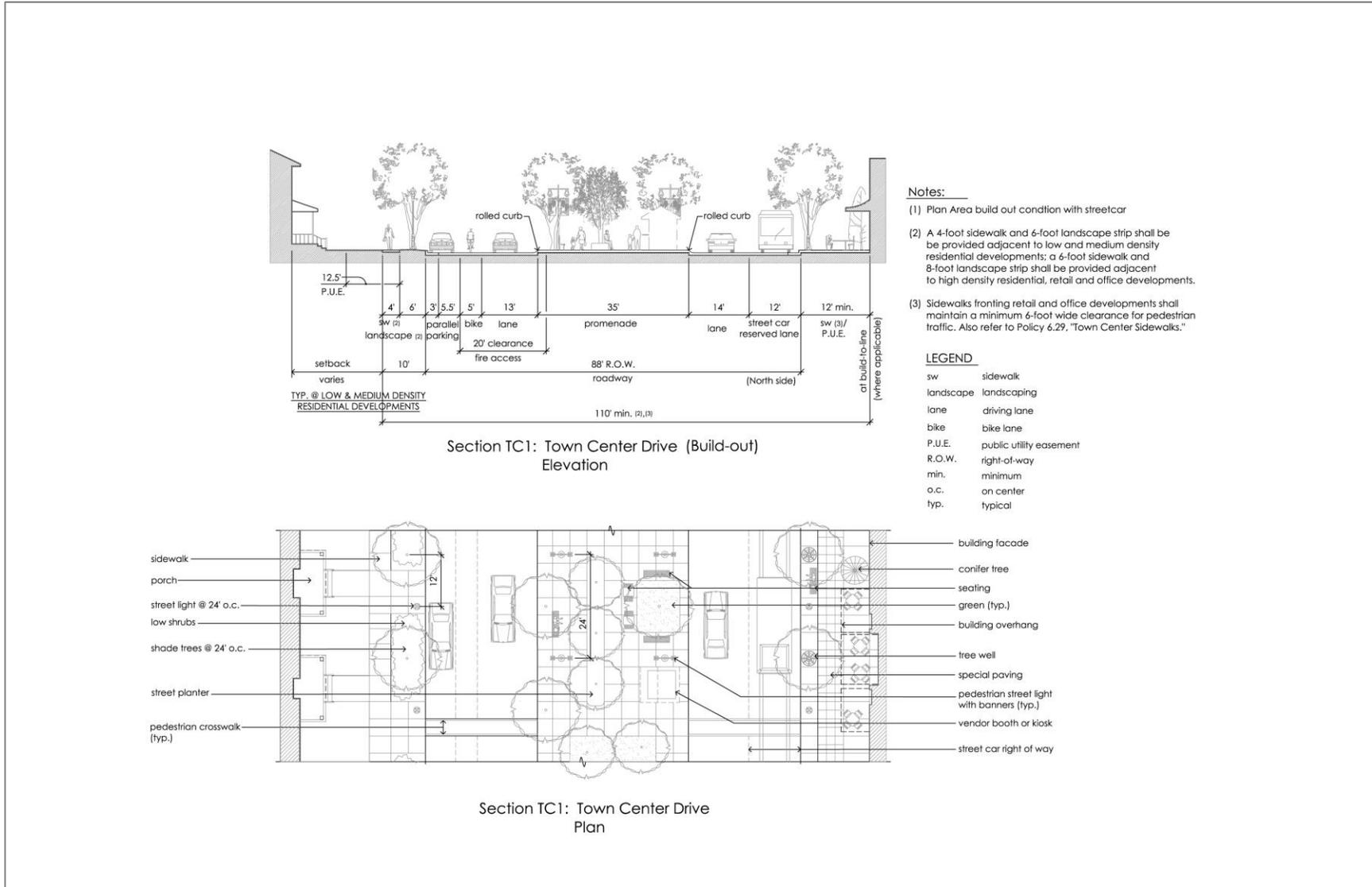


Figure 6.10 - Town Center Street Section

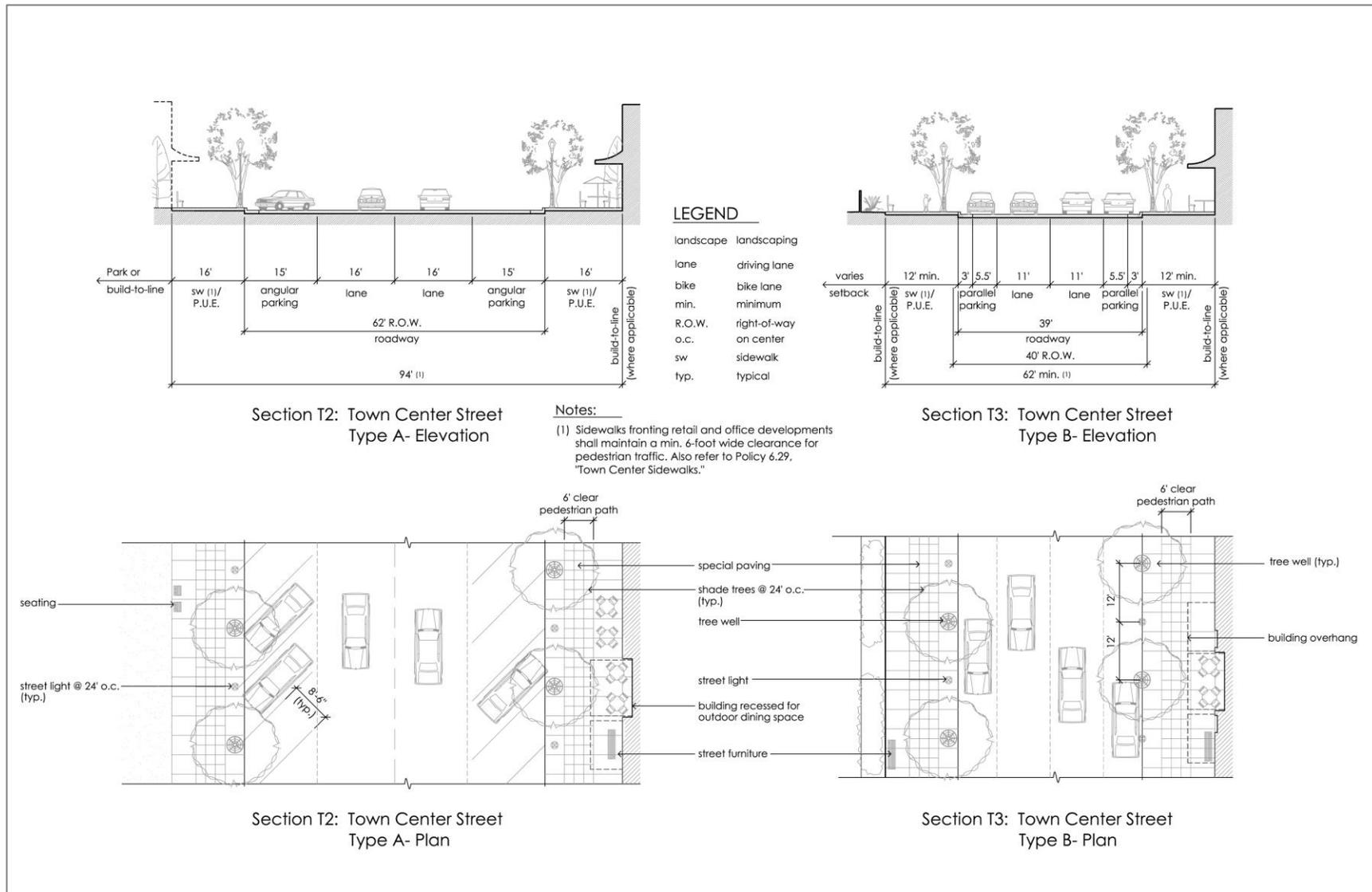


Figure 6.11 - Town Center Street Section

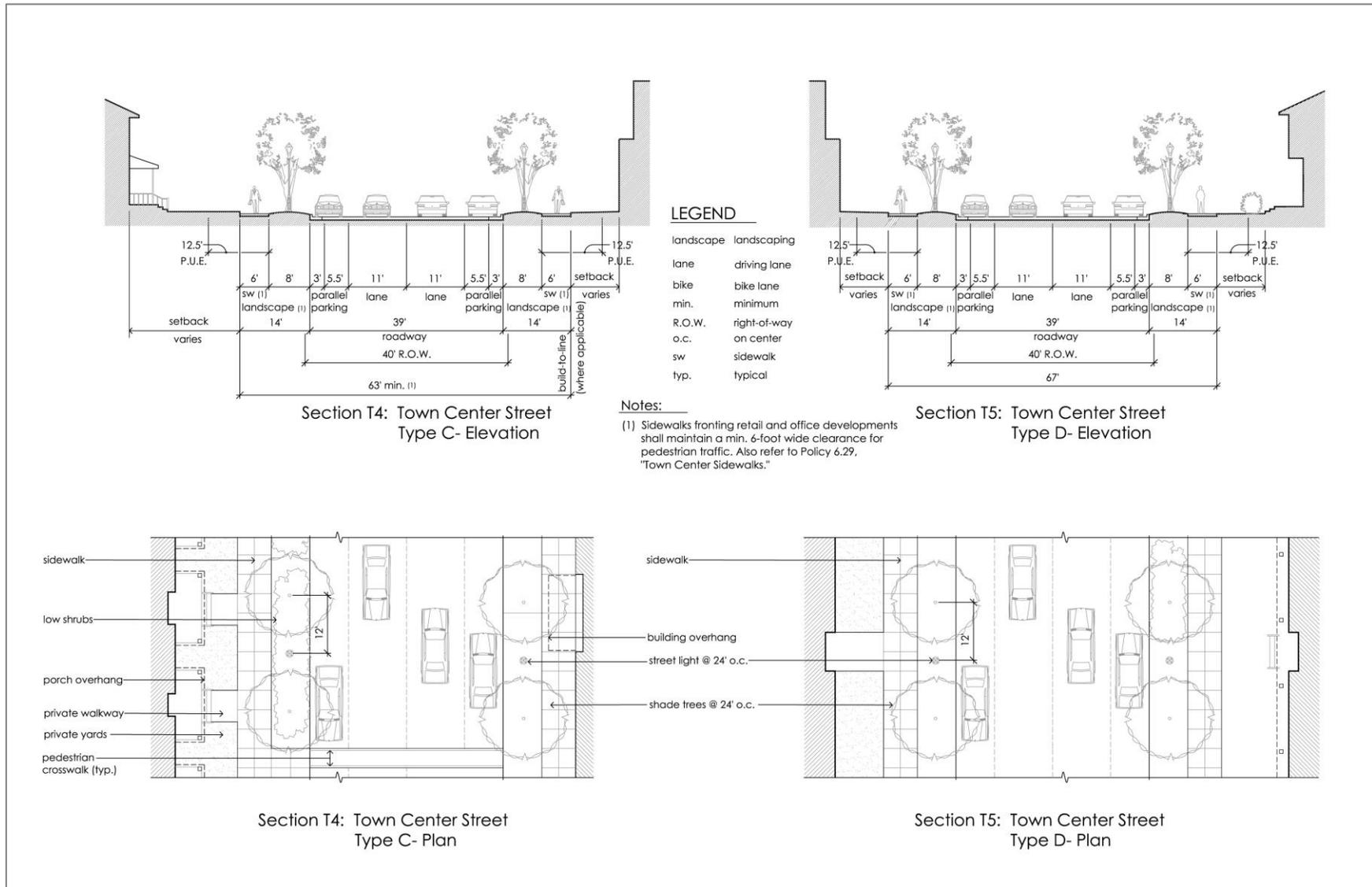


Figure 6.12 - Town Center Perspective Sketch



## COMMUNITY DESIGN

### TOWN CENTER SITE AND BUILDING ORIENTATION DESIGN GUIDELINES

Recognizing that the build-out of the town center will occur over a 20-30 year time frame, development within the town center shall be guided by the following.

1. The town center is centered on a “town green,” designed as an urban park and intended to function as the formal outdoor, public gathering place for the community. Refer also to Chapter VII for a description and conceptual diagram of the town green.
2. The town green should provide convenient pedestrian access and circulation from other areas of the town center, comfortable seating areas protected from the sun, and other pedestrian amenities, such as large-canopy shade trees, water features, public art, and a small outdoor band shell or space equipped for portable performance set-up for public events.
3. Buildings in the town center fronting onto the public-streets and town green are encouraged to be surrounded with outdoor eating and dining areas with cafes, coffee shops, restaurants, and other food services located on the periphery. See Figure 6.8 for a conceptual massing diagram.
4. The town center should consist of compact districts contained within a regular grid street pattern to create small, walkable blocks in the commercial core. In general, blocks should typically be about 300 feet in length.
5. In limited areas, parcels may be aggregated to form larger development sites to accommodate a variety of retail and office uses in the town center and to better serve the community shopping needs, such as, a grocery store, hospital site, or department store. Refer to Appendix A, “Land Use and



*A mix of first and second story uses in the town center*



*Illustration of build-to-line*



*Town center commercial street articulated with the use of planters, bollards, and decorative paving*

Development Standards,” for commercial site design standards.

6. In the event that a large site aggregation occurs, driveways and pedestrian linkages serving these large parcels should be located mid-block to better integrate the development into the urban pattern and preserve the street grid.
7. Buildings on larger aggregated sites should be organized to fit into the overall urban character of the town center and avoid large parking areas separating the retail use from the town center core.
8. Commercial streets surrounding the town green should be designed with diagonal parking and corner bulb-outs to increase pedestrian safety and access across streets.
9. Sidewalks should be designed with appropriate and coordinated pedestrian furniture, including seating, trash receptacles, pedestrian lighting, newspaper racks, bicycle parking areas, drinking fountains, signs, information kiosks, and bus shelters.
10. Additional landscape amenities may be provided as decorative elements such as landscape planters for flowers, specialized ground cover, corner bulb-outs, flower boxes along outdoor eating areas, and hedge planting used to screen surface parking lots.
11. Pedestrian and bicycle paths should be designed appropriately to provide for safe pedestrian access, circulation, and connections to surrounding residential neighborhoods.

12. A mix of ground-floor and second-story uses is encouraged, particularly adjacent to the town green. Second-story uses are encouraged at street intersections and to terminate vistas along the major axis of the town green.
13. Street elevations within the town center should reflect varying styles and should not be limited to one particular theme or style. The scale and massing of the elevations should have a pedestrian scale, creating visual interest, variety, and avoiding continuous blank walls.
14. All sides of buildings visible to the public should be designed as active, interesting façades. Service areas and façades that are not generally visible from the public streets and open spaces may be simpler in design treatment.

**TOWN CENTER BUILDING DESIGN GUIDELINES**

The overall building style of the town center should be developed to use traditional materials and building forms to create an architectural character that is unique to Placer Vineyards.

**Roofs Forms**

Roof forms should be used to identify and articulate different buildings and distinguish different tenants within a larger building complex. Roof forms in the town center may include flat roofs, pitched and hipped roofs, and other distinctive roof shapes and forms. Pedestrian areas should be enhanced by shed and gabled roof elements that extend into the pedestrian realm as arcades to provide cover and shade. Dormer elements are encouraged to provide an added layer of detail, shadow, and variety to roof forms. Use of contrived or fake mansard roof elements to screen roof top equipment should be avoided.



*Varied architectural features and outdoor furniture accent the mix of ground-story and second story uses*



*A tower element can enhance the identity of the Town Center and serve as a public focal point or landmark*

**Cornices**

Varied cornice elements help to give the town center a “built-over-time” appearance.

**Canopies, Awnings and Arcades**

Canopies, awnings, and arcades should be used to provide the patrons of the town center with protection from the weather during winter rainy months and hot summer months, to identify and articulate the pedestrian way and major entries, or to carry additional signage.

**Towers**

Tower elements situated and massed appropriately can enhance the town center’s identity and character. Towers can serve as focal points and transitions between spaces. One larger tower should be incorporated in the design of one of the town center community buildings to identify it with a landmark, visible from the surrounding community.

**Windows**

Window sizes and proportions should be appropriate to the individual building styles and uses. Window forms and shapes may vary from building to building to subtly reflect a “built-over-time” appearance in the town center.

**Use of Color**

Colors should be consistent within the town center district. Colors should be used to bring together materials used throughout the town center, to complement the building architecture— stone, concrete, wood, stucco, glass, fabrics, or other materials. Colors should be used to give distinct character to different buildings and different tenants within large building complexes.

## COMMUNITY DESIGN

### Materials

Materials used should reflect the style and overall character of buildings. The materials used for the Placer Vineyards town center should be high quality, long lasting, and low maintenance. Use of the following materials in the town center is encouraged:

- ◆ Stone,
- ◆ High-quality wood products,
- ◆ Brick,
- ◆ Steel,
- ◆ Smooth stucco finishes, and
- ◆ Glass

Use of the following materials in the town center is discouraged:

- ◆ Heavy stucco finishes,
- ◆ Contrived stone veneers (stucco stone),
- ◆ Unfinished tilt-up wall panels,
- ◆ Large unbroken window walls,
- ◆ Exposed concrete block walls, and
- ◆ Exposed aggregate walls



*A variety of materials – brick, stone, stucco and glass are encouraged to be used in the Town Center*



*Signage and lighting in the Town Center should reflect the overall character and theme of the site*

### Signage

Signs in the town center should reflect its overall character and theme and preserve the integrity of the building's architecture. The form, size and fonts used in the town center signage systems should vary from building to building and tenant to tenant to create visual interest and give the effect of a cumulative building process. Pedestrian-scale permanent monument signs are also encouraged at information kiosks to guide the visitor from one point to another throughout the town center and the Plan Area.

Generally, the main identification signs should be mounted flush to the building façade. Projecting signs extending over pedestrian walkways designed as iconic symbols of the retail use are encouraged. Freestanding monument signs within the town center core area are discouraged. Neon signage, if used, should be artistically applied as a contrasting element in the building façade.

### Lighting and Pedestrian Furniture

Pedestrian-scale street lights shall be provided at an appropriate spacing within the town center, located at street intersections, parking lot access points, pedestrian alleyways, and walkways. Streetlights should be equipped with standards for hanging decorative banners, flags, and flower baskets.

### 6.3.4 VILLAGE CENTERS

Two mixed-use village centers are planned for Placer Vineyards. One village center is located on the east side of the Plan Area, along Watt Avenue. The second village center is located on the west side on Dyer Lane. These village centers are pedestrian-oriented, mixed-use activity nodes, providing retail sales and services to the immediate surrounding neighborhoods.

Village centers are intended to provide a unique, small cluster of activities, with offices and residential uses above ground-floor retail uses. Streets and pedestrian and bicycle paths connect the village centers to surrounding residential neighborhoods. Buildings are clustered around small neighborhood plazas, greens, or gathering places where local residents can meet for activities and events.

Village centers are intended to provide a mix of uses with office or residential uses either adjacent to or above ground-floor, neighborhood-serving, retail activities. Potential village center retail sales and services may include uses such as delis, grocery stores, specialty food stores, drug stores, cafes, and restaurants, bakeries and donut shops, laundry facilities, hair salons and barber shops, gas stations, video stores, and offices.

**Goal 6.20** Provide two village centers in Placer Vineyards.

**Policy 6.31 Village Centers.**

*Village centers are higher density centers surrounded by high-density residential neighborhoods and located within one-quarter mile walking distance of lower density residential neighborhoods. Two mixed-use, pedestrian-oriented village centers will be created in Placer Vineyards. The east village center is located at the intersection of Watt Avenue and Town Center Drive. The west village center is located at the intersection of West Dyer Lane and Town Center Drive.*

*Village centers incorporate civic and public uses such as a public green or plaza, day care uses, religious facilities, a post office, transit stops, and other similar uses that support civic activity and community life.*

**Policy 6.32 Transit Access**

*Village centers shall be transit-oriented activity nodes. Bus turnouts, shelters, and clear pedestrian paths from the street to the commercial centers, transit centers, parks, and other public facilities should be incorporated into the design of the village centers.*



*Village Center Site Location Diagram*

## COMMUNITY DESIGN

### WEST VILLAGE CENTER

The west village center, located along Dyer Lane and Town Center Drive, provides a cluster of mixed-use commercial and higher intensity residential uses in the vicinity of surrounding residential neighborhoods and a range of public facilities, the new high school, and local religious facilities. The west village center also provides connections to open space corridors and roadways.

A range of locally serving retail uses are encouraged in the west village center. Such uses include restaurants and cafes, grocery stores, drug stores, delis and specialty food stores, hair salons and barber shops, laundry and dry cleaning services, video stores, hardware stores, wine stores, liquor stores, gas stations, bakeries, ice cream shops, shoe stops, appliance stores and repair shops, and other neighborhood serving goods and services. In addition, the west village center provides opportunities for a range of small offices and professional services such as dentists and doctors and accounting and real estate offices.

The west village center is designed as a mixed-use, pedestrian-and transit-oriented (bus stop) center. It is designed with higher density residential uses or offices over ground-floor retail uses, located within a 5-minute walk (roughly one-quarter mile) from the surrounding neighborhoods.

Figure 6.13 - West Village Center Conceptual Site Design



#### Legend

- 1 Community Park
- 2 CMU Site
- 3 Medium Density Residential
- 4 High Density Residential

### EAST VILLAGE CENTER

The east village center, located at the intersection of Watt Avenue and Town Center Drive, will feature a range of locally serving retail uses and services and include a transit center and fire station. The east village center is larger than the west village center and is intended to create a higher density, pedestrian-and transit-oriented, mixed-use node of activity.

A range of locally serving retail uses are encouraged in the east village center. Such use include neighborhood retail goods and services, restaurants and cafes, grocery stores, drug stores, delis and specialty food stores, hair salons and barber shops, laundry and dry cleaning services, video stores, hardware stores, wine stores, liquor stores, gas stations, bakeries, ice cream shops, shoe stops, appliance stores, repair shops, and other similar neighborhood-serving goods and services. In addition, the east village center provides opportunities for a range of small offices and professional services such as dentists and doctors, accounting and real estate offices, public and quasi-public uses, and facilities such as day care, religious facilities, outdoor plaza, and gathering places.

The transit center provides a major bus stop and park-and-ride facilities for a bus transfer station and future bus rapid transit (BRT) service planned for Watt Avenue. It also provides connections to a streetcar system with services to the town center and to an internal bus system that will provide links to the neighborhoods, major community facilities, schools, and the town center.

Direct pedestrian and bicycle access is provided from surrounding residential neighborhoods and along adjacent open space corridors.

Figure 6.14 - East Village Center Conceptual Site Design

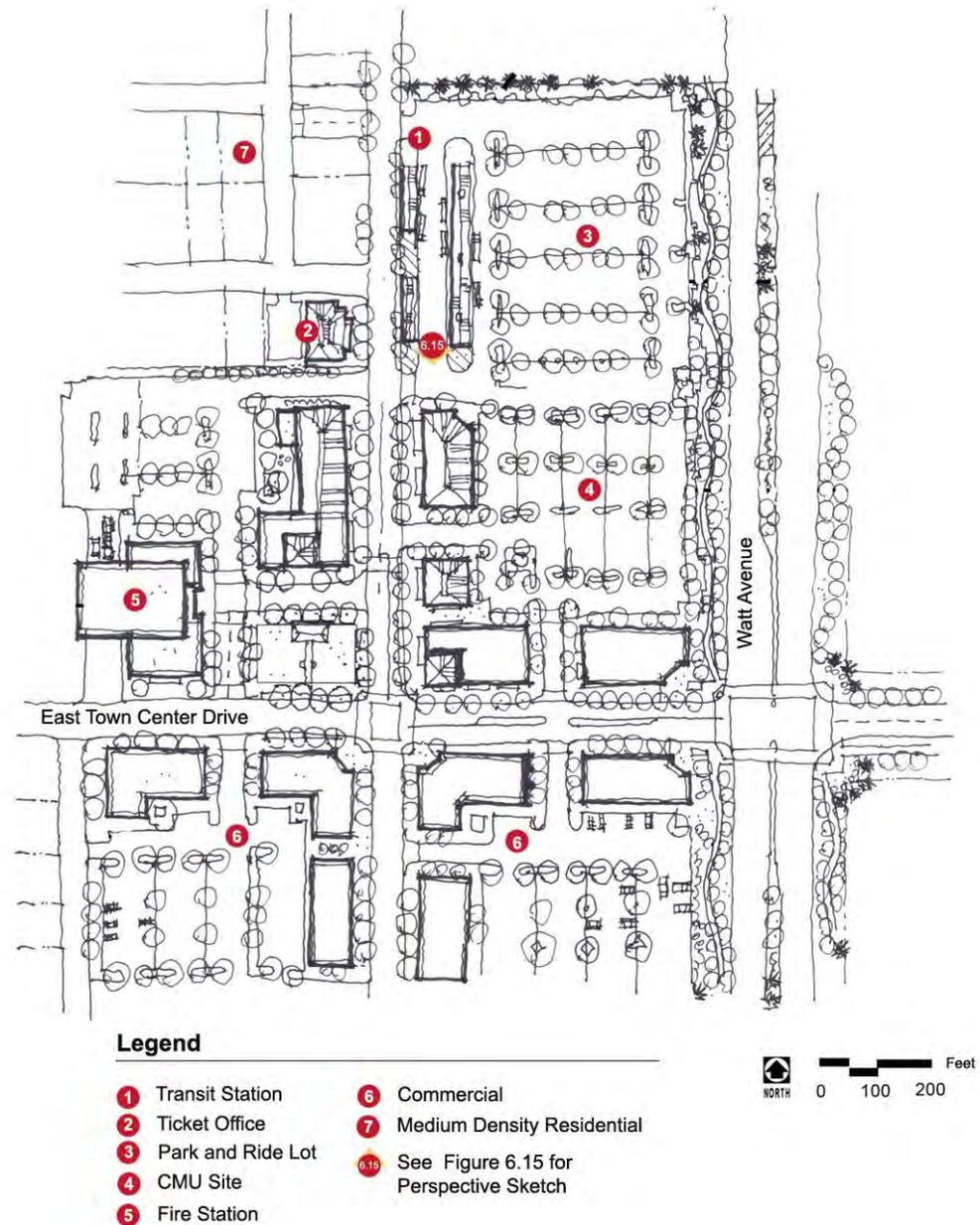
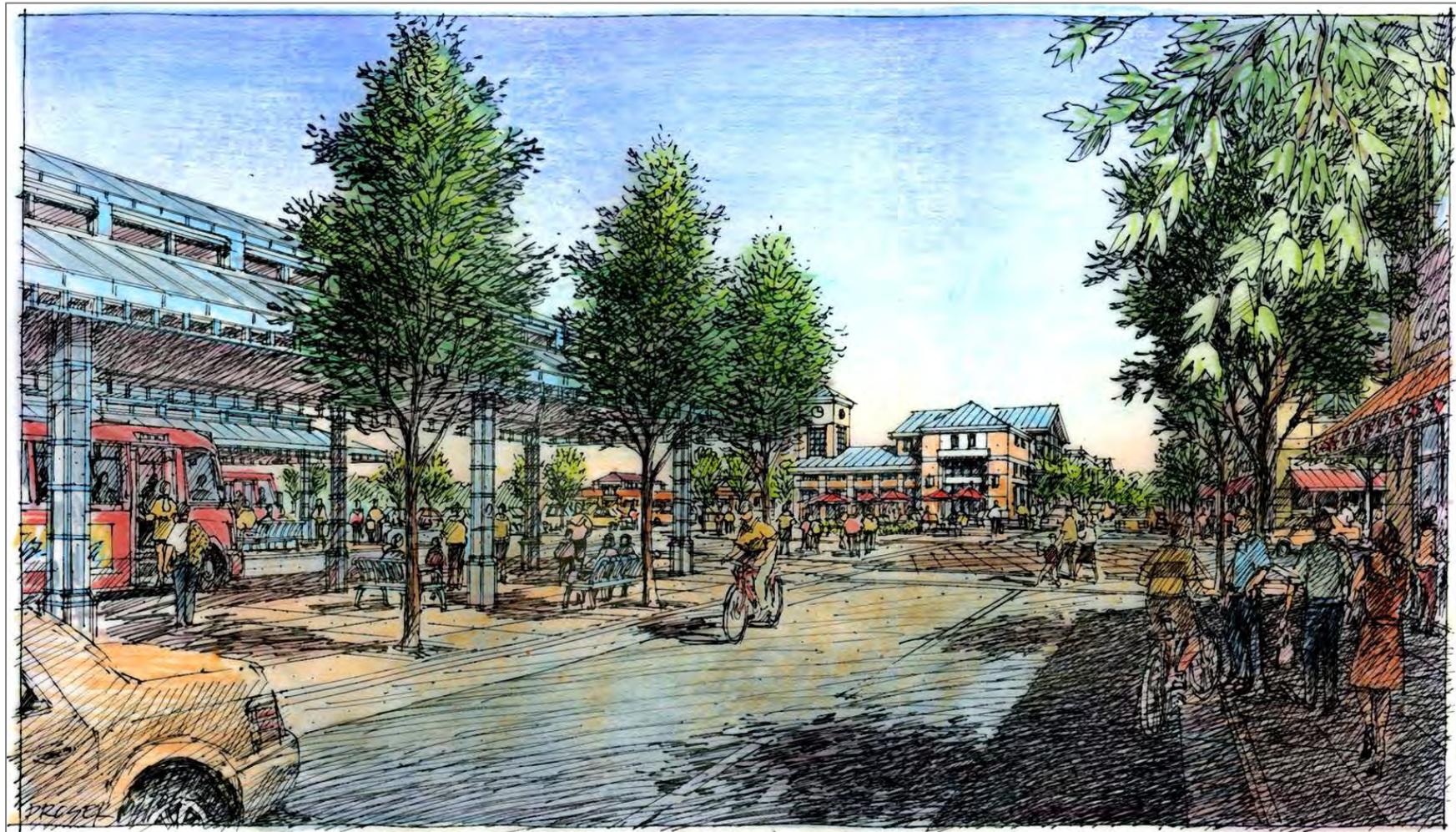


Figure 6.15 - East Village Center Perspective Sketch



**GENERAL VILLAGE CENTER DESIGN GUIDELINES**

**Streetscape Design Guidelines**

Walkways in the mixed-use village centers should be designed to create a pedestrian-friendly environment, should be urban in character, and should be designed with the following features:

1. Wide sidewalks designed to support the active pedestrian environment with adequate widths for strollers and wheelchairs to pass one another on the same walkway.
2. Curb ramps and design features to emphasize the pedestrian movement across traffic lanes at key intersections of thoroughfares, arterials, and collector streets.
3. Textured or colored pavement highlighted with clear white striping and bulb-outs or curb extensions at major crossings, used to improve the visibility and safety of pedestrians.
4. Attractive and consistent signage and pedestrian-scale lighting to create a sense of place and identity within Placer Vineyards.
5. Comfortable and coordinated street furniture.

**Site Design Guidelines**

1. Village centers are encouraged to provide higher intensity housing and commercial uses within a convenient, 5-minute walking distance from the center.
2. The village center should be organized to provide convenient walking connections from the surrounding neighborhoods.



*A pedestrian plaza gives identity and a gathering spot for the Village Center*



*Landscaping and street furniture create a pedestrian-scale environment for the Village Center*

3. The building and site design of the village center should create a pleasant and enjoyable place that makes walking attractive and preferable to driving. The pedestrian experience should include large-canopy street trees, landscaped spaces, and seating areas.
4. Streets and pedestrian paths should be interconnected with the surrounding neighborhood to encourage walking and cycling and distribute traffic to minimize volumes on local streets.
5. Transit stops should be located in the core of the village center with shelters, seating and other pedestrian amenities all in close proximity to other village center uses.
6. Parking for the village centers should be clustered in smaller parking courts behind buildings, away from the main public view, and should allow for shared use by all village center land uses and property owners.
7. For mixed-use projects, parking may be shared between uses, as defined in Policy 3.20.
8. Pedestrian amenities, landscaping, furniture, signs, and lighting should conform to a common landscape design theme to help provide a consistent village image and character. The theme should strive to create a consistent and pedestrian-scale treatment to buildings and the landscape without being overly historical in approach, or relying on styles that may become outdated in the near future.

## COMMUNITY DESIGN

### Building Design Guidelines

1. Each village center should have its own unique design elements that distinguish it from other village centers.
2. Buildings in the village center should be designed with a consistent theme or architectural character, using a similar family of building materials, building styles, design elements, and use of color and details. Earth tones and natural materials are encouraged.
3. Buildings should be clustered close to street sidewalks and provide multiple windows and entries on the ground floor to activate the pedestrian space and increase the safety and visual control of the street.
4. Use of architectural elements that create a more active street life and pedestrian scale at the street level are encouraged. Such elements include ground-floor commercial windows; entries; awnings, overhangs, and arcades; outdoor seating, eating areas, and sidewalk seating; street trees for shade; pedestrian-scale lighting; signage; public art; and other streetscape elements.



*Building materials, colors, and architectural features should be coordinated to give a unique identity to the village center*



*Buildings with architectural elements on the ground floor and street furniture close to the sidewalks provide interest and activity for the pedestrian*

### 6.3.5 BASE LINE ROAD REGIONAL COMMERCIAL CORRIDOR

The Base Line Road commercial corridor is intended to provide services and promote a balance of employment in the region, as well as generate revenue for the County. It includes business parks, offices, regional commercial centers, and a power center.

**Goal 6.21** Provide attractive commercial development and a mix of uses along Base Line Road that provide employment, attract economic development, a diverse base of tenants, and is easily accessible to the community.

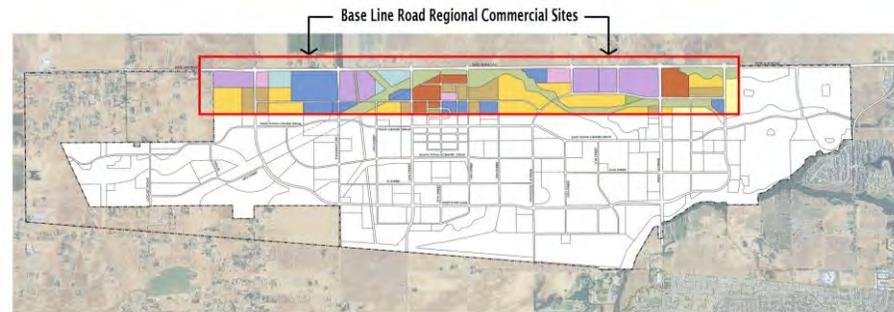
**Policy 6.33 Base Line Road Commercial Corridor.**

*The Base Line Road commercial corridor offers products, services, and employment catering to the broader Placer Vineyards region. It is guided by the following standards:*

1. *The Base Line Road commercial corridor will provide easy access and transportation connections to neighborhood areas;*
2. *Direct access connections shall be avoided from Base Line Road (see Policy 5.12, Access within the Development Site);*
3. *Structures in the Plan Area (not including parking and landscaping uses) located immediately south of lands in active rice farming or lands under the Williamson Act contract, shall be setback 200-feet from the existing Base Line Road northern edge of pavement (see Policy 3.28 and Figure 3.4).*

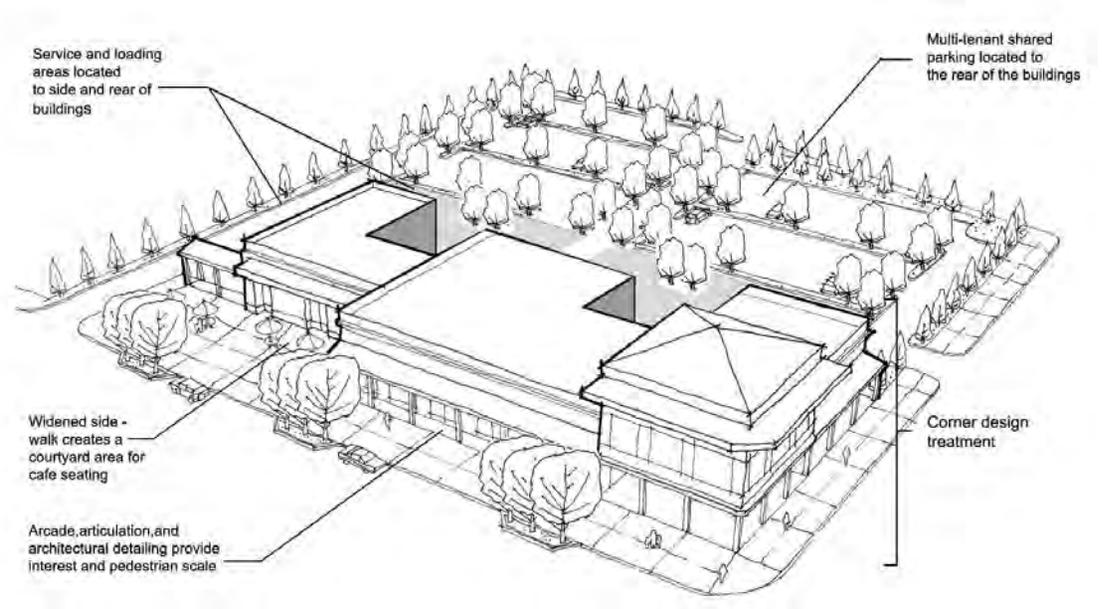
**Policy 6.34 Commercial Center Design.**

*Commercial buildings located next to Base Line Road and a 50-foot landscape corridor shall provide buffers internally in the Plan Area to the noise and traffic generated on Base Line Road.*



*Regional Center Site Location Diagram*

**Figure 6.16 - Conceptual Commercial Center Site Design**



## COMMUNITY DESIGN

### ***Policy 6.35 Location of the Power Center.***

*A Power Center will be strategically located in the Base Line Road commercial corridor at the southwest corner of Watt Avenue and Base Line Road to supply large-volume goods and services.*

### **Design Guidelines for Regional Commercial Centers**

1. Encourage buildings to be clustered to allow for internal courtyards and landscaping that minimizes views of parking areas and allows separation of parking and vehicular traffic from the pedestrian experience. Place buildings in close proximity to high-use pedestrian and transit streets to shorten the distance between transit and building entrances.
2. Design for site accessibility
  - a. Provide wide sidewalks and walkways from parking areas and transit stops.
  - b. Provide bicycle facilities, seating, and other pedestrian amenities at a convenient location to building destinations.
  - c. Use textured, colored pavement, and on side streets with no thru-traffic. signage to delineate pedestrian areas and bikeways from vehicular areas.
  - d. Provide plenty of shade along sidewalks, commercial frontage, and access routes
3. Design commercial sidewalks at a comfortable width to allow for adequate pedestrian access and visibility between adjoining retail storefronts and outdoor activity.



*Cluster buildings around courtyards to insulate the pedestrian from the traffic and view of the parking areas*



*Delineate pedestrian walkways from vehicular surfaces*

- a. Design sidewalks with appropriate and coordinated pedestrian furniture including seating, trash receptacles, pedestrian lighting, newspaper racks, bicycle parking areas, drinking fountains and signs.
  - b. Establish an internal primary street front but allow variations in the placement of buildings that front directly onto commercial sidewalks. This variation is intended to accommodate building entries or additional café seating, landscape courtyards, and plaza spaces that function as outdoor spaces and encourage pedestrian activity.
4. Give special design treatment to street corners which are the most visible areas of the site and natural focal points. Buildings are encouraged to be placed at street-corner intersections.
  5. Organize buildings on larger aggregated sites to avoid large parking areas that separate the pedestrians from their destinations.
    - a. On-street parking is encouraged within the parking lots of regional commercial sites or on side streets with no thru-traffic.
    - b. Shared parking is encouraged on multi-tenant sites.
    - c. Plant deciduous canopy trees in parking lots and provide shade along sidewalks (see Policy 6.25 for parking lot shading requirement).
  6. Separate access for loading from the primary driveway access. Loading areas and trash containers are encouraged to be located behind buildings or to the sides of buildings, accessed by service alleys, or screened by walls and landscaping.

### 6.3.6 NEIGHBORHOOD COMMERCIAL CENTERS

Neighborhood commercial centers are intended to provide a range of neighborhood-oriented retail services and products to the residential neighborhoods immediately surrounding them. These centers are sized to allow major tenants, such as supermarkets, drug stores, and hardware stores, as well as convenience service stations, fast-food restaurants, and support office uses, including real estate, insurance and dental offices.

Generally sited on the corners of major arterial and collector intersections, neighborhood commercial centers are located near higher density residential uses, public/ quasi-public uses, and parks and open space.

More than just convenience retail centers, neighborhood commercial centers provide focal points of activity within the local neighborhood. Designed to encourage pedestrian access, they serve as local gathering places that enable other forms of neighborhood activity and interaction to occur.

**Goal 6.22** Provide local neighborhood services within the community designed to be easily accessible and pedestrian friendly.

**Policy 6.36 Neighborhood Commercial Centers.**

*Neighborhood commercial centers are mixed-use core areas that provide local services and retail to serve the surrounding neighborhoods. These centers will provide neighborhood commercial needs, offering professional services, public/ quasi-public facilities, high density residential uses, and easy access to transit services.*

**Policy 6.37 Pedestrian Access.**

*Neighborhood commercial centers shall be designed to encourage pedestrian access along the face of commercial buildings and along public sidewalks.*

**Policy 6.38 Auto Access.**

*Auto access connections shall be designed to slow and discourage cut-through traffic with the use of traffic calming devices, stop signs, or delineated pedestrian crossings and other features.*

**Policy 6.39 Transit Access.**

*Bus turnouts, shelters, and clear pedestrian paths from the street to the major commercial tenants shall be incorporated into the design of neighborhood centers.*



*Neighborhood commercial area with outdoor seating along pedestrian walkway*



*Mixed-use neighborhood commercial center integrated into the surrounding residential neighborhood*

### Design Guidelines for Neighborhood Commercial Centers

1. Covered walkways and awnings should front on to major anchor stores and connect with other multi-tenant retail shops.
2. Bicycle and pedestrian trails should be provided to allow convenient access between neighborhood commercial centers and surrounding residential neighborhoods.
3. Buildings should be oriented to and located next to pedestrian walkways and street edges. Parking should be placed behind or to the side of buildings to minimize its appearance.
4. Pedestrian-scale street lights shall be provided at appropriate spacings, at street intersections, within parking lots, and along pedestrian alleyways and walkways. Street lights should be equipped with standards for hanging decorative banners, flags, and flower baskets. The maximum height for street lighting shall be 14 feet.
5. For mixed-use sites, parking may be shared between uses, as defined in Policy 3.20.
6. Loading access should be separated from the primary driveway access. Loading areas and trash containers should be located behind buildings, to the sides of buildings, accessed by service alleys, or screened by walls and landscaping.

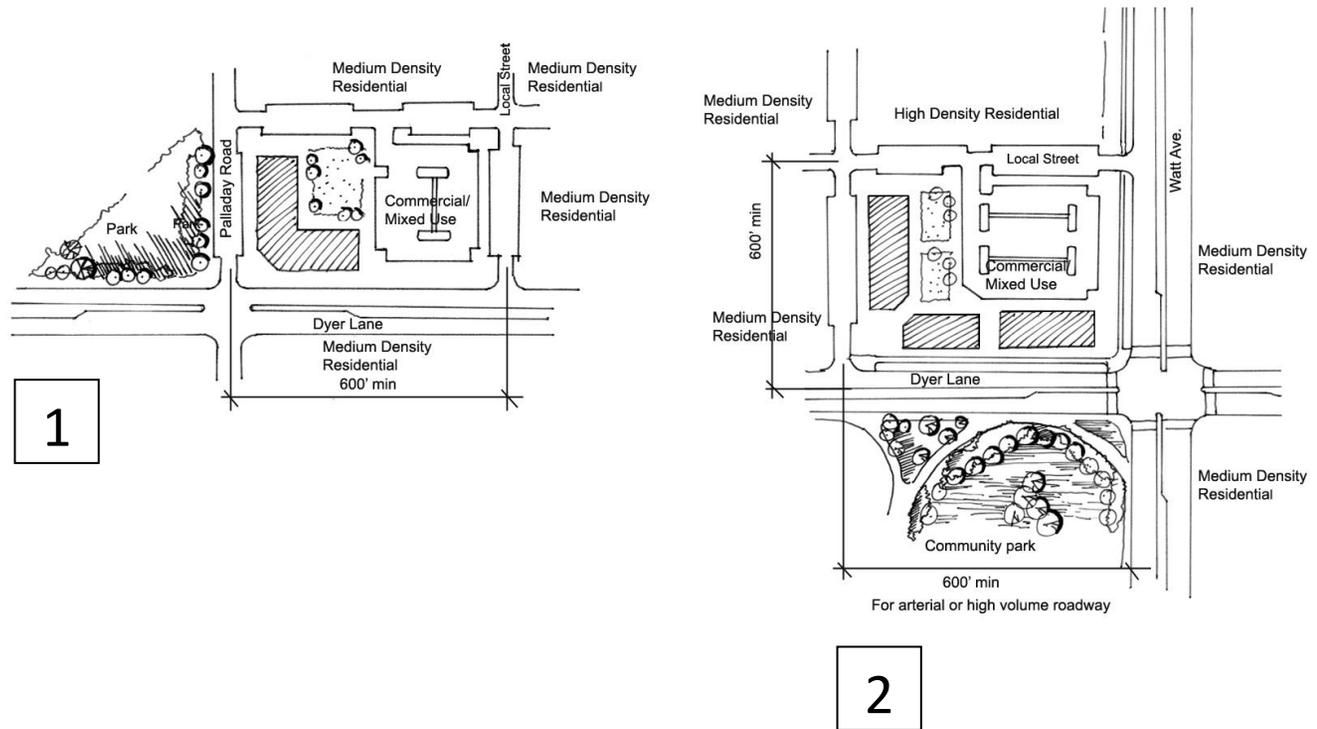
COMMUNITY DESIGN

**Figure 6.17 -- Conceptual Access Diagram into Neighborhood Commercial Sites**

The following diagrams address recommended access into commercial/mixed-use sites for two test sites in the Plan Area. Access into commercial sites should be avoided on arterial and major roadways and should instead be provided from secondary streets. The recommended minimum spacing from an intersection to a development driveway encroachment onto a major arterial or collector street is 600 feet. Minimum distances onto a local collector or low-volume road segments shall be 300 feet.



*Neighborhood Commercial Center Key Diagram*



## 6.4 NEIGHBORHOOD DESIGN

Placer Vineyards is organized as an assembly of neighborhoods, each designed with distinct site attributes, anchored with community-serving amenities, and connected through a system of roadways and greenways. The following design guidelines are intended to promote quality design and a cohesive residential environment for a wide array of single-family (detached and attached) and multi-family housing types. It also provides guidance for the siting of homes in relationship to the street, to open space, and to other neighborhood features.

- Goal 6.23** Create distinct districts and neighborhoods that help define a sense of place and character within the larger Placer Vineyards community.
- Goal 6.24** Design new development that is attractive, functional, and adds to the creation of a sense of place for the Placer Vineyards community.

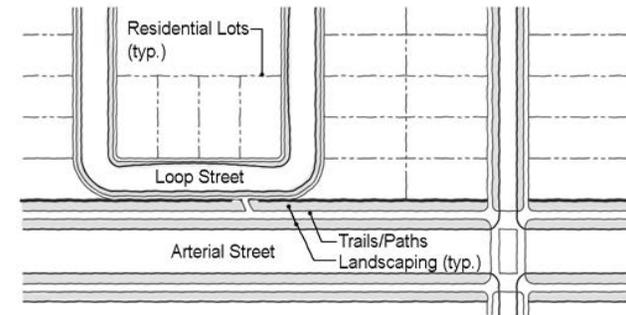
### 6.4.1 GENERAL LOT DESIGN

- Goal 6.25** Promote lot design and development standards that respond to the unique characteristics of the site and provides the basic organizing framework for each development, but allows flexibility to accommodate a wide range of land use and housing types, styles, and design solutions.
- Goal 6.26** Encourage new, creative, and imaginative site designs that provide a variety of solutions to land use types throughout Placer Vineyards.

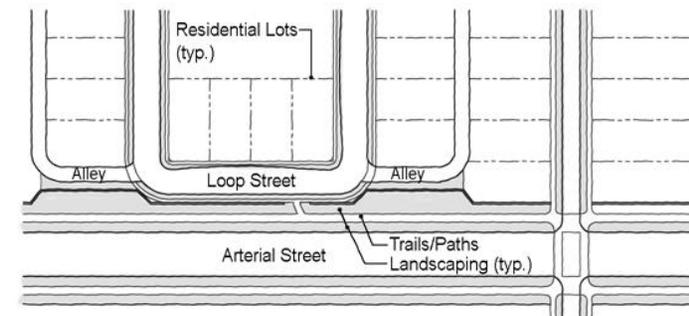
**Policy 6.40 Lot and Development Standards.**

*All development in Placer Vineyards shall comply with the intensities found in Table 3.3 in Chapter III, "Land Use," and in Appendix A, "Land Use and Development Standards."*

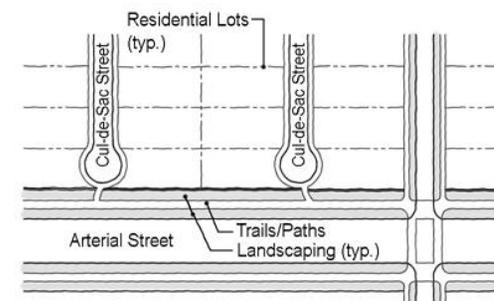
**Figure 6.18** Lotting Conditions Adjacent to Major Roadway



*Plan A: Lots Fronting on Loop Road with Alley Access*



*Plan B: Lots Fronting on Loop Road*



*Plan C: Open Ended Cul-de-Sac Options*

## COMMUNITY DESIGN

### Design Guidelines for Residential Lotting

1. Residential lotting adjacent to major roadways should minimize the need for continuous sound walls through the use of a variety of road and lot configurations. Possible alternative lotting and road configurations are provided in Figure 6.18. Also refer to policies 6.43 through 6.49.
2. Residential lots should be organized in block layouts that encourage walking, biking and use of alternative modes of transportation.
  - a. Neighborhoods should be organized into smaller recognizable subareas, where feasible, with schools and neighborhood parks as focal points.
  - b. Residential blocks and streets should be organized in a pattern that reduces regional thru-traffic.

### 6.4.2 RESIDENTIAL DESIGN

The Placer Vineyards Plan is based on the concept of interconnected residential, commercial, and open space areas. Neighborhood residential policies encourage connections to collector streets between the properties of different parcels and between phases of the same parcel. This concept of interconnections is represented in Figures 6.19, 6.20, and 6.21.

- Goal 6.27** Develop residential areas as open and linked neighborhoods that encourage alternative modes of transportation—walking, biking, and transit use—with a school or neighborhood park located within easy walking distance of the surrounding community as the focal point.
- Goal 6.28** Create an interconnected community that increases the opportunity for pedestrians to make shortcuts by providing points of access from residential neighborhoods.
- Goal 6.29** Provide a diversity of neighborhood streetscapes and architectural designs. Variety in design character helps to reduce the visual repetitiveness of neighborhoods and contributes to a sense of scale that relates to the street and de-emphasizes the automobile as the dominant visual component in the physical landscape.

### **Policy 6.41 Residential Neighborhood Site Design.**

*Residential neighborhood site design shall provide opportunities for pedestrian and bicycle connections to core areas and other neighborhoods.*

1. *Streets shall be laid out in a pattern that allows for internal connections between adjacent residential neighborhoods without having to drive to an exterior thoroughfare or arterial street.*
2. *Residential developments shall be compatible with and connect to the broader community.*
3. *Residential developments shall avoid using sound walls, when possible, or site designs that insulate or separate the development.*
4. *Residential development shall be designed to have single-loaded streets, located along parks, drainage ways and open space areas. Single loaded streets shall always be provided adjacent to the Dry Creek corridor. Frontages without single-loaded streets may also be approved under certain conditions. Potential conditions when side-on or back-on lot designs next to parks or open space may be approved are:*
  - a. *In small housing development areas where fronting streets is difficult or infeasible, and*
  - b. *Frontages along drainage corridors when frequent openings (i.e. open-ended cul-de-sacs and pedestrian ways) are provided.*

*When homes are planned to back onto parks, drainage ways, and open space areas, rear lot conditions shall not extend more than 400-feet without being broken with an open-ended cul-de-sac or pedestrian accessway.*

### **Policy 6.42 Gated Developments.**

*Gated developments are allowed but not encouraged in the Plan Area. Gated developments will only be allowed at the discretion of the County. When evaluating gated development proposals, the County will ensure that the development is well integrated into the fabric of the Specific Plan and that public pedestrian connection areas through the gated community are provided to adjacent developments and open space areas. The County will also ensure that the design and location of the gates will provide safe and sufficient circulation for emergency service providers and the ability for vehicles to turn around in front of the gate to prevent them from backing out into the adjacent roadways. Gated communities must be designed to ensure that through streets are provided, connecting adjacent neighborhoods and developments and that overall traffic circulation within Placer Vineyards is accommodated.*

Figure 6.19 - Conceptual Residential Interconnections Diagram



COMMUNITY DESIGN

Figure 6.20 - Residential Site Design Example (DISCOURAGED)



Figure 6.21 - Residential Site Design Example (ENCOURAGED)



## COMMUNITY DESIGN

### RESIDENTIAL NEIGHBORHOOD SITE DESIGN GUIDELINES

Residential neighborhood design layouts should be designed consistent with the following standards and guidelines.

1. Roadways and pedestrian access paths should link adjoining neighborhoods as an interconnected network, providing easy access to schools, parks, and open spaces. Neighborhood layout should provide multiple access points, thereby maximizing the number of streets that carry traffic into each residential area and distributing traffic loads.
2. Residential neighborhoods and associated landscape plans should be organized to create feature or place that makes the neighborhood unique or distinct. This feature may include parks, open space, and creek corridors, or school sites that are easily accessed from surrounding residences.
3. Religious sites should be located on the periphery of residential neighborhoods along community collector streets or arterial roadways.
4. Residential streets should be organized to slow traffic and create a more pedestrian-friendly and safe environment through the selective use of roundabouts, bulb-outs, median planting, cul-de-sacs, special paving, and other site or architectural features.
5. Neighborhood layouts should ensure that open spaces and creeks are visible and accessible from public areas, streets, and trails. Lot and residential building layouts should face onto open spaces wherever possible to provide visual surveillance and security to open space.



*Pedestrian access path should create connections linking residential areas to neighborhood destinations*



*Parks can be organized to create a focus for a community*



*Homes should front onto open space whenever possible to provide visual surveillance to open space areas*

6. Residential site designs should ensure outdoor activity areas shall not exceed the County noise standards. Designs should, however, minimize the need for sound walls adjacent to collector streets with Placer Vineyards by utilizing the planning techniques defined in Policy 6.48 through 6.50 and as illustrated in Figures 6.18, 6.20, 6.21, 6.24, and 6.25.
7. Pedestrian and emergency access should be provided from neighborhoods adjacent to open space and creek corridors.
  - a. Access can be provided by local, single-loaded streets parallel to open space and creek corridors.
  - b. Access may also be provided from open-ended cul-de-sacs, stub streets, loop streets or pedestrian easements between lots.
  - c. Pedestrian access points along open spaces should be no more than 400 feet apart.
  - d. Use of narrow pedestrian connections between lots should be minimized and used only where site constraints preclude access directly from abutting streets.
  - e. Where used, pedestrian access easements should provide functional, safe connections and be a minimum of 20 feet in width. Portions of pedestrian access easements may be wider while still providing visual surveillance from the abutting streets.
  - f. Housing units adjoining access easements should orient homes toward, and be designed to provide visual surveillance of, the pedestrian path from the major living areas of the units, major entries, and/or windows.

8. All neighborhood site layouts should provide a minimum of two access points on local collector streets.
9. Neighborhoods should have a mix scale of homes, including a mix of one-and two-story homes
10. Private residential streets are allowed in neighborhoods following Design/Site Review approval (see Chapter IX, “Implementation” for more information on the Design/Site Review process). Refer also to Policy 5.5 on private residential streets.

**Design Guidelines for Residential Buildings**

Residential building design should provide a mix of façades and floor plans along the same street to avoid repetition or monotony. A variety of design techniques may be used to create variety and visual interest along the street, including the following:

1. A mix of elevations, building styles, and setbacks are encouraged to provide variety in the appearance of the street. Houses of identical elevation should not face one another across the street, nor should they be located next to each other on the same street.
2. Front elevations of residential units should be designed to emphasize entries, porches, and windows into living areas and de-emphasize garages.
3. The building façades in each neighborhood should provide for a variety of styles, materials, colors, and details with some elements that create continuity between units.



*Neighborhoods should have a mix scale of homes*



*A variety of building styles is encourage to provide variety along the neighborhood street*



*The front elevation of residential homes should emphasize entries, porches, and living areas*

4. Large wall surface areas on building facades should be varied through the use of offsets, overhangs, recesses, balconies, or other architectural elements to provide visual relief and interest. Design attention should also be given to side and rear building facades visible from arterial streets, parks, or other public use areas.



*Articulate long building facades through the use of overhangs, recesses, and other architectural features*

## COMMUNITY DESIGN

### Design Guidelines for Entries and Porches

Residential building design should emphasize building entries and porches oriented to the street, providing visual surveillance of the public realm.

1. Entries to residences should be located on the front façade and articulated with special architectural elements such as a deep-set roof overhang, trellis, porch, an offset entry stoop, entry garden, courtyard, or entry portal.
2. All front porches should have a minimum depth of 6 feet (measured from the house to the face of the support columns), large enough to be functional as outdoor seating areas.

### Design Guidelines for Roofs

Residential neighborhoods and building designs should incorporate a variety of roof forms and treatments to create visual interest. Roof forms, materials, and colors are a major visual element in establishing the style, character and appearance of residential neighborhoods. Roofing materials used should reflect the style and overall character of the building and should be compatible with roofs in adjacent neighborhoods. Color should be used to bring together materials from the site or in the architecture of the building. Use of colors to differentiate between buildings or tenants within larger multi-family complexes or residential developments is encouraged.

Careful consideration and the following standards have been set to avoid the monotonous, repetitive, or massive views of roofs from off-site locations.

1. A variety of roof forms should be provided for each floor plan within a neighborhood, and should be compatible with the architectural style of each building.



*Residential building design with well-articulated front entry using entry landscaping and deep roof overhangs*



*Roof forms should be compatible with the architectural style of the building, varied, and articulated to give unique identity to the home*

2. Roof pitches should vary within each neighborhood or project area.
3. Simple roof forms that cover the majority of the main body of the house are preferred. However, roof forms should be articulated through the use of gables, hips, dormers, clerestories, offsetting ridge lines, or other architectural features to reduce the appearance of one large unarticulated building mass.
4. Where possible, roof designs should provide large eaves or overhangs to reduce the visual scale of the building, provide shadow lines and shading to windows to reduce the heat loads coming into the home.
5. All roof top or ground mounted mechanical equipment should be screened from public view.
6. Roof vents grouped and located to the rear of the ridge line, away from the public streets, parks and major pedestrian areas, or the use of ridge line vents is encouraged, to the greatest extent possible.
7. Rooftop screening should be designed as an integral part of the building style and roof type.
8. Active and passive solar-powered systems are encouraged and if provided, should be integrated into the rooftop and building architecture.

### Design Guidelines for Garages and Driveways

A variety of garage placements and driveway configurations are encouraged in residential neighborhoods to reduce the visual scale and dominance of cars and garages along the street. See Figure 6.22 for examples of garage and driveway configurations.

#### Garages

The following are guidelines for locating garages (Refer also to Appendix A, “Land Use and Development Standards,” for design standards for garages.):

1. Homes with side-loaded garages may be set forward of the front façade of the living area, provided the garage side on the street is set back consistent with the minimum front setback for that area.
2. Garages served by an alley may be attached.
3. Homes on corner lots should provide driveways from the side street when feasible, as shown in Figure 6.22. Façades of buildings with side-entry garages should be designed with windows, overhangs, arbors, entryways, or other design elements to avoid continuous blank walls on the façades of side facing garages.
4. Single-width garage doors are encouraged, especially for two-car garages.
5. The location of detached garages in the rear half of the lot are encouraged.
6. The location of detached garages in the rear half of the lot are encouraged.

#### Driveways

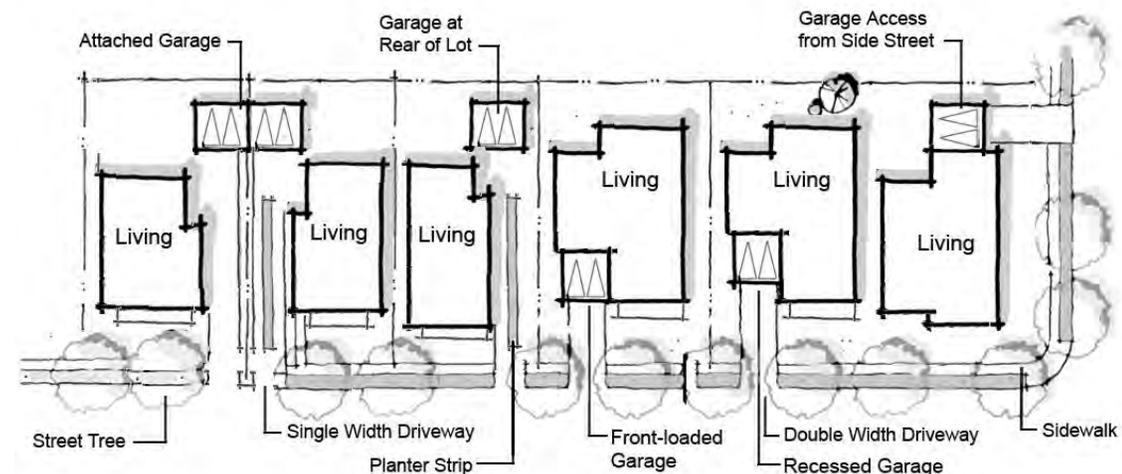
The following guidelines are recommended for the design of driveways. Driveways with parking on the driveway shall conform to the minimum aisle widths and parking stall depths for parking lots, specified in Appendix A, “Land Use and Development Standards.”

1. Direct driveway access to individual residential units from a four-lane or six-lane arterial street is prohibited.
2. Use of planter strips, landscaping, and special paving are encouraged for driveways and alleyways.



*Planter and special paving are encouraged along driveways and alleyways.*

**Figure 6.22 - Driveway and Garage Configurations**



## COMMUNITY DESIGN

### 6.4.3 WALLS, FENCES, AND SCREENING

Walls and fences throughout Placer Vineyards and on property lines provide for privacy security and sound attenuation, and can help to shape individual homes and neighborhoods. Walls and fences influence the character of neighborhoods and they can reduce connectivity by creating physical and visual barriers between neighborhoods. This section includes a variety of techniques and standards that must be used to satisfy the above goals. These require that sound walls be used only when absolutely necessary.

The policies of the Placer County General Plan encourage the use of setbacks, building orientation, noise barriers and other alternatives as noise mitigation in lieu of sound walls. In the event that sound attenuation is required, the techniques and standards specify that all available planning tools and design strategies be used to avoid the use of sound walls to meet noise level standards. Refer also to the noise standards in Chapter IV, “Environmental Resources.”

The design intent of this Specific Plan is to limit the use of sound walls along arterial and collector roads. To mitigate traffic noise and the possible negative visual impacts of continuous sound or privacy walls, a variety of design treatments and land use relationships are recommended. These design treatments include:

- ♦ Land use patterns planned to be compatible to the scale of roadways;
- ♦ The arrangement of lots and streets, including frontage or loop streets and open-ended cul-de-sacs to provide an additional setback or interrupt the continuous wall;
- ♦ Consistent wall design with interruptions to wall massing for pedestrian openings, connections, and wall offsets with optional trellises and privacy gates; and
- ♦ Landscape treatment, such as earth berms, to buffer pedestrian paths and soften or minimize the presence of the wall.

**Goal 6.30** Design communities to provide increased visual surveillance of all parks, open space, and pedestrian ways.

**Goal 6.31** Encourage open communities. Limit the use of sound walls and fences that can separate neighborhoods.

**Goal 6.32** Implement measures to reduce traffic noise on-site to acceptable levels along major thoroughfare and arterial routes (Watt Avenue, Base Line Road, Dyer Lane) and the major collector roadways with general outdoor noise levels in excess of 60 dB DNL, where such routes and roadways are adjacent to low- and medium-density residential development.

### **Policy 6.43 Attenuating Noise at Low-and Medium-Density Residential Areas Along Major Roadways.**

*The following shall establish the primary and secondary means for achieving acceptable sound levels along streets that will carry varying levels of traffic. See Policy 6.44 for a description of the means of implementing these techniques.*

1. **Thoroughfares and Arterials.** *Watt Avenue and Dyer Lane will carry the highest level of traffic within the community. Residential uses along these streets will be protected from sound levels in excess of the 60 dB DNL standard by the use of sound walls and landscape berms. Open ended cul-de-sacs and loop streets (see Figure 6.18, Plan A, B and D) shall be used to minimize the unbroken length of the sound walls. On Dyer Lane west of Palladay Road and on 16th Street, north of Dyer Lane, where traffic volumes will be lower, design features described as appropriate for collector streets shall be implemented, if approved by the County.*
2. **Collector Streets.** *Many of the collector streets within the community will carry traffic volumes likely to generate noise levels requiring strategic site planning to accommodate noise impacts. Figures 6.20 and 6.21 present examples of designs for neighborhood subdivisions. The designs in these figures are discouraged and encouraged, respectively, when considering the goal of providing residential interconnections on collector and residential streets, where the use of sound walls is discouraged. Appropriate design techniques include open-ended cul-de-sacs (Figure 6.18, Plan D), front-facing development, frontage streets, and loop streets (Figure 6.18 Plans A, B, and C). Figure 6.25 shows a typical street design plan designed in accordance with these standards that minimize the impact of sound walls.*

### **Policy 6.44 Edge Treatments for Use at Low-and Medium-Density Residential Areas.**

*The use of sound walls shall be considered only in conjunction with a minimum of one of the other practical design-related noise mitigation measures described below. Access through sound walls should be provided according to the guidelines listed below so long as it does not introduce noise levels into neighborhoods that exceed County General Plan Noise Element standards. Conceptual designs for a typical residential layout and neighborhood entry along a collector street are shown in Figures 6.23, 6.24, and 6.25.*

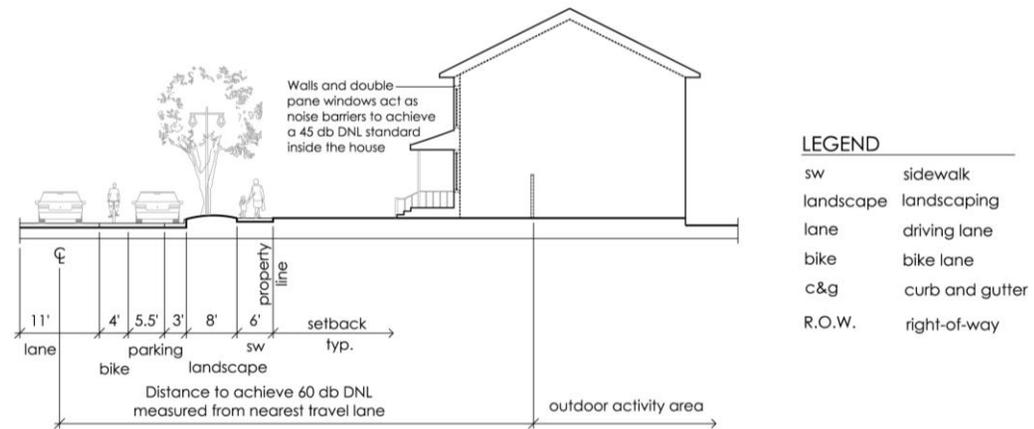
1. **Sound Attenuation on Collector Roadways.** *The preferred treatment to accommodate noise levels on collector streets shall be the use of rear loaded homes fronting onto the street and buildings that act as noise barriers. Homes shall be designed such that the home and side and rear yard fence placement ensures a maximum 60 dB DNL in the outdoor activity area.*

Unless otherwise determined by the Planning Director during the Subsequent Conformity Review process, for the purposes of this Specific Plan, the outdoor activity area is defined as a private outdoor living space enclosed by side and rear yard fences, or an enclosed courtyard, balcony, private patio, or deck. Alternatively, a fence or wall designed and maintained as a noise barrier can be used to obtain the 60 dB DNL transportation noise standard at the outdoor activity area. This would require an acoustical study and barrier maintaining entity, and shall be approved by the County as part of the Subsequent Conformity Review process described in Chapter IX, "Implementation."

The following types of housing can be designed for acceptable noise levels while fronting on these streets: townhomes, multiunit buildings that have the appearance of a single home from the street, small lot, motor court, and large lot rear-loaded single family homes. (Refer also to Appendix A, "Land Use and Development Standards," for examples of these housing types).

2. **Sound Walls.** Sound walls that may be required along Watt Avenue and high traffic sections of Dyer Lane, 16th Street, and A Street shall generally not exceed a maximum unbroken length of 400 feet. Sound walls on collector streets, shall be avoided, however, if they are required, as determined by the County, they shall not extend more than 300 feet along these streets without being broken by the use of an open-ended cul-de-sac, a section of fronting streets, or homes facing onto the street (see Figure 6.24).

Figure 6.23 - Sound Attenuation on Collector Streets



The minimum sound wall opening dimension shall be 25'. Greater width openings are preferred if it is designed in accordance with County noise standards, as demonstrated by an acoustical study.

The preferred noise attenuation treatment shall consist of relatively short lengths of sound wall, interrupted by street intersections, open-ended cul-de-sacs, use of landscape berms with lower built-in walls or fences, pedestrian access easements, and wall offsets (see discussion of these features in the points that follow). Sound walls shall be designed such that the entire length of a street will have a consistent appearance. The aesthetic design of sound walls shall be standardized along the streets. Sound wall designs shall be addressed in the Landscape Master Plan (See Policy 6.1).

For conditions where a sound wall is required, the height of sound walls shall be no more than 6 feet measured from the adjoining finished grade on the street side of the wall and no more than 8 feet from the finished grade on the residential/ commercial side of the wall. When changes in elevation occur linearly along the wall or fence, the structure shall be stepped in equal vertical increments. No step shall exceed 18 inches in height.

The preferred sound wall design shall be split face concrete masonry with pilasters. Trees, shrubs, and vines shall also be planted along the length of the sound wall.

3. **Frontage and Loop Streets.** Frontage and loop streets allow residential development to face the arterial street without the need for a wall or fence along the street. The right-of-way for the frontage or loop street may be reduced in width and the sidewalk on the opposite frontage of the residences may be eliminated. See Figure 6.18, Plans A, B, and C.
4. **Open-Ended Cul-de-Sacs.** Open-ended cul-de-sacs that end at collector streets are intended to reduce the length of sound walls facing onto the major streets and provide pedestrian and bicycle access to the roadways. See Figure 6.18, Plan D.
5. **Large Lots.** Large lots with single-family homes or multiple dwellings are typically accessed from intersecting side streets or from the rear with the primary entries facing the street. Sound or privacy walls and fences in front yards are allowed only as specifically approved by the County.
6. **Landscaped Setbacks and Buffers.** Additional setback buffer areas that are landscaped can be used between residential areas and streets. In this condition, local streets, loop streets, or frontage roads face onto a landscape buffer. Privacy walls or fences are not allowed in front yards of adjacent residential lots. The landscape buffer may incorporate earth berms, trees, shrubs, and other screening vegetation. The right-of-way of local streets adjacent to the landscaped buffer may be reduced in width and the sidewalk may be eliminated from the landscape buffer side of the street.
7. **Landscaped Berms.** Landscaped berms shall be designed not to exceed a maximum 2:1 slope.



Open ended cul-de-sacs interrupt the length of sound walls and allow pedestrian connections



Landscape berms and terraces used to reduce the appearance of the soundwall along the sidewalk



Edge treatment along a major collector roadway

**Policy 6.45 Edge Treatments at Other Areas Along Major Roadways.**

1. **Compatible Land Uses.** All parks, houses of worship, and other noise sensitive uses shall be protected from exposure to noise levels in excess of 60 dB DNL. See noise policies in Chapter IV, "Environmental Resources." Commercial, office, public and other nonresidential uses are planned along the major arterial thoroughfares, Base Line Road and Watt Avenue. These non-residential uses will not require the use of sound walls along the street. Where sound walls are required, sound walls shall be designed according to the standards found in Policy 6.44(2), "Sound Walls." A variety of landscaping, berming, or other screening techniques should be used to screen parking lots from pedestrian sidewalks.
2. **Front-Facing Development.** Buildings facing onto the street are the preferred treatment in the Town Center, high-density residential developments throughout the Plan, and along collector streets. Residential uses exposed to transportation noise in excess of 60 dB DNL will be required to design effective mitigation measures to reduce noise in outdoor activity areas to 60 dB DNL and noise in interior spaces to 45 dB DNL. Appropriate noise mitigations will give preference to proper site planning and design over the use of noise barriers or sound walls. For example, high-density projects should be designed such that active outdoor spaces are shielded from noise impacts by buildings or parking areas between the street and the building or active outdoor space. Building may also be designed with sound-rated windows and added wall insulation to act as noise barriers, capable of achieving the indoor noise requirement of 45 dB DNL.

*Sound walls shall only be considered after all other practical design-related noise mitigation measures have been integrated into the project. The applicant may be required to prepare a study demonstrating how these standards shall be met.*

***Policy 6.46 Edge Treatment at Corner Lots on Neighborhood Streets.***

*The side-yard treatment occurs in conjunction with intersecting side streets, open-ended cul-de-sacs, or loop streets. Privacy walls and fences may be used for side yard conditions along the street. These privacy walls and fences on side yards shall not overlap the front house façade. This side yard wall or fence should be terminated 3 feet behind the front façade. Buildings on corners shall provide windows and entries that orient toward the street corners. This treatment does not apply to rear loaded lot conditions (see Figure 6.24).*

***Policy 6.47 Single Loaded Streets Fronting Open Spaces and Parks.***

*This condition occurs when local streets, loop streets, or frontage roads are facing onto an open space corridor or park adjacent to the arterial roadway (see Figure 7.9). The additional open space setback provides a buffer between the residential units and the arterial street. Privacy walls or fences are not allowed in the front yards. Low fences or view fences are preferred (see Policies 6.49 and 6.50).*

***Policy 6.48 Variation in Edge Treatments.***

*Variations in the recommended edge treatments identified above will be allowed as determined by Placer County if one or more of the following conditions apply:*

1. *The treatment fails to provide adequate noise protection.*
2. *The proposed development provides an alternative treatment that meets the goal and intent of the edge treatment policies of this Specific Plan.*



*Side- and rear-yard privacy fences, including lattice fences, shall be a maximum of 6 feet high*

***Policy 6.49 Lot and Yard Privacy Fences or Walls.***

*Privacy fences or walls also occur along lot lines between individual lots and structures.*

*Generally privacy fences or walls between lots are placed on the lot line and shall not be visible from major public streets or public use areas. Privacy fences or walls shall be subject to the following design standards.*

1. *A solid “good neighbor” fence or wall provides for privacy, security, and occurs in either the side-or rear-yard conditions.*
2. *Fences (including lattice and similar attachments) and walls (measured from the finished grade of the public street side) shall be no more than 6 feet high. If located within the clear site distance triangle or within a required front yard setback, the maximum height of a wall or fence shall be 3 feet (refer to the residential setback standards in Appendix A).*
3. *Design of private fences shall be compatible, complement the building architecture, and should be consistent within each residential neighborhood or development phase.*
4. *Fences or walls shall be constructed of durable materials, and shall present a finished appearance from both properties.*
5. *For corner lots, street side fencing shall not overlap with the front façade of the building (see Policy 6.46). Fences or walls that connect two separate units and are visible from the public streets and public use areas should be of the same materials and color, and should be compatible with and complement the building architecture.*

## COMMUNITY DESIGN

6. *The visual prominence of walls and fences can be reduced through the use of landscape screening, trees, vines, shrubs, and hedge plants.*
7. *Front, side, and rear yard fences may consist of wood-picket fencing, wood-rail fencing, decorative iron fencing, or split-rail fencing in keeping with the historic rural character of Placer Vineyards.*
8. *Wall or fences along rural residential and agricultural areas adjacent to Placer Vineyards shall incorporate fencing designs characteristic of rural, agricultural fencing types to provide a transition into these areas. Use of wood-rail fences, split-rail fences, wire fencing, rock walls, or wrought iron or picket fences is preferred. Where possible, view fences should be used (see Design Guidelines for View Fences that follow).*

### **Policy 6.50 Security Fences.**

*Security fences are restricted to be used only to enclose large facilities in the Plan Area, such as the power substation and corporate yard. The use of wrought iron is encouraged. Chain link fencing with wood slats may be used for security fencing in these conditions. Use of barbed wire or razor wire at the top of security fencing is not allowed.*



*An example of a wood rail front yard fence*



*Wood rail fences are preferred as a buffer treatment adjacent to rural residential and agricultural properties*

### **Design Guidelines for View Fences**

View fences are intended to provide privacy and separation, yet allow for views into and added visual surveillance of adjoining open space, parks, and public use areas from adjoining private lots and buildings. View fences may consist of wood or steel posts with wood pickets, wood rails, or decorative wrought iron. View fences are subject to the following:

1. View fences should be located where residential uses abut open space areas and creek corridors, or adjacent to rural residential and agricultural lots.
2. View fencing is limited to a maximum of 6 feet high.
3. Chain-link fencing, barbed-wire fencing, or razor wire is prohibited on residential properties.
4. View fences may use solid materials (stone, decorative concrete, wood, etc.) for the first 4 feet in height, with the use of more opaque or see-through materials up to 6 feet high. See-through materials may include lattice, wrought iron, ornamental metal, pickets, or wire mesh.



*View fences are preferred for homes fronting on open space*

Figure 6.24 - Sound Wall on Arterial at a Typical Neighborhood Entry

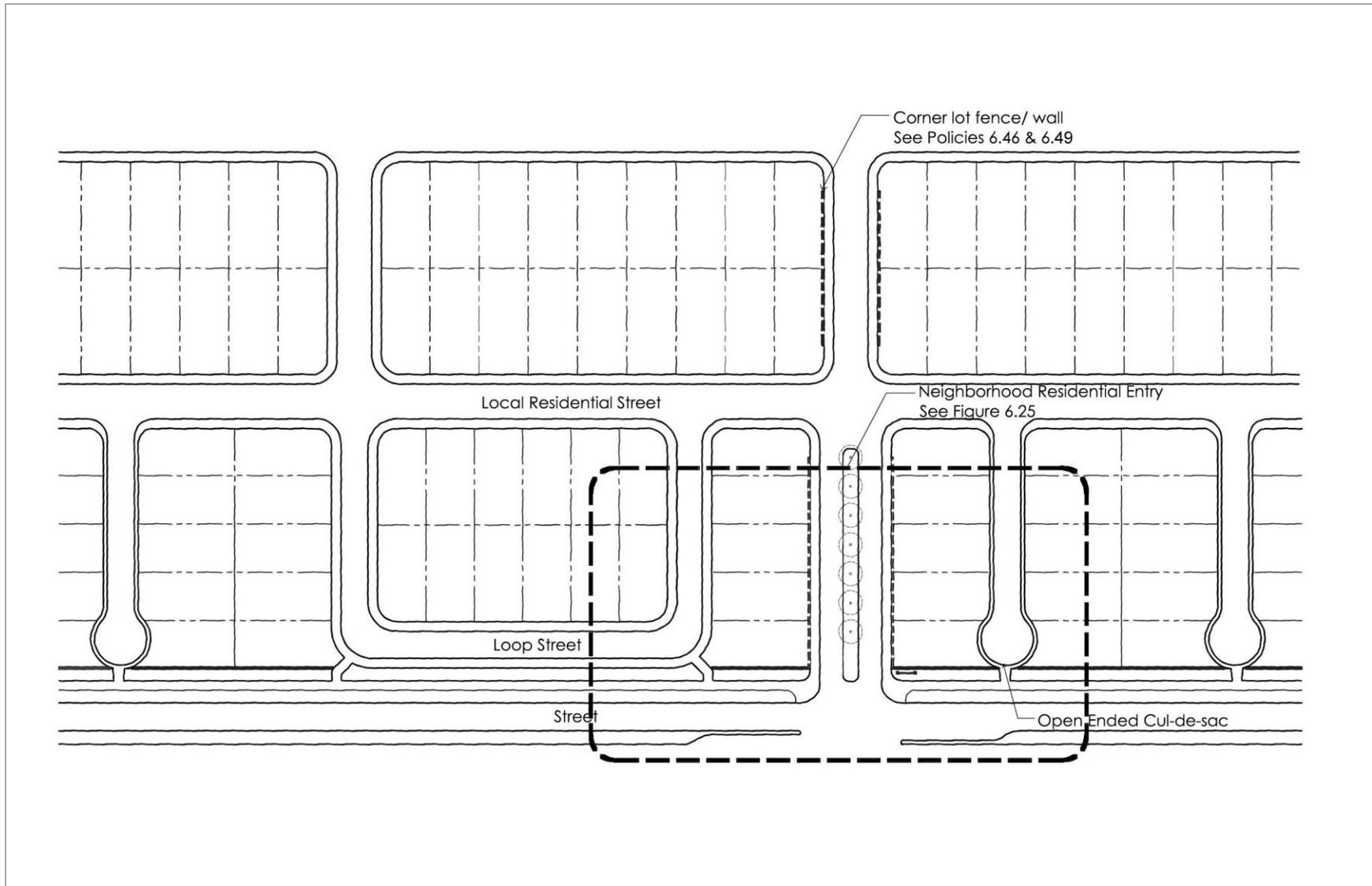
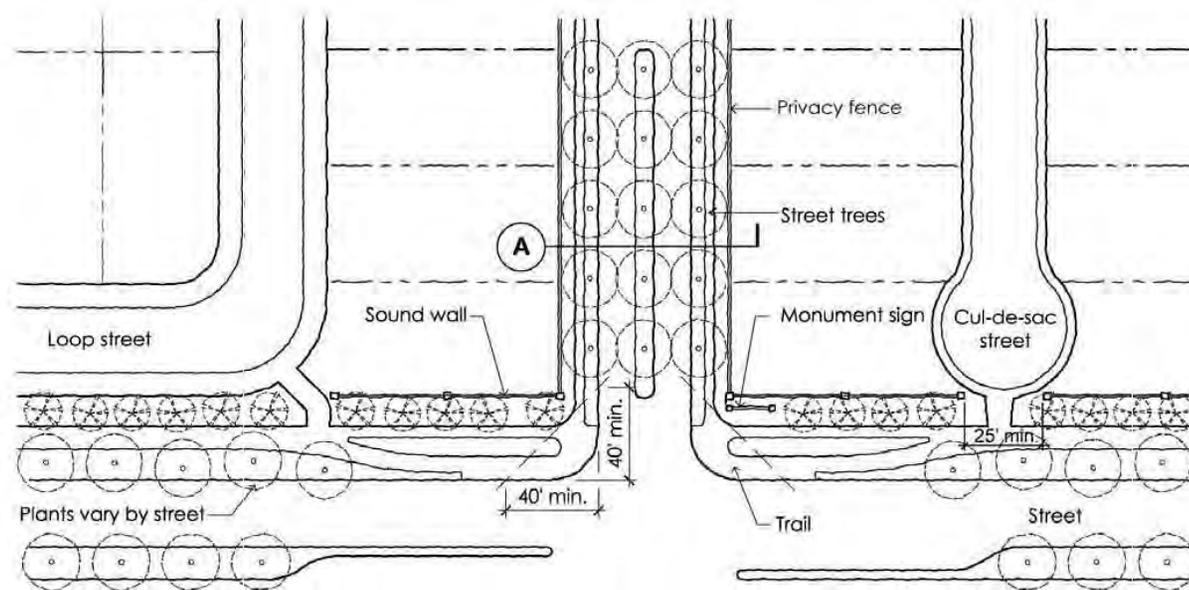
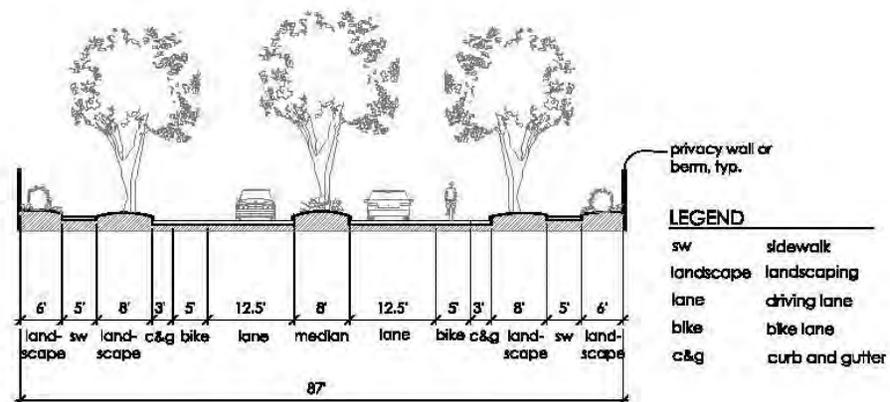


Figure 6.25 - Neighborhood Residential Entry Plan and Elevation



Plan



Section A

CHAPTER VII: PARKS AND OPEN SPACE



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## 7.1 PARKS AND OPEN SPACE CONCEPTS

### Context

This section provides an overview of the parks and open space system designed for Placer Vineyards. The intent of this Specific Plan is to ensure the timely implementation of parks and open space facilities concurrent with the development of the Plan Area.

The parks and open space system for Placer Vineyards consists of the active parks, recreation facilities, passive open space recreation areas, street landscape corridors, and open space buffer areas. This chapter should be used in consultation with the design standards for trails found in Chapter V, “Transportation and Circulation,” and with the landscape and streetscape design guidelines in Chapter VI, “Community Design.”

The *Placer County General Plan* requires new development to provide a minimum of 5 acres of improved parkland and 5 acres of passive recreation area or open space for every 1,000 new residents. Based on a projected population in the Plan Area of 31,786 people (exclusive of the SPA), approximately 159 acres of improved parkland and 159 acres of passive recreation area must be provided in the Placer Vineyards community, for a total of 318 acres. A total of 842.8 acres of parks and open space are provided in the Plan Area. Figure 7.1 shows the location of parks, open space, and landscape buffer areas for the Plan Area. Table 3-3 identifies the park and open space requirements for each property within the Plan Area.

### 7.2 PARKS

The proposed park system shown in Figure 7.1 includes 128 acres of public parks and 22 acres of private parks for a total of 150 acres. The Placer County General Plan park land dedication requirement for Placer Vineyards is 159 acres and this requirement will be met as follows:

Facility	Acres
Public Parks Credit	128.0
Private Parks (50% of 22 Acres Private Parks) Credit	11.0
Park Maintenance Facility in Corporate Yard Credit	2.0
In-Lieu Fees Credit	18.0
<b>Total Park Land Credits</b>	<b>159.0</b>

The property owners and the County have prepared a conceptual Public Facilities Concept Plan (PFCP) to illustrate how the proposed park facilities described in Table 7-1 can be accommodated on the Community and Neighborhood Park sites. The County, the Park District and the property owners will use the PFCP as a planning guide when designing the Community and Neighborhood Parks. The PFCP is a conceptual planning guide; the final design of the Community and Neighborhood Parks shall be approved by the County and/or the Park District.

See Figure 7.1 for the types, general location, and size of parks. Refer also to Table 7-1 for a summary of parkland facilities recommended by the Placer County General Plan.

**Goal 7.1** Satisfy the Placer County General Plan requirement to provide a minimum of 5 acres of active or improved parkland and 5 acres of passive recreation area or open space for every 1,000 new residents.

**Policy 7.1 Park Recreational Facilities.**

*Recreational facilities required by the General Plan are listed in Table 7-1, “Summary of Recommended Park Facilities.” Facility needs identified in the table will be met on Plan Area public or private park sites or on school sites through joint use agreements with the school district.*

**Policy 7.2 Dedication of Parks and Open Space.**

*Landowners shall offer for dedication the areas within their property planned for parks and open space, including both active and passive-use parks. The location and size of parks and open space are indicated in Figure 7.1. The final location of parks will be shown on small lot tentative maps for individual projects. The timing for the development of parks and open space for individual projects and details regarding park fees, land dedications, and on-site park development shall be described in the Public Facilities Financing Plan and defined in the Development Agreement.*

**Policy 7.3 Operation and Funding for Recreation Programs and Park Maintenance.**

*Recreation programs and the maintenance of parks, trails and open space shall be operated by the County and/or Park District<sup>1</sup> with funding provided from the Park Services Community Facilities District (CFD). The recreation program may include typical urban recreation services such as sports leagues, senior programs, youth programs, teen programs, and aquatic programs as determined by the County and/or Park District with input from future Plan Area residents.*

<sup>1</sup> The Development Agreement outlines the required steps for the formation of the Park District.

## PARKS AND OPEN SPACE

**Table 7-1 Summary of Recommended Park Facilities**

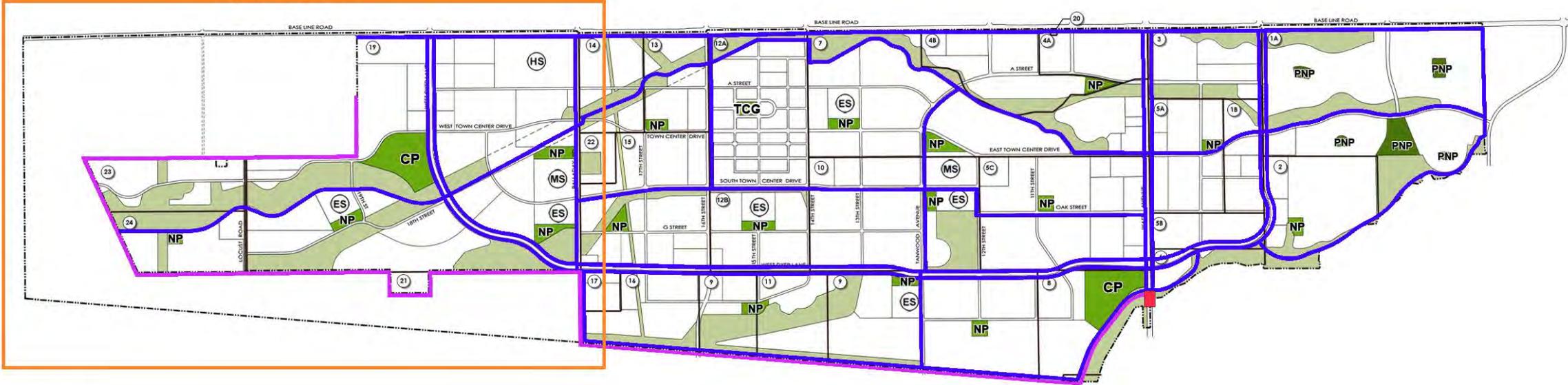
The Recommended Park Facilities table below provides a list of park facilities that the County has deemed the functional equivalent of the Placer County General Plan. The property owners and the County and/or Park District will strive to include these facilities and amenities in the designs for the community and neighborhood parks within the Placer Vineyards community.

FACILITY/AMENITY TYPE	Community Park East	Community Park West	Town Center Green	Neighborhood Parks	Joint Use Parks	Private Parks	Total for Plan Area
<b>OUTDOOR FACILITIES</b>							
Lighted Baseball Game Field w/90' Basepath (2 Joint Use @ HS and 2 @ West Community Park)		2			2		4
Lighted Adult/Older Youth Softball Field w/60-65' Basepath (2 Joint Use @ HS)					2		2
Youth Softball/Baseball Game Field (Includes 2 Lighted Fields @ West Community Park)	2	2		12			16
Lighted All-Weather Football/Soccer Game Field		1					1
Soccer/Rugby/Football/La Crosse Game Field	4						4
Multi-Purpose Practice Field	2			9	7		18
Tennis Courts (6 @ HS [4 Lighted]); 4 Unlighted @ MS & 2 Unlighted @ East Community Park)	2				10		12
Basketball - 1 Full Court @ East Community Park and Half Court @ Town Center Green	1		1	6			8
Combined Playground & Tot Lot (1 Per Park Site, Exclusive of Private Parks & Joint-Use)	1	1	1	9			12
<b>SPECIAL PURPOSE FACILITIES</b>							
Gymnasium (10,000 Sq. Ft. Joint Use with Middle School)					1		1
Multi-Purpose/Multi-Generational Community Center		1					1
Teen Center - Included within Multi-Purpose/Multi-Generational Community Center		1					1
Senior Center - Included with Multi-Purpose/Multi-Generational Community Center		1					1
Aquatics Center (Joint Use with HS)					1		1
Restroom/Concession Building	1	1					2
Restroom Building (Number to be Determined by Park District)							
Skateboard Park		1					1
Outdoor Stage/Informal Amphitheatre (Town Center Green & East Community Park)	1		1				2
Large Group Picnic Area (50 to 100 persons)	1	1					2
Small Group Picnic Area (30 to 50 persons)				8			8
Off-Street Parking (East & West Community Parks and Selected Neighborhood Parks)							
Off-Leash Dog Park	1						1
Universally Accessible Playground		1					1

Note: Each Lighted Field/Court Expands Public Use by 50-100% Versus Unlighted Fields/Courts

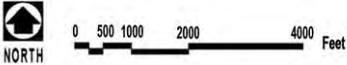
Figure 7.1 - Park and Open Space Diagram

See Figure 7.10 for Buffers Adjacent to the SPA



LEGEND

- █ **Class I Bike Paths**  
Total Distance: +35 miles
- █ **Multi-Purpose Trails**
- Existing Trails
- = Roads
- █ **Parks**  
Total Area: 150 acres
- █ **Private Park**  
Total Area: 22.0 acres (11 acres credit)
- █ **Open Space**  
Total Area: +692 acres
- ES **Elementary School**
- MS **Middle School**
- HS **High School**
- Park Legend**
- █ **CP** Community Park
- █ **NP** Neighborhood Park
- █ **PNP** Private Neighborhood Park
- █ **TCG** Town Center Green
- █ **Class 1 Bike Path connection on Watt Avenue bridge**



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**PARK CLASSIFICATIONS**

Types of parks designed for the Plan Area include community parks, a town center green and neighborhood parks, as identified in Figure 7.1, “Parks and Open Space Plan.” There are also a few private parks, located primarily within the properties set aside for an active-adult community. The park types are described in the sections that follow. Potential Facility allocations are shown in the park descriptions that follow and in the conceptual park layouts shown in Figures 7.2 through 7.5. The conceptual park designs are for illustrative purposes only; the final designs may be modified by the County and/or Park District.

**Goal 7.2** Provide a range of park types and sizes to accommodate the recreational needs of the community.

**Community Parks**

Two large community parks are located in the Plan Area. The east community park (community park #1) is located on the eastern portion of the Plan Area, adjacent to the Dry Creek Parkway. The park will incorporate parking, staging, and access to Dry Creek for cyclists, pedestrians, and equestrians (see Figure 7.2 for a conceptual site diagram of the east community park). The west community park (community park #2) is located on the western portion of the Plan Area, near the village center retail, the SPA area, and the open space corridor for the east-west power line (see Figure 7.3 for a conceptual site diagram of the west community park). Offering both active sports fields and passive recreation areas, community parks serve a range of community activities that may include youth and adult leagues, picnics, and neighborhood events.

Potential facilities and amenities for East Community Park may include:

- ◆ 2 Little League baseball/girls softball fields
- ◆ 4 Soccer fields (for under 16 age group)
- ◆ 2 Soccer fields (for under 12 age group)
- ◆ 2 Tennis courts (lighted)
- ◆ 1 Basketball court (lighted)
- ◆ 1 Large covered group picnic area
- ◆ 1 Off-leash dog park (fenced)
- ◆ 1 Restroom/concession building
- ◆ 1 Informal amphitheater/stage area
- ◆ 1 Large playground complex (swings tot lot)
- ◆ Lighting for sports activities and extended hours of play.
- ◆ Amplified sound.
- ◆ Off-street parking (number to be based on the type and location of facilities)

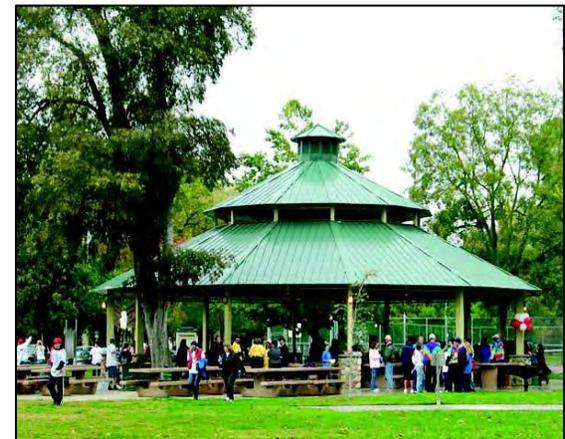
Potential facilities and amenities for West Community Park may include:

- ◆ 1 Multi-purpose/multi-generational community center (size to be determined)
- ◆ 4 Youth baseball/softball complex
  - ◆ 2 Fields with 60-90’ skinned infield, with 300’ fence outfield
  - ◆ 2 Fields with 60-65’ skinned infield with 250’ fence outfield
- ◆ 1 Restroom/concession building within ballfield complex
- ◆ 1 Covered picnic area (adjacent to ballfield and restroom/concession building)
- ◆ 1 Full size artificial turf soccer/football/lacrosse field (lighted)
- ◆ 1 Water play feature
- ◆ 1 Universally accessible playground (all ages)
- ◆ 1 Large group picnic area (covered)

- ◆ 1 Small restroom (adjacent to picnic & playground)
- ◆ 1 skate park
- ◆ Landscape buffer between active park elements and streets or parking lots
- ◆ Lighting for sports activities and extended hours of play.
- ◆ Amplified sound.
- ◆ Off-street parking lot (number to be based on the type and location of facilities)



*Example of an active sport field in a community park*



*Shade structure for a picnic area in a community park*

## PARKS AND OPEN SPACE

### **Policy 7.4 Construction of Community Parks.**

*The planning, design, construction and equipping of community park facilities is the sole responsibility of the County and/or the Park District, subject only to the participating Placer Vineyards property owners obligation to pay Development Mitigation Fees and establishment of maintenance funding through a Park Services CFD or similar funding mechanism approved by the County.*

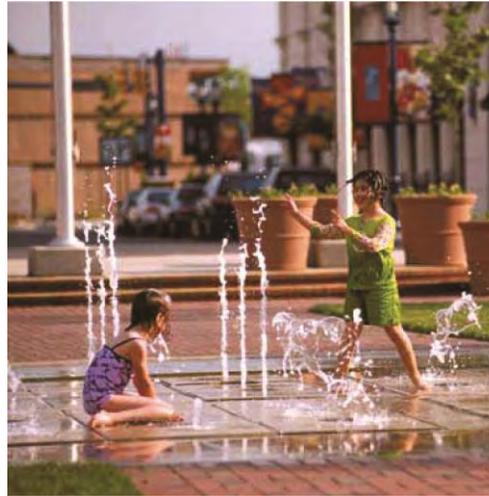
### **Town Center Green**

The town center green, at the heart of the town center, is a 3.5 acre civic community park with walkways, active and passive use areas, fountains, special lighting and landscaping, and pedestrian features designed to complement the town center themes (see Figure 7.4 for a conceptual site diagram of the town center green). It will serve as a civic/cultural focal point and gathering place for the town center. Ground-floor storefronts, restaurants, cafes, and public buildings face into the park. Activities in the parks may include evening performances, farmers' markets, public gatherings or ceremonies, and everyday informal uses such as picnics and children's playtimes.

The town center green may include the following facilities:

- ◆ Tot lot and equipment for ages 2-5
- ◆ Play lot/structure for ages 6-12
- ◆ A small outdoor community gathering space configured for performances
- ◆ A central identifying landmark feature such as a clock tower, a small sculpture, fountain, or water feature
- ◆ Lighting and pedestrian elements that complement the town center landscape themes
- ◆ Special landscape themes

- ◆ Open areas for informal recreation
- ◆ Half-court basketball court
- ◆ Lighting to support evening activities
- ◆ Amplified sound



*Fountains or water features are encouraged in the Town Center Green*



*Joint-use school/park facilities are encouraged*

### **Neighborhood Parks**

Neighborhood parks in the Plan Area range from approximately 2 to 4 acres in size (refer to Figure 7.6 – Park & Open Space Diagram for the locations of neighborhood parks). The Plan Area provides 22 neighborhood park sites: 8 to be built as potential joint-use parks with proposed school facilities, 5 to be built as private parks and 9 to be constructed as conventional neighborhood parks. Facilities and amenities will vary based on available acreage and may include:

- ◆ 10 Soccer fields (overlay full size)
- ◆ 6 Multi-purpose turf playfield/informal sports practice fields
- ◆ 12 Youth baseball/softball fields (60' base paths, full backstops and wing fences only)
- ◆ Restrooms (number dependent on planned uses)
- ◆ 9 children's playground areas (swings, older children's playground, tot lot)
- ◆ 8 Small covered group picnic area (one with minimum seating for 50 persons; the second with minimum seating for 30-35 persons)
- ◆ 6 Full size basketball courts
- ◆ Selected neighborhood parks may contain half-court basketball, game table, horseshoes, volleyball courts, BBQ areas.
- ◆ Joint use parking (amount and location to be determined)
- ◆ Street frontage parking on all sides except those that border public land; possible on-site parking

The facilities and amenities listed above, except those expressly designated for potential joint-use parks, will be distributed proportionately within the neighborhood parks.

**Policy 7.5 Neighborhood Park Design.**

*Neighborhood parks shall be located and designed according to the following specifications.*

1. Designated neighborhood parks within the Plan Area shall be developed in the general locations indicated in Figure 7.1, "Parks and Open Space Diagram."
2. There are 98 total acres of neighborhood parks designated in the Specific Plan, which includes 22 acres of private neighborhood parks and the 3.5 acre town center green.
3. Approximately 35 acres of neighborhood parks may be potential joint-use parks, shared with and located adjacent to schools. These parks shall be a minimum of 4 acres in size.
4. Neighborhood parks shall be sited and designed to maximize their visibility along streets and thereby enhance the public right-of-way and neighborhood character.
5. Neighborhood parks shall generally have street frontage on all sides, except where they abut open space, drainageways, schools or public uses. Street frontage should be on collector or residential streets, as appropriate, however no more than one frontage shall be on a collector street (see Figure 7.8).
6. Neighborhood parks shall be designed with different character or themes, landscape treatment, and uses, in order to encourage variety between residential neighborhoods.



*Parks shall be sited to maximize their visibility*



*Neighborhood parks should be designed with a variety of themes, landscaping, and uses*

7. Parking for neighborhood parks shall be provided on nearby streets, at adjacent schools, or on-site as required by the needs of the park as determined by the County and/or the Park District.
8. Potential Joint-use parks may be designed to operate independently of adjacent school facilities.

**Policy 7.6 Construction of Neighborhood Parks.**

*Property owners shall design and install park improvements for a neighborhood park site(s) planned for the property, according to the funding and timing mechanism identified in the Development Agreements, Public Facilities Financing Plan and the following provisions, which will be included in the Development Agreement.*

1. The number, size, and location requirements for neighborhood park sites shall be satisfied. In addition, when more than one park site is proposed for the property, tentative subdivision maps shall identify the appropriate phases responsible for the construction of the park sites. Property owners shall dedicate neighborhood park sites to the County and/or the Park District as provided for in the Development Agreement.
2. Each park site shall be improved at the time of development of the applicable neighborhood assigned to the development of the park site. Park facilities will be constructed and improved according to a plan prepared by the property owner and approved by the County and/or the Park District.

## PARKS AND OPEN SPACE

3. *Park facilities will be designed in accordance to the guidelines of the Specific Plan, and the standards for facility improvements provided by the County and/or the Park District.*
4. *Property owners are responsible for all costs associated with the preparation and approval of the park improvement plans as defined by the Development Agreement.*
5. *Upon satisfactory completion of neighborhood park improvements, the County and/or the Park District shall assume responsibility for park maintenance as provided for in the Development Agreement.*

### **Private Parks:**

A total of 22 acres of private parks are located in the active-adult community (property #1A), as indicated in Figure 7.1, “Parks and Open Space Plan Diagram.”

#### ***Policy 7.7 Private Parks.***

*Private parks shall qualify for up to 50 percent credit toward the park dedication requirements of the General Plan in accordance with the provisions of Section 16.08.100-I of the Placer County Zoning Code provided the following requirements are met:*

1. *The park and its facilities satisfy all other requirements of this Specific Plan.*
2. *The facilities shall be privately owned and maintained by future residents of the development.*
3. *The facilities are restricted for park and recreational uses by covenants, conditions, and restrictions.*
4. *Residents are not charged additional fees for the use of the park and its facilities.*

**General Park Design Guidelines**

The design of park site layouts should be consistent with the following guidelines:

1. Roads should be sited to provide a public focus and should be located next to collector streets, residential areas, schools, and open space. Community parks should provide site access from local collector streets.
2. A village green or small public plaza should be integrated into the site design of the town center.
3. Locating parks adjacent to open space is encouraged. Site design of residential neighborhoods should avoid large areas with lots backing onto parks.
4. Parks should be shaped and sized to accommodate park uses and should not be odd or leftover spaces.
5. Parks should be designed to engage the natural vegetation, topography, and features of the site. Parks should be linked by a system of greenways and parkways with paths separated from vehicular traffic.
6. Parks should be linked by a system of greenways and parkways with paths separated from vehicular traffic.
7. Parks should be centrally located in neighborhoods.
8. Parks should be located adjacent to streets for public access and visibility.
9. Streets that cut through or bisect parks should be avoided.



*Small plazas are encouraged to be integrated into the design of the town and village centers*



*Parks should be designed to engage the natural vegetation, topography, and features of the site.*

10. Parking for neighborhood parks should be provided on the street or shared with school lots. Parking for community parks should be adequately sized to avoid spillover parking into adjacent residential communities.
11. Refer also to Policy 6.18 for the lighting of recreational areas and athletic fields.



*Parks should be linked to a system of greenways and paths separated from vehicular traffic.*

PARKS AND OPEN SPACE

Figure 7.2 - Conceptual East Community Park Site Design

**Legend**

- A Little League Baseball/ Girls Softball
- B Soccer Fields (for under 16 age groups)
- C Soccer Fields (for under 12 age groups)
- D Tennis Courts
- E Basketball Court
- F Large Covered Group Picnic Area
- G Off-Leash Dog Park
- H Restroom/ Concession Building
- I Informal Amphitheater/ Stage Area
- J Large Playground Complex
- K Skate Park

Note: This graphic is a conceptual representation of one potential design solution for the various park facilities. Ultimate designs are subject to change and will be evaluated on an individual basis for consistency with the themes and intent of the Specific Plan and other approved documents.



Figure 7.3 - Conceptual West Community Park Site Design

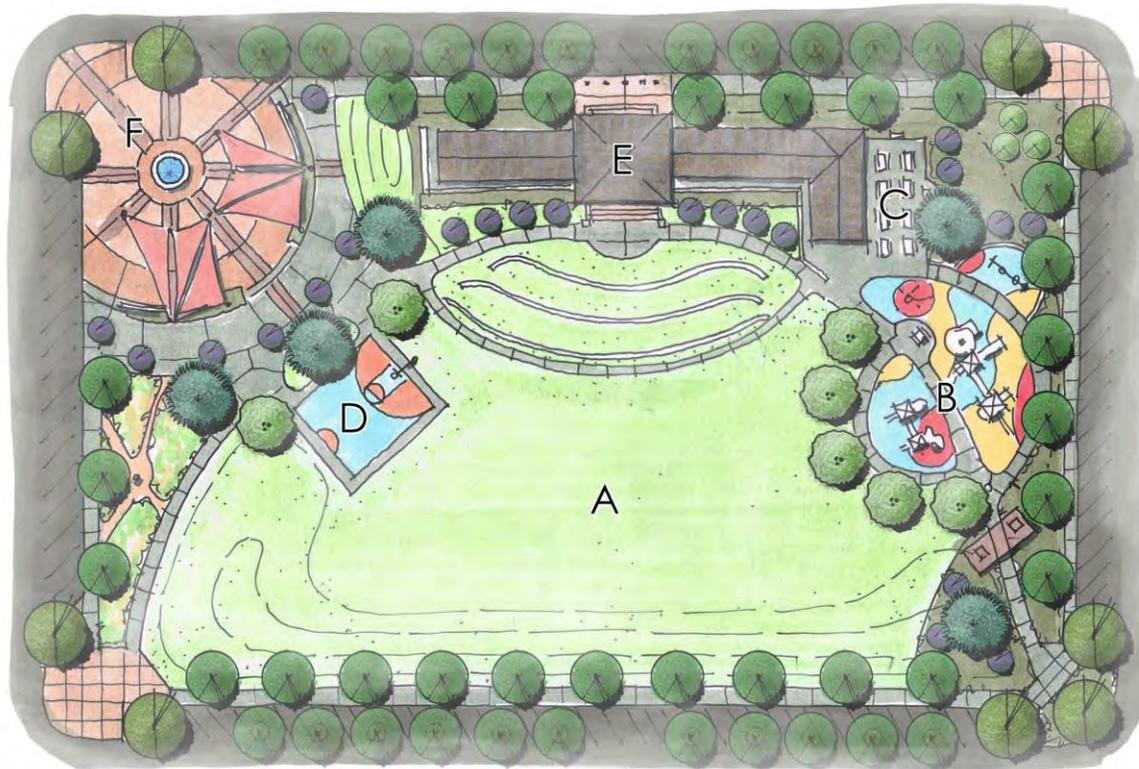


**Legend**

- A Community Center
- B Youth Baseball/ Softball Complex
- C Restroom/ Concession Building
- D Covered Picnic Area
- E Soccer/ Football/ LaCrosse Field
- F Universally Accessible Playground (all ages)
- G Large Group Covered Picnic Area
- H Restroom

Note: This graphic is a conceptual representation of one potential design solution for the various park facilities. Ultimate designs are subject to change and will be evaluated on an individual basis for consistency with the themes and intent of the Specific Plan and other approved documents. Although not depicted graphically on this exhibit, a Community Pool Facility may be constructed at this site, or at the joint use High School site.

Figure 7.4 - Conceptual Town Center Green Site Design



**Legend**

- A Multi Purpose Playfield/  
Informal Sports Practice Field
- B Children's Playground
- C Small Group Picnic
- D Half Court Basketball
- E Community Gathering Plaza
- F Sculptural Shade Structure

Note: This graphic is a conceptual representation of one potential design solution for the various park facilities. Ultimate designs are subject to change and will be evaluated on an individual basis for consistency with the themes and intent of the Specific Plan and other approved documents.

**Figure 7.5 - Conceptual Neighborhood Park Site Designs**

(For illustrative purposes only. The final park designs will be determined by the County and/or the Park District).

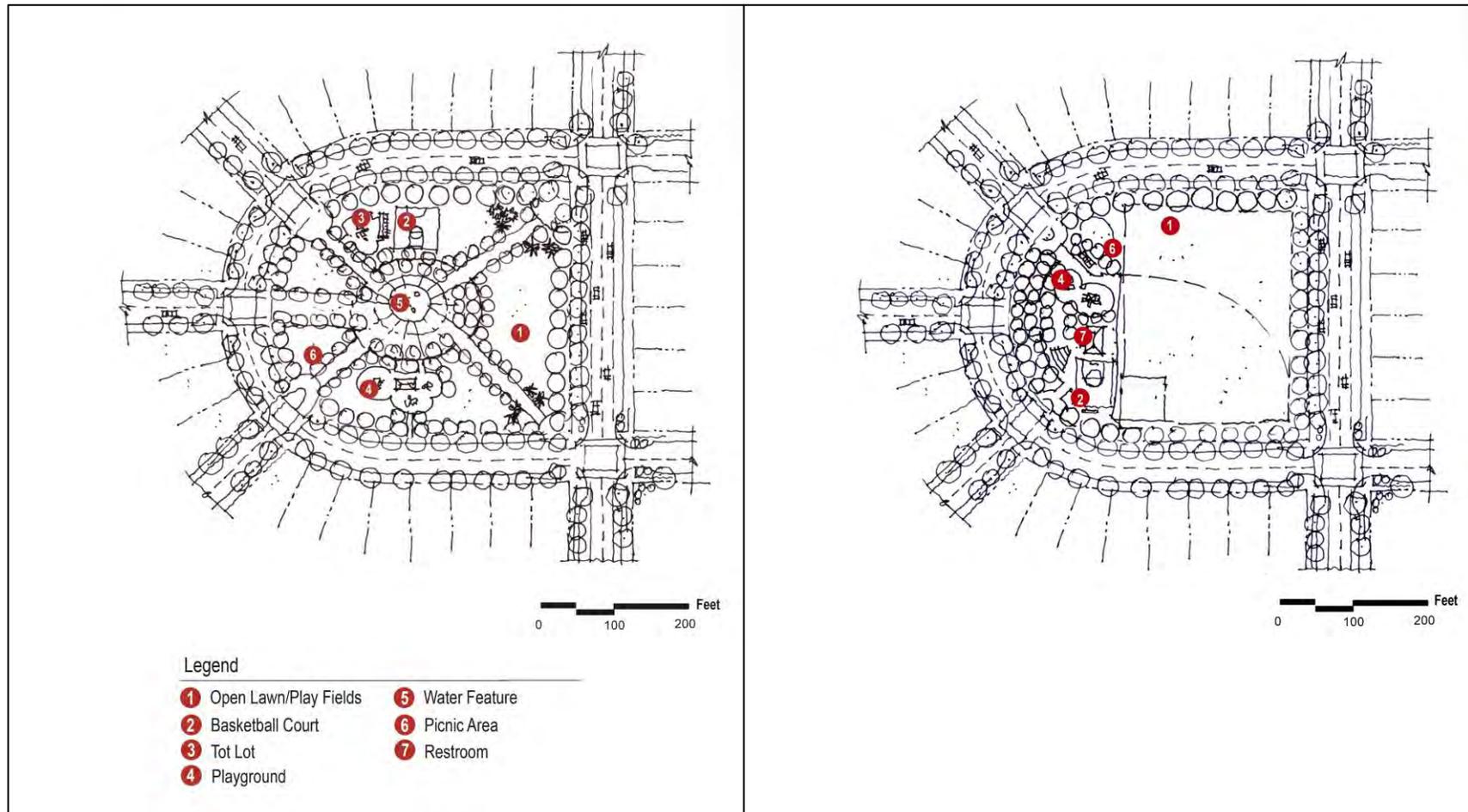
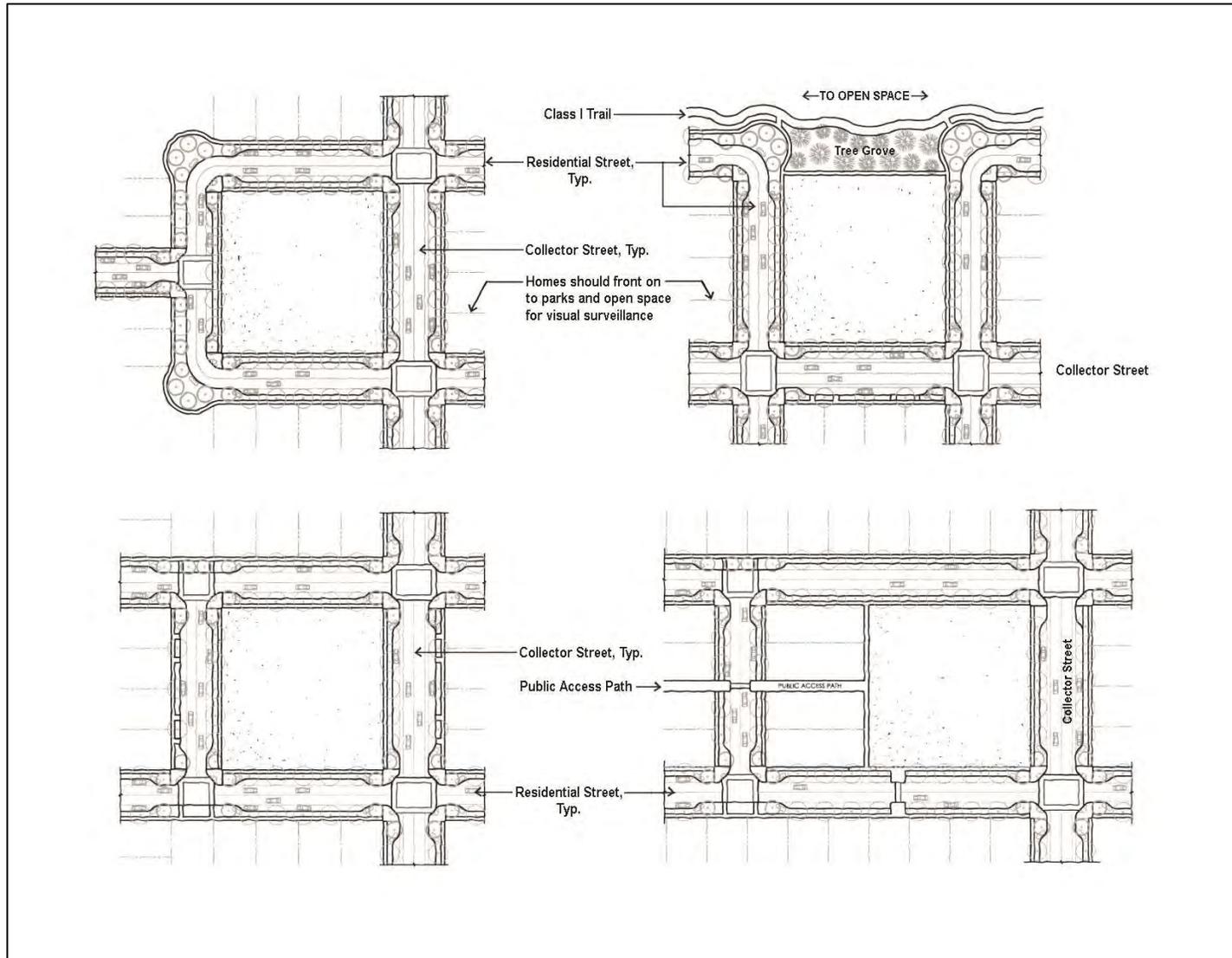


Figure 7.6 - Conceptual Roadway Designs Surrounding Parks



Parks surrounded by roads can occur only on residential streets. Only one front is allowed to side on a collector street.

### 7.3 OPEN SPACE

The open space system includes drainage ways, floodways, riparian and wildlife corridors, protected woodlands and other sensitive habitat areas, greenways (e.g., trails and landscaping), and utility and power line easements.

Open space corridors and easements in the Plan Area provide trails, stormwater conveyance, flood detention, opportunities for wetland mitigation, and buffers and transition areas between different land uses. Open space and landscape buffer areas may also have passive recreation facilities, which may include community-wide bicycle trail loop systems, interpretive signs for preserved wetlands, bird watching areas, rest stops with benches, overlooks, picnic areas, gathering areas, and gardens.

Open space buffer areas on the periphery of the Plan Area are provided as a landscape transition between the urban character of Placer Vineyards and the adjoining rural residential and agricultural uses in the county. These are landscaped setback areas tied into the open space and trail system and provide opportunities to connect to the system of trails, stormwater drainage swales, and habitat corridors. Open space buffers are also intended to screen residential neighborhoods from odors and other potential land use incompatibilities created by agricultural activities that may still be occurring in the SPA.

Passive park spaces are also provided in 2 open space areas that contain oak groves: one at the northwest corner of Dyer Lane and 12th Street; the second on the east side of the Plan Area along the Dry Creek corridor. These open space areas are intended to preserve significant clusters of existing oak trees on-site and provide recreational opportunities for the Placer Vineyards community.

A network of trails will provide public access through the open space system, connecting to the communities within and outside of the Plan Area. This community-wide trail system includes a key east-west link from the SPA along the Sacramento/Placer County line to Gibson Ranch Park, and will connect north to the Doyle Ranch subdivision along the Dry Creek corridor. The location and design standards for trails are set forth in Chapter V, “Transportation and Circulation.”

**Goal 7.3** Create an interconnected system of open space that encompasses the preservation and enhancement of natural habitat areas for the use, appreciation, and enjoyment of the community.

**Goal 7.4** Locate open space accessible to residents and link these lands to community activity areas and recreation areas.

#### ***Policy 7.8 Types of Open Space Land.***

*The Placer Vineyards Plan satisfies the County requirement for the dedication of 5 acres of passive park land per 1,000 Plan Area residents. The following types of open space will be considered passive parks and count toward meeting the passive park requirement:*

- ◆ *Floodways*
- ◆ *Site protected wildlife corridors*
- ◆ *Greenways with potential for trail development*
- ◆ *Open water (ponds, lakes, and reservoirs)*
- ◆ *Protected woodland areas*
- ◆ *Protected sensitive habitat area, provided that interpretive displays are provided (i.e., wetlands and habitat for rare, threatened, or endangered species)*

#### ***Policy 7.9 Dedication of Open Space Land.***

*The Specific Plan contains 692.8 acres of open space land, of which (approximately 21.8 acres per 1,000 residents (exclusive of the SPA area). A minimum of 5 acres per 1,000 residents of passive recreation area will be provided with the total open space area to satisfy County passive park requirements. As provided for in the Development Agreement, open space land shall be dedicated to Placer County. Upon formation of the Park District, the County shall transfer open space areas to the District subject to the County’s reservation of access easements over all drainage areas including drainageways, channels, detention or retention ponds or other such ancillary drainage facilities as more fully described in the Development Agreement. It is understood that the County retains the right, but not the obligation to maintain the drainage facilities described above (refer to Policy 7.10). Where restrictions must be placed on open space lands so as to meet environmental permitting and protection requirements (i.e., preservation, protective setbacks), such lands shall be restricted from public access.*

#### ***Policy 7.10 Maintenance of Open Space Land.***

*Maintenance of open space land and improvements therein shall be provided by the Park District, the County or other management entity with funding provided by the Park Services Community Facilities District as more fully described in the Development Agreements and the Public Facilities Financing Plan.*

#### ***Policy 7.11 Facilities in Open Space.***

*Specific design features and functions of open space shall be defined by the Specific Plan.*

## PARKS AND OPEN SPACE

### ***Policy 7.12 Facilities in Open Space Areas.***

*Recreation facilities in open space and buffer areas shall accommodate passive uses such as walking, jogging, bird watching, picnics, interpretative signage and teaching areas, rest stops, and overlooks.*

### **Open Space Buffers**

**Goal 7.5** Use landscape buffers to protect the natural environment from the built environment, to separate incompatible land uses, and to provide transitions from higher intensity urban development to more rural developments around the Placer Vineyards Plan Area.

### ***Policy 7.13 Buffer Areas Adjacent to the Special Planning Area.***

*Buffers shall be provided along the entire edge of the Special Planning Area. Figure 7.8, the “Conceptual Special Planning Area Berm and Open Space Buffers Diagram” provides the required open space buffer setbacks and lot design treatments adjacent to the SPA. Refer also to Policy 3.28, “Compatibility to Adjoining Large-Lot Rural and Agricultural Uses.”*

### ***Policy 7.14 Buffers along the County Line.***

*A 200-foot wide buffer with single loaded streets along its northern side shall be designed along the Placer/Sacramento County line from Tanwood Avenue to Palladay Road. A 50-foot wide buffer with single loaded street along its northern side shall be provided along the Placer/Sacramento County line, adjacent to Gibson Ranch Park (see Figures 7.13 and 7.14).*

### ***Policy 7.15 Oak Grove Open Space Areas.***

*Concentrations of significant oak trees on the site shall be preserved in 2 large oak grove open space areas. One will be located at the northwest corner of Dyer Lane and 12th Street and the other will be located on the east side of the Plan Area along the Dry Creek Corridor (see Figure 7.1). These open spaces shall preserve the existing stands of oak trees and serve as passive open space areas that provide a visual and educational resource to the community.*

### **Open Space Buffers Design Guidelines**

The design guidelines of open space and buffer areas should be consistent with the following:

1. To the extent feasible, trails and park amenities should be carefully sited to avoid disturbance of sensitive natural resources on-site. Sensitive preserve areas, wetland areas, or stands of oak trees may be protected using fences to discourage access and help establish plantings.
2. Within open space areas, grading, realignment, and excavation will be required for flood protection, stormwater drainage, or retention ponds.
3. Fences, 4 feet high and open in character, shall be used to protect sensitive habitat and other preservation areas or to restrict vehicular access at streets.
4. Within open space areas, landscaping will consist of drought-tolerant grasses, ground covers, California native trees, and the plants recommended for use in open space areas in Appendix B, “Recommended Plant List.”
5. Within the berm areas along the County line and the SPA, landscaping will consist of low water use plants, such as evergreen and deciduous trees, shrubs, and ground cover.
6. See Figure 7.1 for the locations of open space buffers and Figures 7.8–7.14 for more detailed plans and cross sections.

### **Open Space Character: Valley Oak Savanna**

Open space areas should complement the character of the existing site (which is predominantly Valley Oak or Savanna). Open space areas may consist of woodland canopies dominated by valley oak trees. They may also include tree species such as buckeye, western redbud, and California coffeeberry. Understory shrubs that support the woodland include upland scrub species such as California wild grape, elderberry, manzanita, toyon, and California blackberry. Ground cover and meadow areas consisting of native grasses and forbs including penstemon, monkey flower, California brome, barley, wild rye, and needlegrass can also be included.

These open space areas can integrate wildlife habitat enhancement and restoration while providing local residents with opportunities for passive recreation, gardening, ecological observation, and education. A recommended plant palette is provided in Appendix B.

## Community Gardens

Implementation of community gardens by gardening clubs, non-profit organizations, or local neighborhood associations and groups is encouraged. Community gardens provide places for local residents to grow their own vegetable gardens and learn new gardening techniques. They can help to promote healthy communities by strengthening community bonds, providing home-grown food, and promoting environmental awareness. Community gardens may be located at various open space areas and along the corridors of electrical power lines. Gardens benefit the community by increasing adjacent property values, reducing air pollution, moderating the climate, and supporting a diversity of plant and animal life. Community gardens can also be used to grow many diverse plant communities such as native plants, wildflowers, roses, vegetables, herbs and ethno-botanical (medicinal) plants. Certain gardens can be planted to attract butterflies, hummingbirds, and other wildlife.

Figure 7.7 - Open Space Buffer Design Guidelines

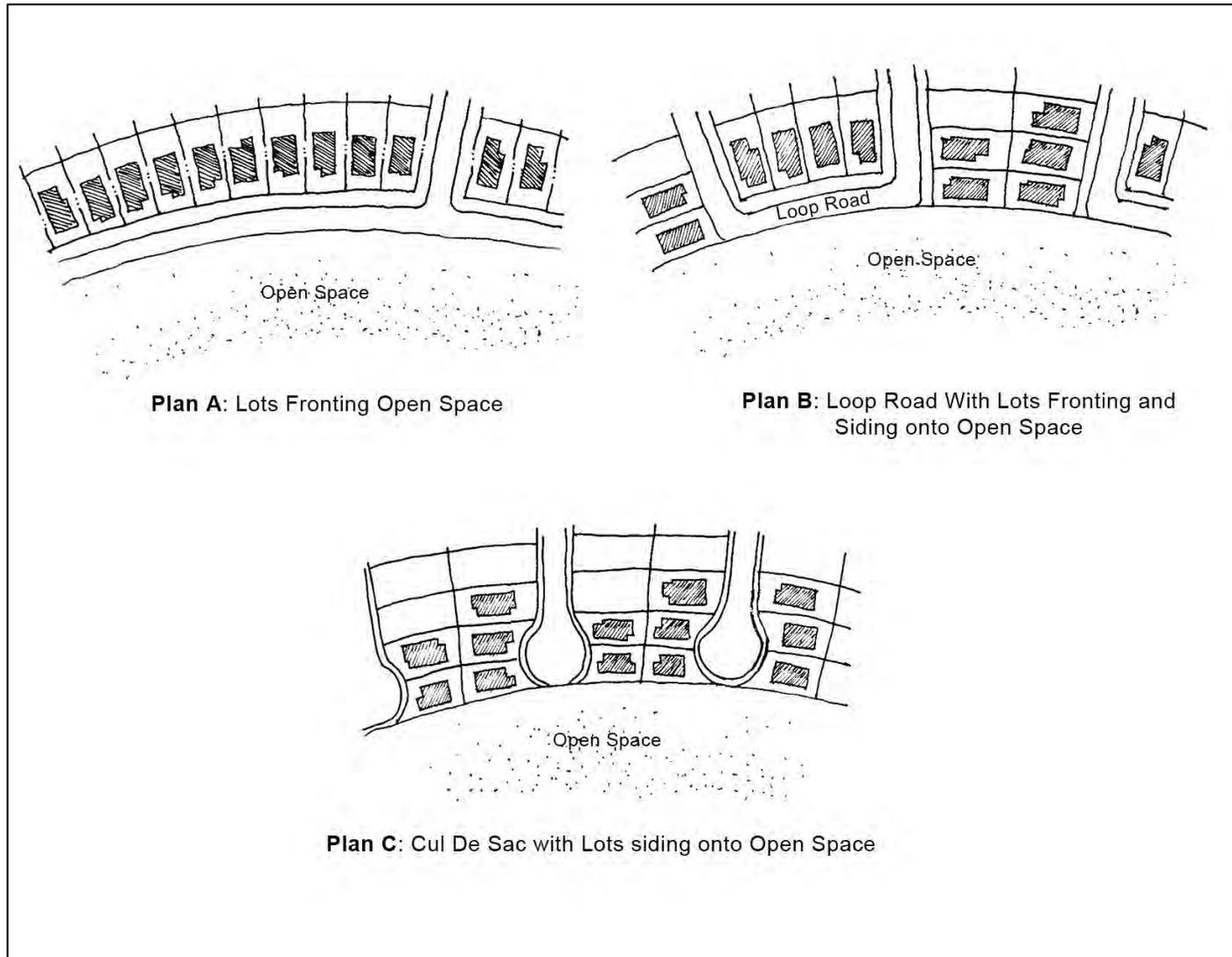
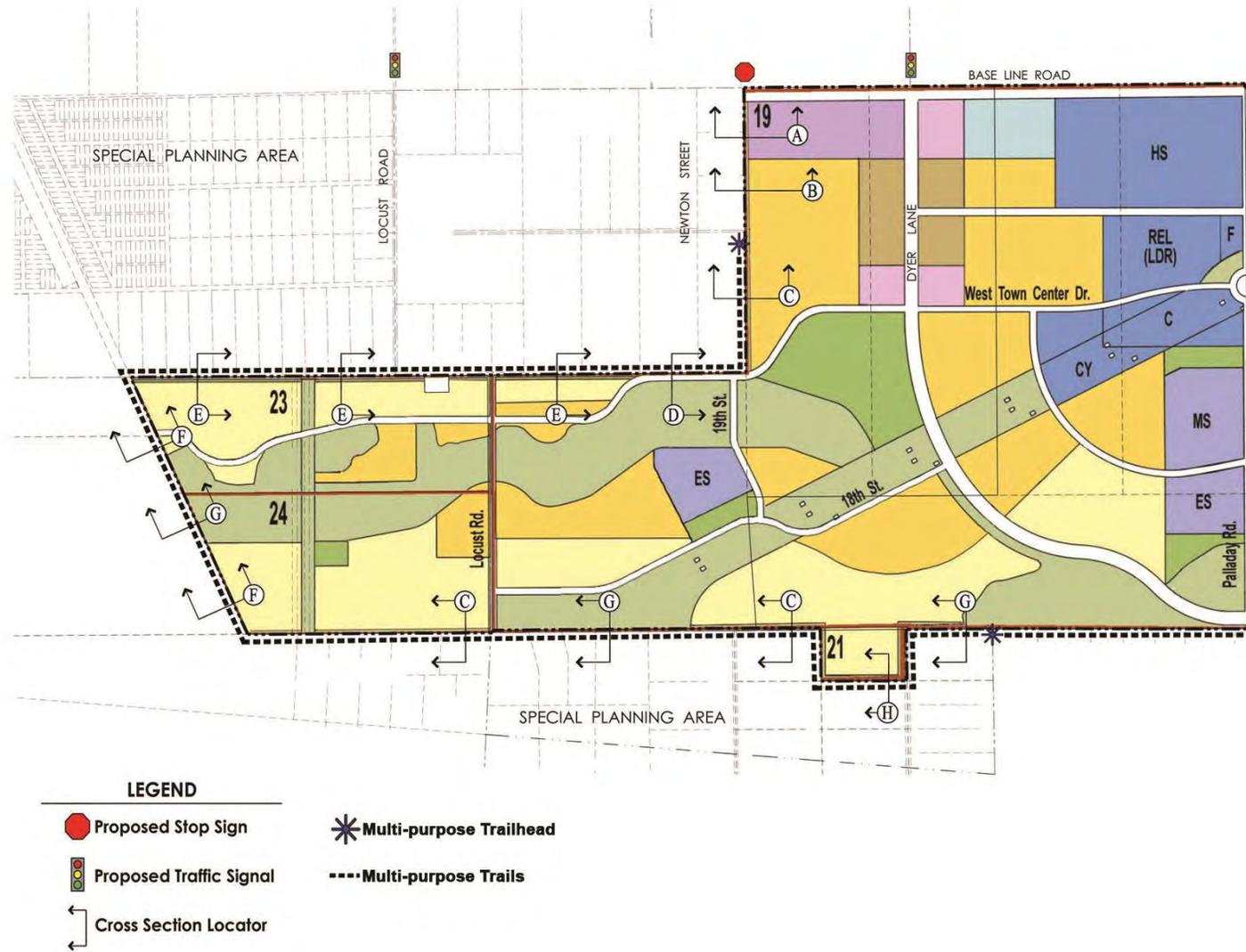
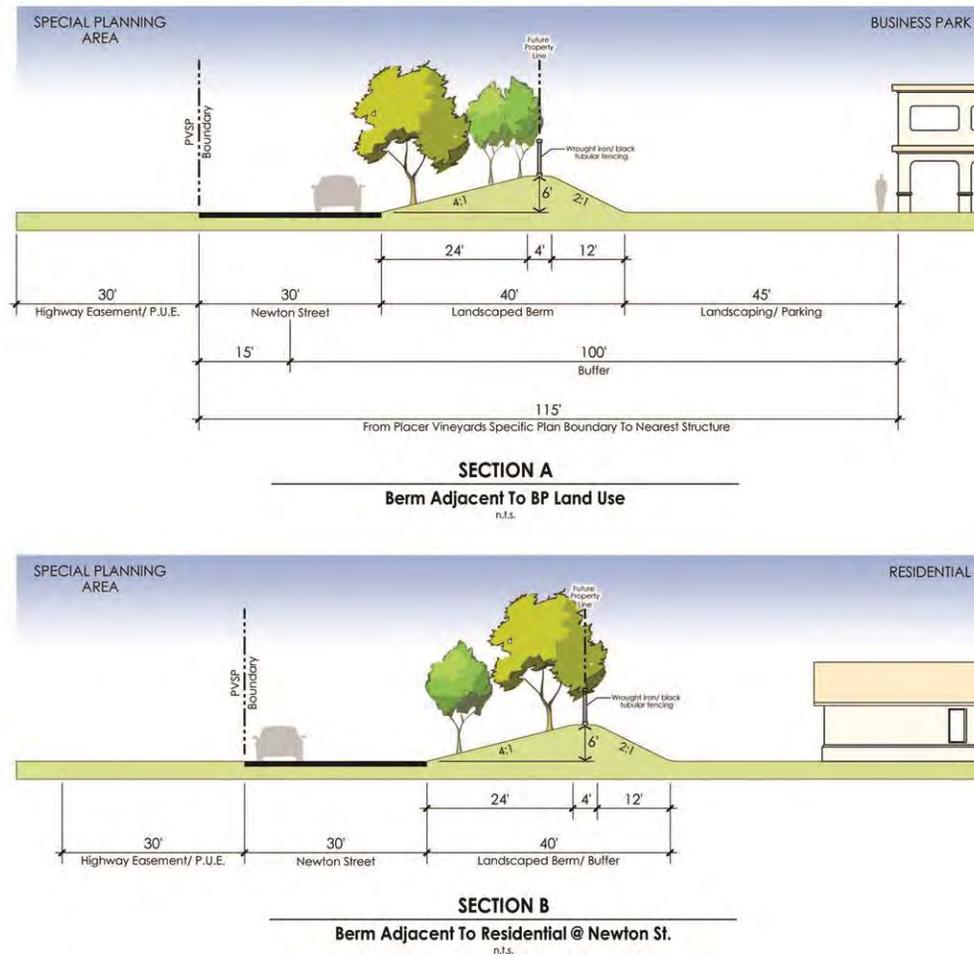


Figure 7.8 - Conceptual Special Planning Area Berm and Open Space Buffer



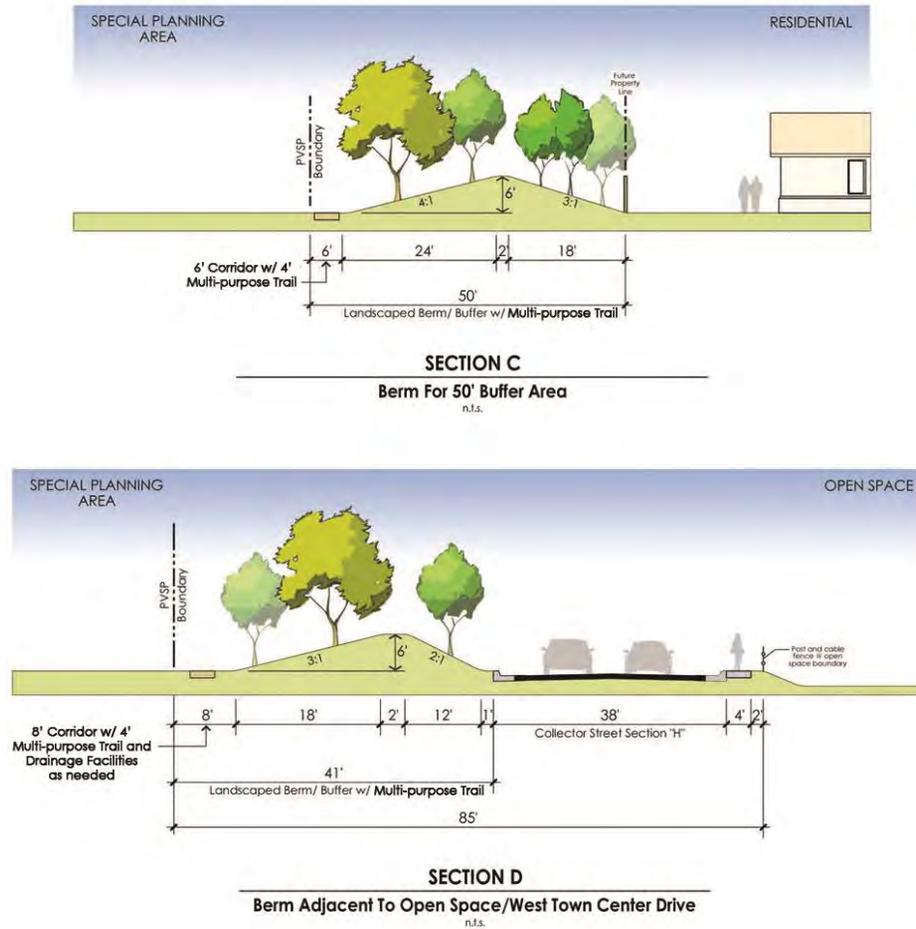
PARKS AND OPEN SPACE

Figure 7.9 - Open Space Buffer Section



Source: MacKay & Soms, 2007

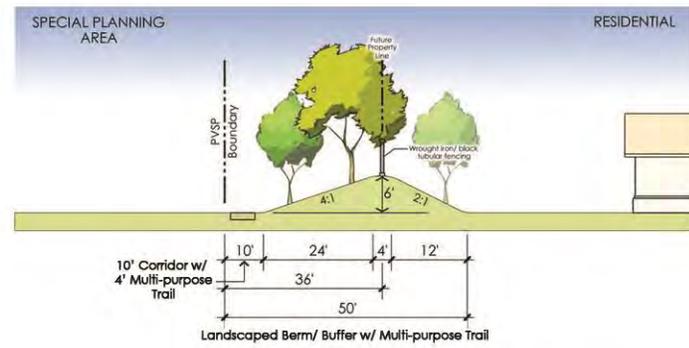
Figure 7.10 - Open Space Buffer Section



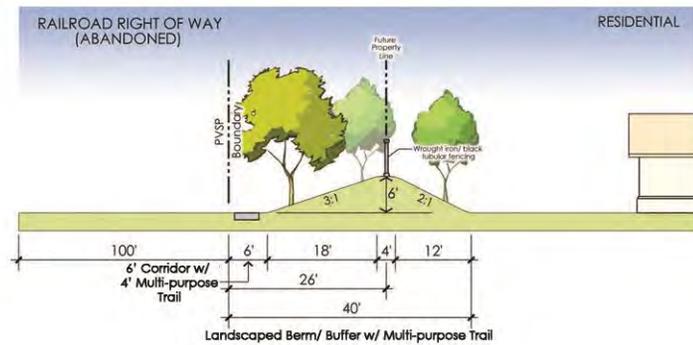
Source: MacKay & Soms, 2007

# PARKS AND OPEN SPACE

## Figure 7.11 - Open Space Buffer Section



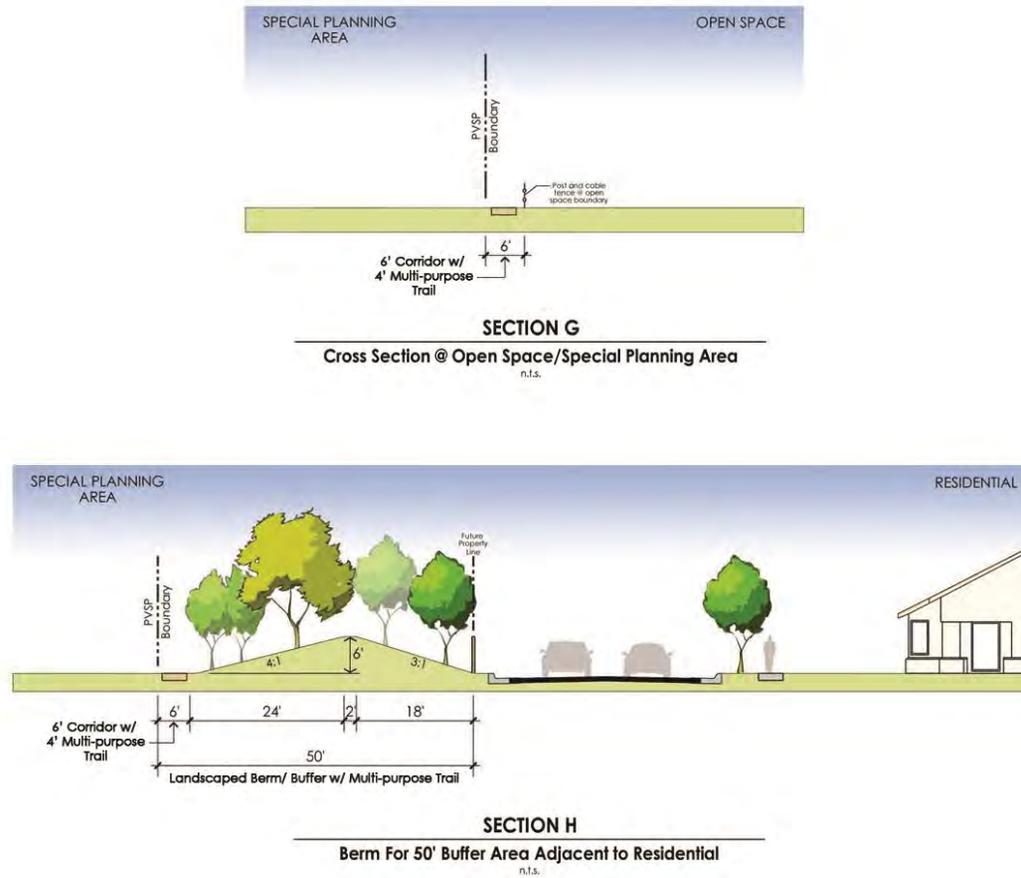
**SECTION E**  
Berm Adjacent To Northern Residential/ SPA Boundary  
n.t.s.



**SECTION F**  
Berm Adjacent To Western Project Boundary  
n.t.s.

Source: MacKay & Soms, 2007

Figure 7.12 - Open Space Buffer Section



Source: MacKay & Soms, 2007

Figure 7.13 - Open Space Buffer Section

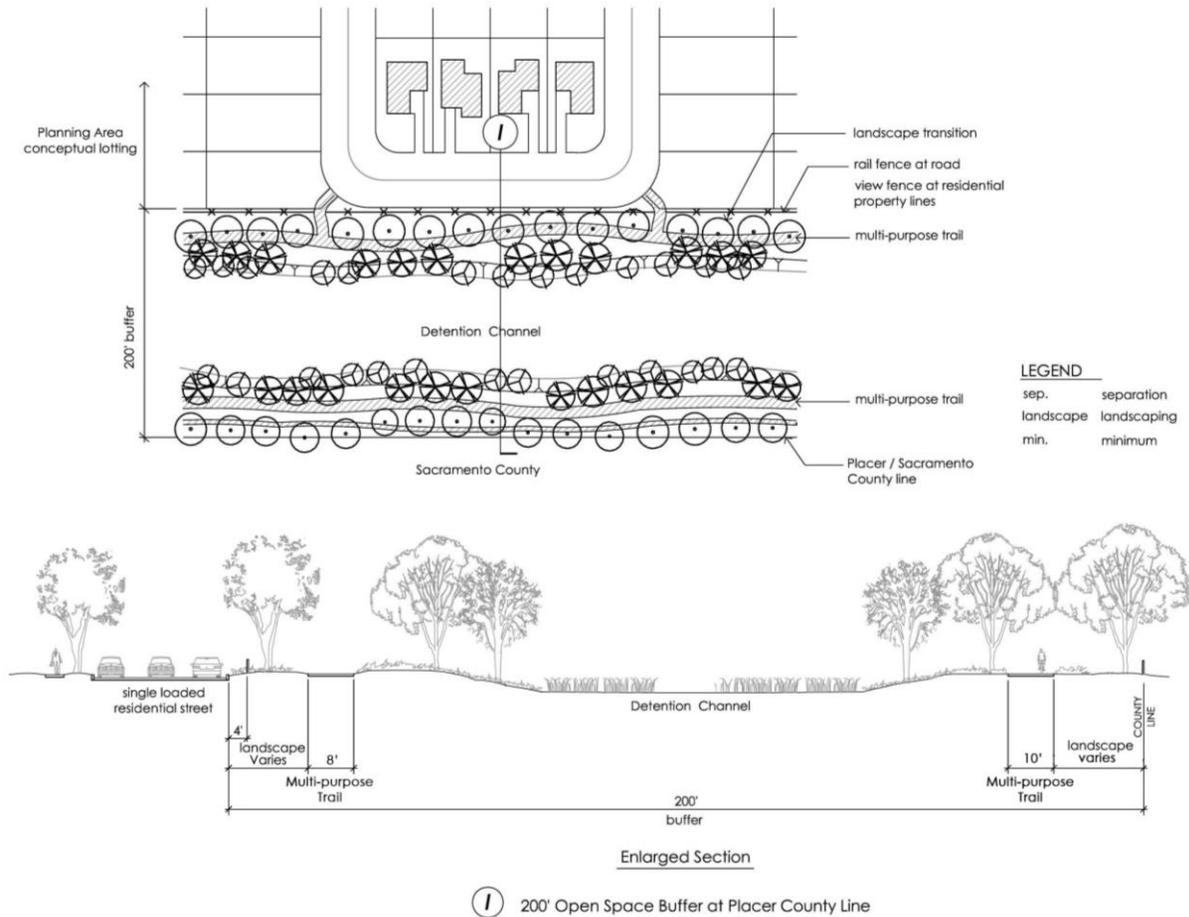
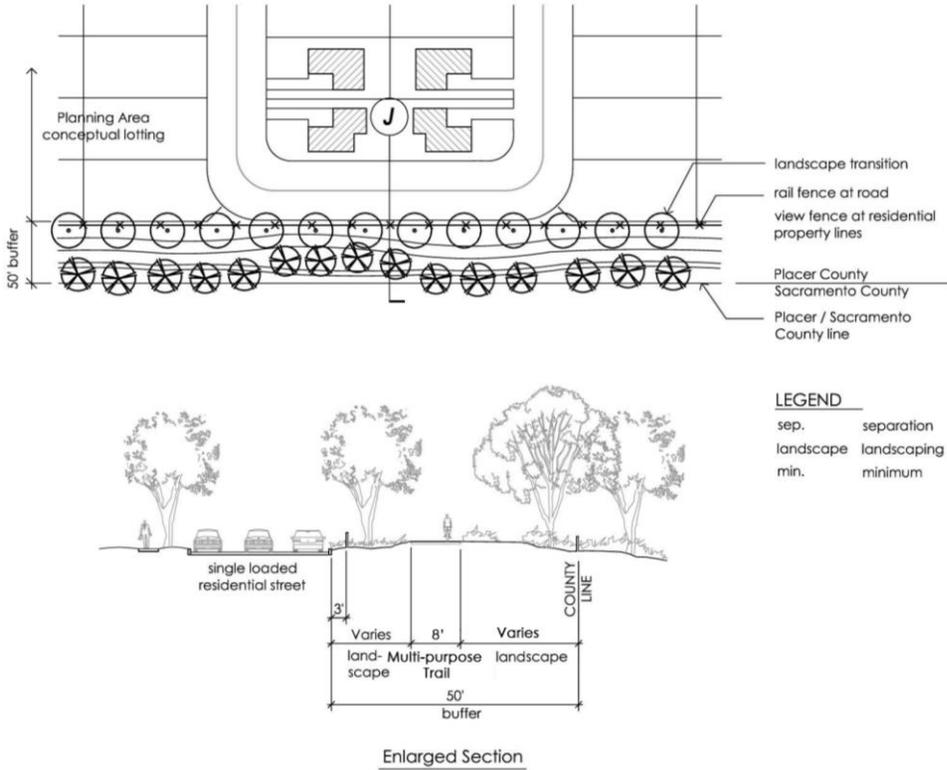


Figure 7.14 - Open Space Buffer Section



J 50' Open Space Buffer at Placer County Line

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## CHAPTER VIII: PUBLIC UTILITIES AND SERVICES



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## 8.1 PUBLIC UTILITIES AND SERVICES CONCEPTS

### Context

This section provides an overview of the existing public utilities and services and identifies the backbone infrastructure necessary to serve the build-out of the Placer Vineyards Plan Area (Plan Area). The intent of this Specific Plan is to ensure the timely implementation of public utilities and services to maintain the specified service levels for the Plan Area, as defined in the *Placer County General Plan*.

Specific utilities that will be required by the development include roadways, sewer, water, storm drainage, and solid waste disposal. Public services that will be provided in the Plan Area include schools, general County services, a library, fire protection, law enforcement, and parks and recreation. Refer to Chapter V, "Transportation and Circulation," for information on roadways, public transit systems, and trails. Parks and recreation facilities are discussed in Chapter VII, "Parks and Open Space." For timing and financing of facilities refer to Chapter IX, "Implementation." Table 8-1 provides a summary of the service providers for the Plan Area.

## 8.2 PUBLIC UTILITIES GOALS AND POLICIES

This section addresses the public utilities that will be required to serve the Plan Area. Refer also to the following technical studies and infrastructure plans prepared for Placer Vineyards for more specific details.

- ◆ Placer Vineyards Wetland Delineation (ECORP)
- ◆ Corps of Engineers 404 Permit Application (ECORP)
- ◆ Master Drainage Plan (Civil Solutions)
- ◆ Sewer Master Plan (MacKay & Soms)
- ◆ Water Master Plan (Brown and Caldwell)
- ◆ Recycled Water Master Plan (Brown and Caldwell)
- ◆ Dry Utilities Plan (Krause & Associates)

**Goal 8.1** Create a comprehensively planned infrastructure system to serve the needs of future residents and allow existing residents to tie into upgraded facilities.

**Goal 8.2** Provide public facilities in a timely manner, as required to serve new development without adversely affecting existing levels of service.

**Goal 8.3** Conserve water and energy through the use of recycled water and other water-efficient and energy-saving designs.

### **Policy 8.1 Public Facilities Implementation.**

*The following policies provide the framework for implementation of public facilities:*

1. *New development and the public facilities to serve new development shall be planned and developed according to the Specific Plan Development Agreement, master plans and local and state standards.*
2. *The Development Agreement between Placer Vineyards landowners and the County shall ensure that the project pays for its share of construction costs.*
3. *All public facilities shall be constructed and publicly dedicated as reflected in this Specific Plan and as specified in the Development Agreement.*
4. *Reasonable efforts shall be made to facilitate future connections to the system of public utilities and roads.*
5. *Utility lines shall be placed underground to the extent feasible.*
6. *Utilities shall be designed and constructed to minimize future operation and maintenance costs to users.*

### **Policy 8.2 Public Utilities and Services to the Special Planning Area.**

*Specific Plan infrastructure (water, wastewater, and drainage) shall be sized for the subsequent extension of these services into the Special Planning Area (SPA). However, property owners in the SPA will be responsible for the costs of extending infrastructure to their property, including any hook-up, Plan Area, or Special District fees. Developers of properties west of Dyer Lane that abut the eastern project boundary of the SPA shall be required to stub water and sewer mains of a size adequate to serve the SPA to their western project boundary and shall provide any easements necessary to accommodate this infrastructure. The specific number, location, and timing of such extensions shall be established at such time as subdivision tentative maps are approved for these properties.*



PUBLIC UTILITIES AND SERVICES

**Table 8-1: Plan Area Service Providers**

Service	Proposed Provider
Roadways	Placer County
Water	Wholesale: Placer County Water Agency Retail: California American Water Company or Placer County Water Agency
Drainage/Flood Control	County Services CFD and/or Park District
Recycled Water	Wholesale: City of Roseville Retail: Placer County or another public entity <sup>1</sup>
Sewer	Collection: Placer County Treatment: Roseville WWTP (South Placer Wastewater Authority) (Alternative: Sacramento Regional County Sanitation District)
Solid Waste Disposal	Recology Auburn Placer
Electrical Service	Pacific Gas and Electric Company (PG&E)/ Sacramento Municipal Utility District (SMUD)
Natural Gas	PG&E
Telephone	Consolidated Communications/ AT&T
Cable TV	Comcast and/or other provider
Sheriff's Protection	Placer County
Fire Protection	Placer County Fire District, Sacramento Metropolitan Fire District <sup>2</sup>
Schools	Center Unified, Elverta Joint Elementary, and Grant Joint Union High School Districts
Library	Placer County
General County Services	Placer County
Parks and Open Space	Placer County and/or Park District

Note:

- 1 Service responsibility may be assumed by a future multi-jurisdictional joint power authority consisting of the County and the City of Roseville.
- 2 The Sacramento Metropolitan Fire District serves a 1 square mile area in the southwest portion of the Plan Area.

### 8.2.1 WATER SUPPLY AND DISTRIBUTION FACILITIES

The Plan Area is within the service area of Placer County Water Agency (PCWA). The Plan Area will receive water service from a variety of sources during the various phases of its development.

**Goal 8.4** Encourage the use of recycled water as one source for the irrigation of site landscaping.

**Goal 8.5** Meet the Placer County General Plan requirement to assist in the supply of affordable agricultural water, including reclaimed water, to surrounding agricultural lands in South Placer County.

**Policy 8.3 *Agricultural Water Supply.***

*Development within the Plan Area should assist in the provision of agricultural water to surrounding agricultural lands. Sources of such agricultural water include reclaimed and retained water and newly developed surface water sources. Placer Vineyards shall pay agricultural water development fees to the Placer County for use in improvement projects that will increase the storage and supply of recycled water for agricultural customers in southwest Placer County.*

#### Existing Water Supply and Distribution

No public water supply or distribution system currently exists in the Plan Area. Private wells pump groundwater for various agricultural and domestic uses. In the short term, groundwater will continue to supply water for farm operations.

#### Water Supply Planning

PCWA has determined that it has sufficient water rights to meet the projected demands of projects likely to develop in western Placer County through 2030, including the Plan area. PCWA prepared a water supply assessment for the proposed Placer Vineyards Specific Plan as required by both Senate Bill (SB) 221 and SB 610. PCWA concluded that its remaining surface water entitlements are adequate to meet the build-out of the General Plans of the cities and unincorporated area in western Placer County, including the proposed Placer Vineyards Specific Plan.

#### Initial Water Supply and Distribution

Water supplies for the initial development phases will be provided from PCWA's unused American River Middle Fork project water to be diverted at PCWA's new American River pump station, conveyed to, and treated at the Foothill Water Treatment Plant. Treated water will be delivered through PCWA's existing transmission pipeline system to the City of Roseville's water system under a

cooperative agreement between the City of Roseville and PCWA. As shown in Figure 8.1, the initial water supply system will extend from the northeast corner of the Plan Area along Base Line Road and connect to an existing City of Roseville pipeline on Fiddymont Road.

Alternatives to the initial water supply could be made available by PCWA if the Sacramento River diversion has not begun deliveries at the time that additional water supplies are needed. One alternative to the initial water supply is the same source described above, PCWA's unused American River Middle Fork project water, diverted at the American River pump station and treated at the Ophir water treatment plant. A second alternative to the initial water supply is to construct a new pipeline from the project to the San Juan/Sacramento Cooperative pipeline, which currently terminates near the Antelope Road-Walerga Road intersection. This alternative water supply option would also require improvements to the San Juan Water District's Folsom Lake diversion and treatment facility.

#### Long-term Water Supply and Distribution

The source of long-term water is planned to come from a new water diversion at the Sacramento River. (see Figure 8.1). Water will be supplied via a pipeline constructed along Base Line Road, running south on Pleasant Grove Road, west along Elverta Road, and connecting to the Sacramento River. Construction of this joint pipeline will be undertaken by PCWA.

Only if the Sacramento River diversion project described above becomes infeasible, the alternative long term water supply from the Ophir water treatment plant (the same water entitlement described above in the alternative for the initial water supply) will be pursued. If the water entitlement is proposed to be taken from the American River, an enlargement of the American River pump station would be required and a parallel water pipeline would need to be constructed along the same course as the initial water supply, connecting the American River pump station to PCWA's existing water distribution system.

Refer to Figure 8.3 for the proposed route of the alternative water pipeline to serve the Plan Area.

#### On-Site Water Supply and Distribution

A backbone water transmission main located on Base Line Road will provide water to the entire Specific Plan Area. A water distribution grid consisting of mains located alongside the arterial and collector road system will connect to the main on Base Line Road (see Figure 8.1). Five water storage tanks are also distributed throughout the Plan Area to provide approximately 15 million gallons of storage. The supply and distribution system and proposed tank locations are shown in Figure 8.2.

## PUBLIC UTILITIES AND SERVICES

### Recycled Water Supply and Distribution

Recycled water from treated wastewater is generated at the Dry Creek Wastewater Treatment Plant (DCWWTP) and the newly constructed Pleasant Grove Wastewater Treatment Plant (PGWWTP).

#### Initial Recycled Water Supply

The Plan Area will, to the extent feasible, use recycled water to irrigate its parks, schools, and publicly landscaped areas (including roadway corridors and medians). Initially a connection will be constructed to an existing recycled water line south of Dry Creek near Walerga Road. The line will be extended to the west along the south side of Dry Creek and then north at a point east of Watt Avenue, crossing beneath Dry Creek to the Plan Area (see Figure 8.4). Booster pump and water storage tank locations within the Plan Area will be established during final design of the recycled water system.

#### Long-Term Recycled Water Supply

There are plans to construct a recycled water line from the PGWWTP, west along Phillip Road and south along Watt Avenue to serve the West Placer area, including the Plan Area as it builds out. The PGWWTP water supply would supplement and/or replace the DCWWTP service. Placer County plans to provide recycled water to the Plan Area from water obtained from the treatment plants owned and operated by the City of Roseville's wastewater treatment plant. Service responsibility may be assumed by a future multi-jurisdictional joint power authority consisting of the County and the City of Roseville.

#### Agricultural Water Supply

South Placer County agricultural customers currently obtain water from groundwater, PCWA, and the Nevada Irrigation District. Recycled water is available from the City of Lincoln Water Treatment and Reclamation Facility.

### 8.2.2 WASTEWATER COLLECTION AND TREATMENT

The Plan Area consists of two wastewater service areas. The approximately 890 acres generally east of Watt Avenue (indicated as "Shed B" on Figure 8.5) are within the service area of the DCWWTP, which is operated under a joint powers authority agreement between Placer County, the City of Roseville, and the South Placer Municipal Utility District. The larger remaining portion of the Plan Area generally west of Watt Avenue ("Shed A" on Figure 8.4) is not within the DCWWTP's service area. Two options available to serve the Plan Area are described below. Refer also to the Placer Vineyards Specific Plan Sewer Master Plan (MacKay & Soms, Board of Supervisors approved June 3, 2014) for more detailed information.

**Goal 8.6** Provide wastewater treatment and disposal in a timely manner, to serve new development.

#### Existing Wastewater Treatment and Disposal

Wastewater treatment and disposal in the Plan Area is currently provided through individual on-site wastewater disposal systems. These on-site disposal systems are either septic tanks with leaching trench systems or seepage pit systems.

#### Planned Wastewater Collection and treatment

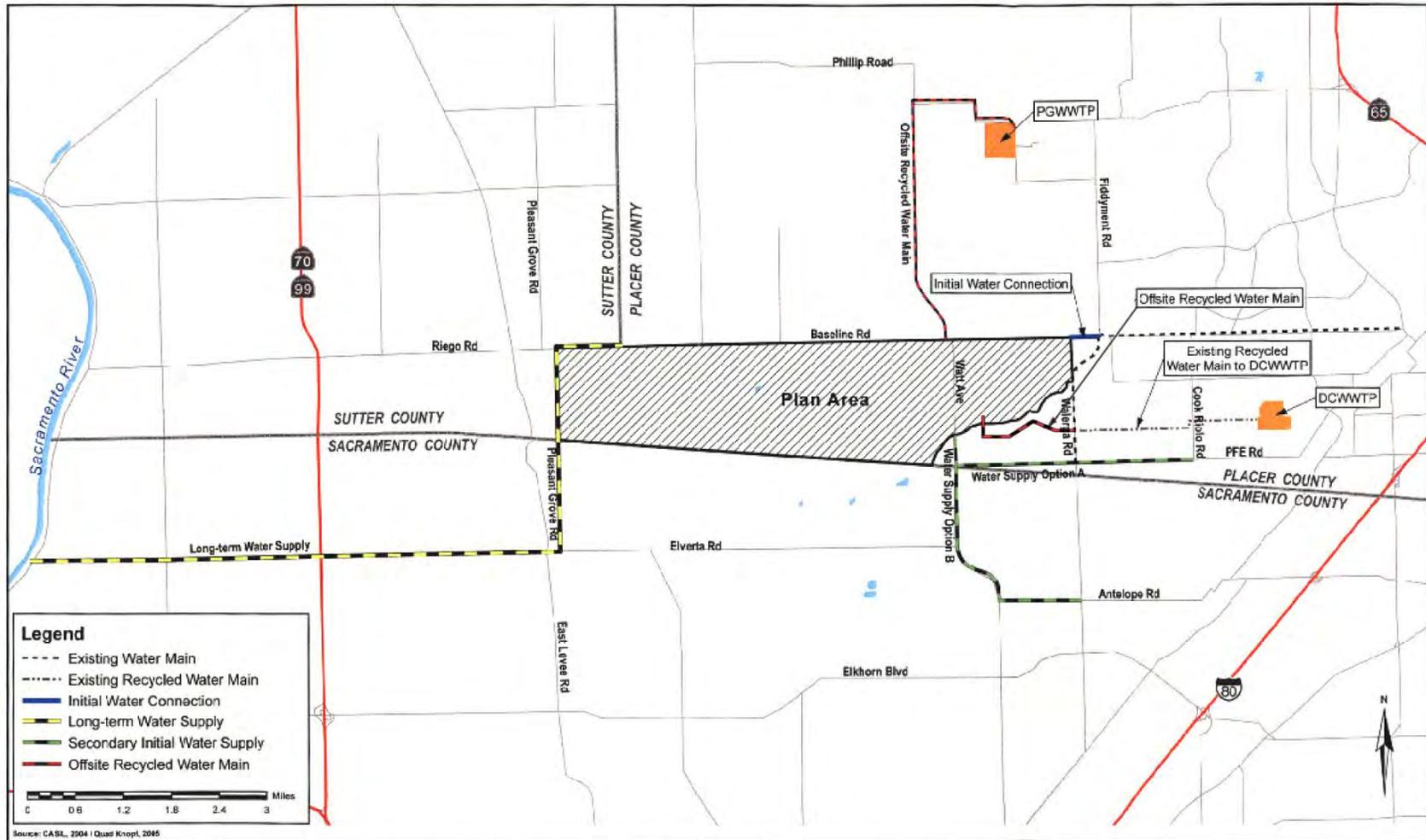
The preferred option for wastewater service is to treat flows from the entire Plan Area at the DCWWTP. Most of the approximately 890 acres in the eastern portion of the Plan Area, which are already within the current service area of the DCWWTP, would be served by a collection system discharging to a sewage lift station to be located south of Dyer Lane and east of Watt Avenue. From the lift station, wastewater flows would be carried in a force main along the south side of Dry Creek to an existing force main, located east of Walerga Road (see Figures 8.5 and 8.6).

Wastewater from the approximately 4,340 acres in the western portion of the Plan Area would be directed to the DCWWTP by way of force main pipelines in the same utility corridor. This corridor would extend from the Plan Area, south along Watt Avenue, east along PFE road, and north to the plant by way of one of two proposed alignments. The preferred alignment would proceed northerly to the plant on the easterly segment of Hilltop Circle through the Roseville Corporation Yard. An alternative alignment would leave PFE road at Cook Riolo Road, turning easterly to the DCWWTP, just north of Dry Creek (see Figures 8.5 and 8.6).

#### Alternative Wastewater Collection and Treatment

An option for the collection and treatment of wastewater from the 4,340 acre western area would be to send wastewater to the Sacramento Regional County Sanitation District (SRCSD) (see Figures 8.5 and 8.7). Under this option, sewage would flow via gravity, following the alignment of Sorrento Road, to the SRCSD Upper Northwest Interceptor at a point on Elkhorn Boulevard (Alternative A), or south from the Plan Area, following the alignment of Elwyn Avenue, west along Elverta Road, south along the alignment of West 6th Street, and connecting to the SRCSD Upper Northwest Interceptor at a point on Elkhorn Boulevard (Alternative B). Wastewater treatment would occur at the Sacramento Regional Wastewater Treatment Plant. If this option were to be pursued, a facility for the storage of peak wet weather flow for later discharge at off-peak hours would be required by the SRCSD. A 1.8 million gallon underground wastewater storage facility is proposed at the current pump station site.

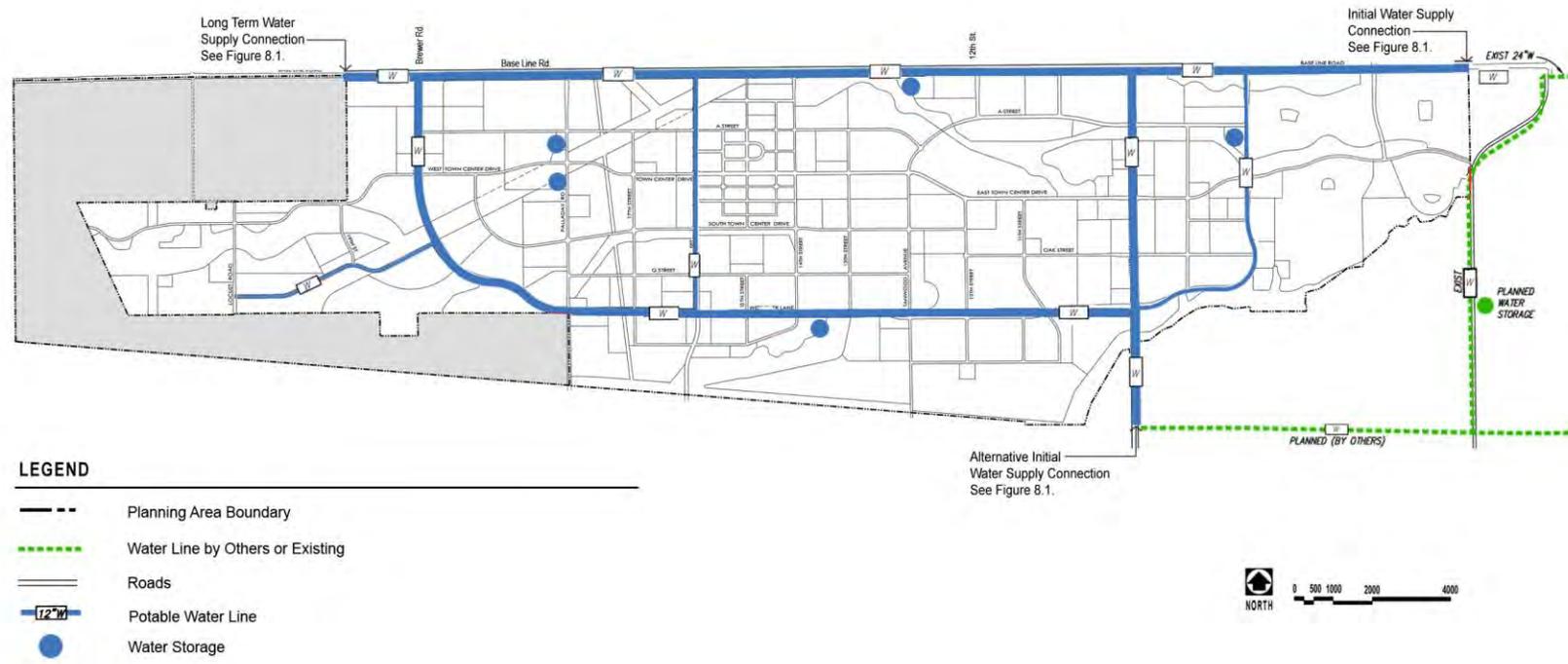
Figure 8.1 - Off-Site Water Supply and Distribution System



Source: Quad Knopf/MacKay & Soms 2005

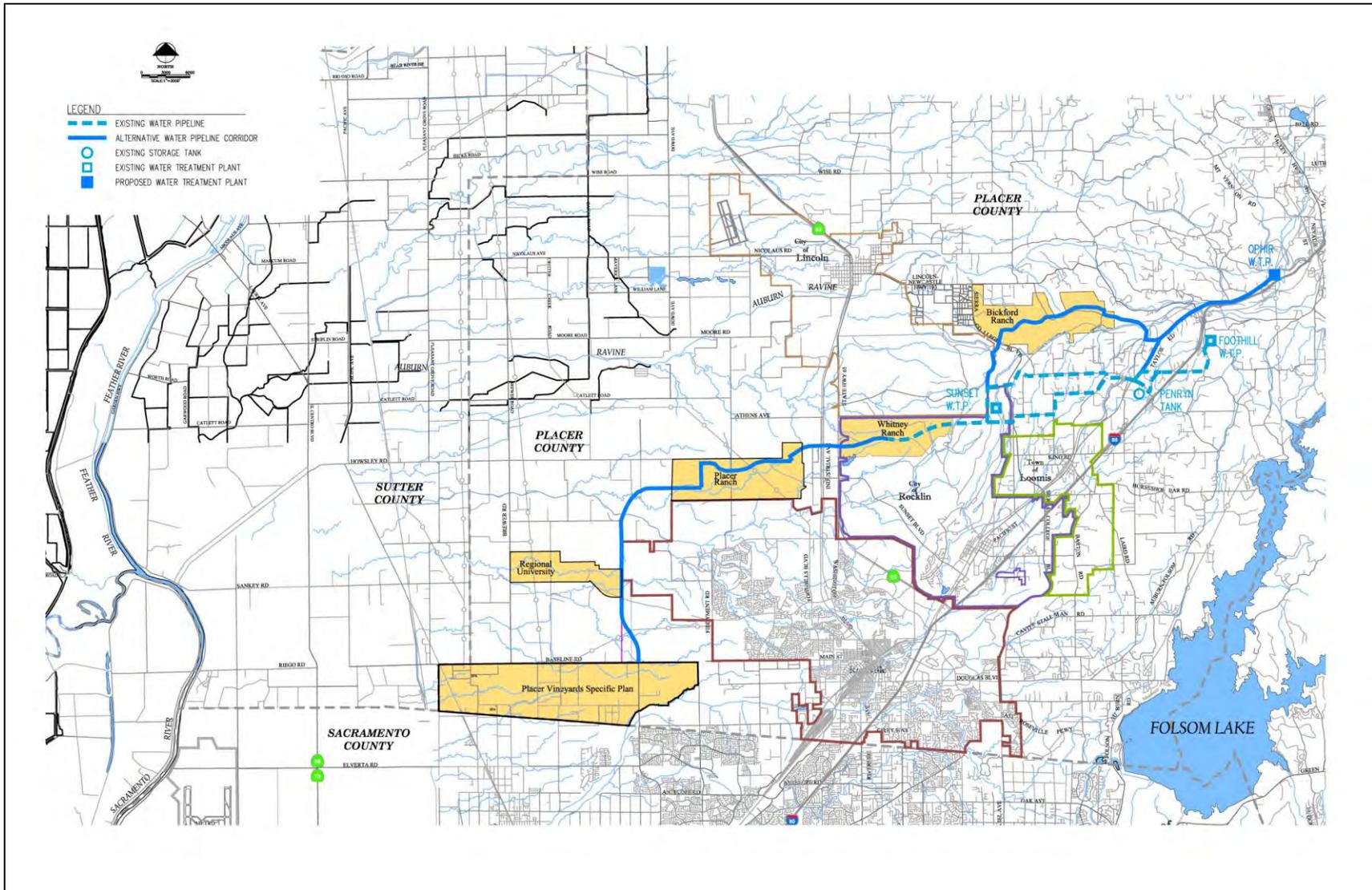
PUBLIC UTILITIES AND SERVICES

Figure 8.2 - On-Site Water Supply and Distribution System



Source: MacKay & Soms 2005

Figure 8.3 - Alternative Off-Site Water Supply and Distribution System



Source: MacKay & Soms 2007

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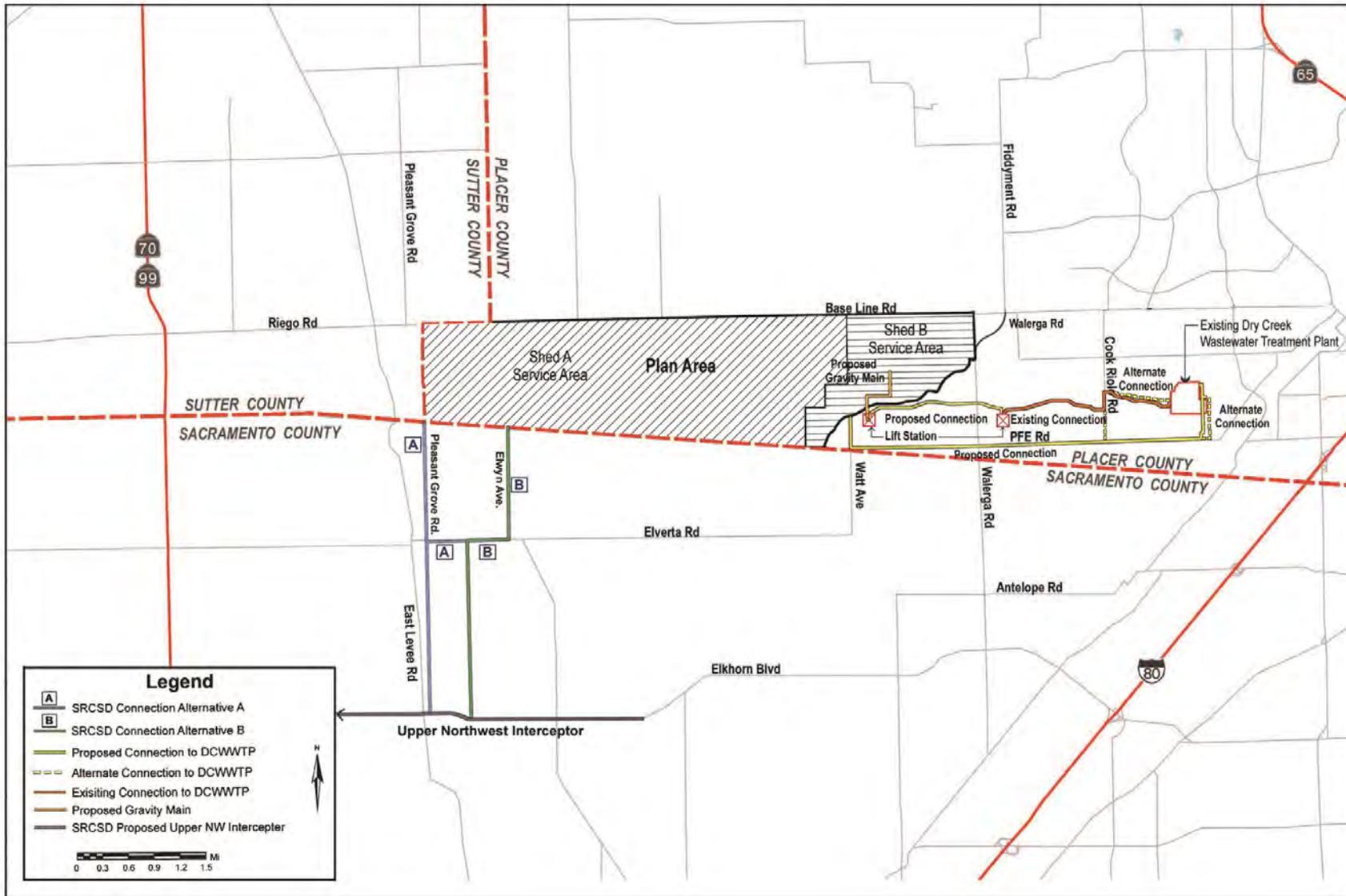
Figure 8.4 - On-Site Recycled-Water Distribution System



Source: MacKay & Somps 2005

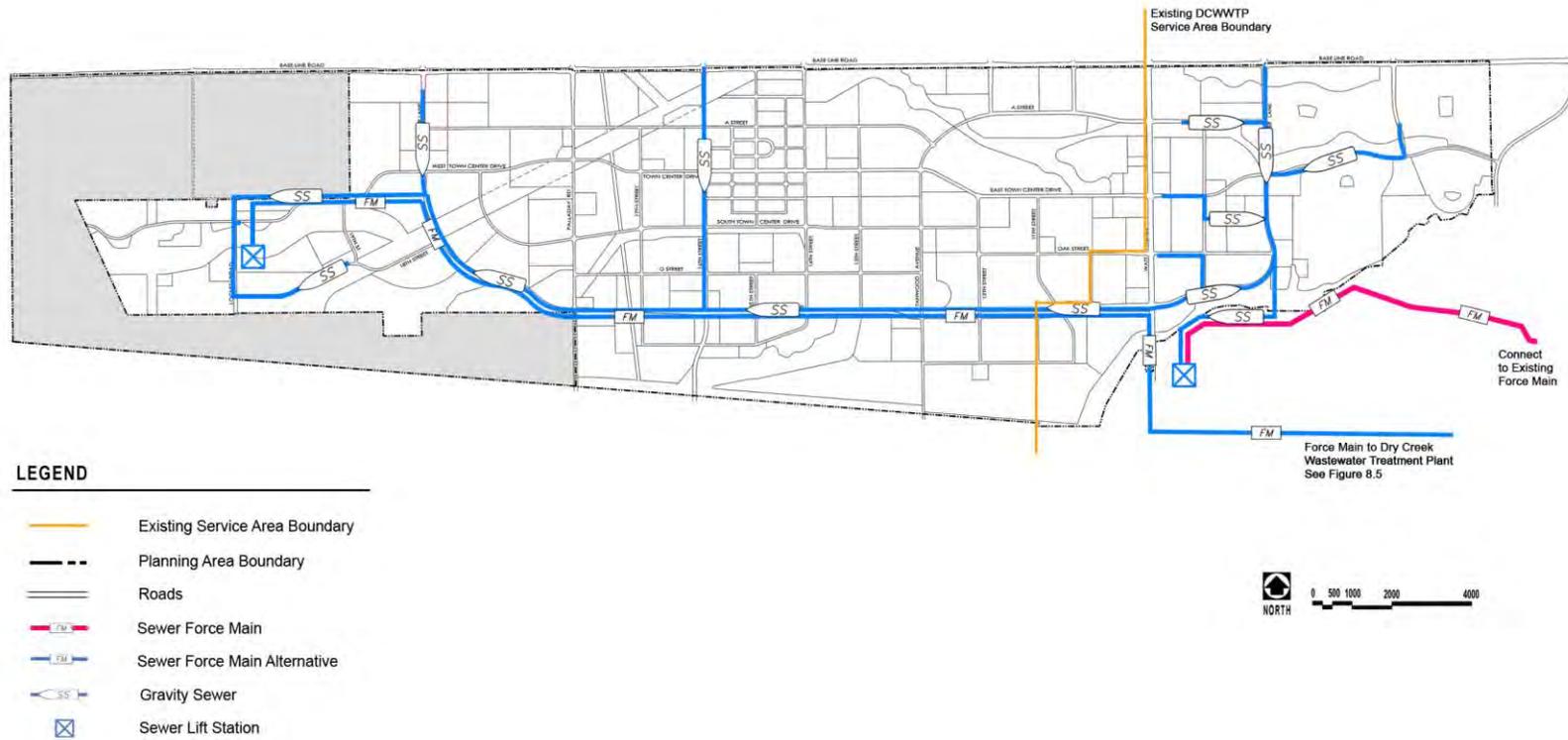
PUBLIC UTILITIES AND SERVICES

Figure 8.5 Off-Site Wastewater Collection System



Source: Quad Knopf/MacKay & Somp 2005

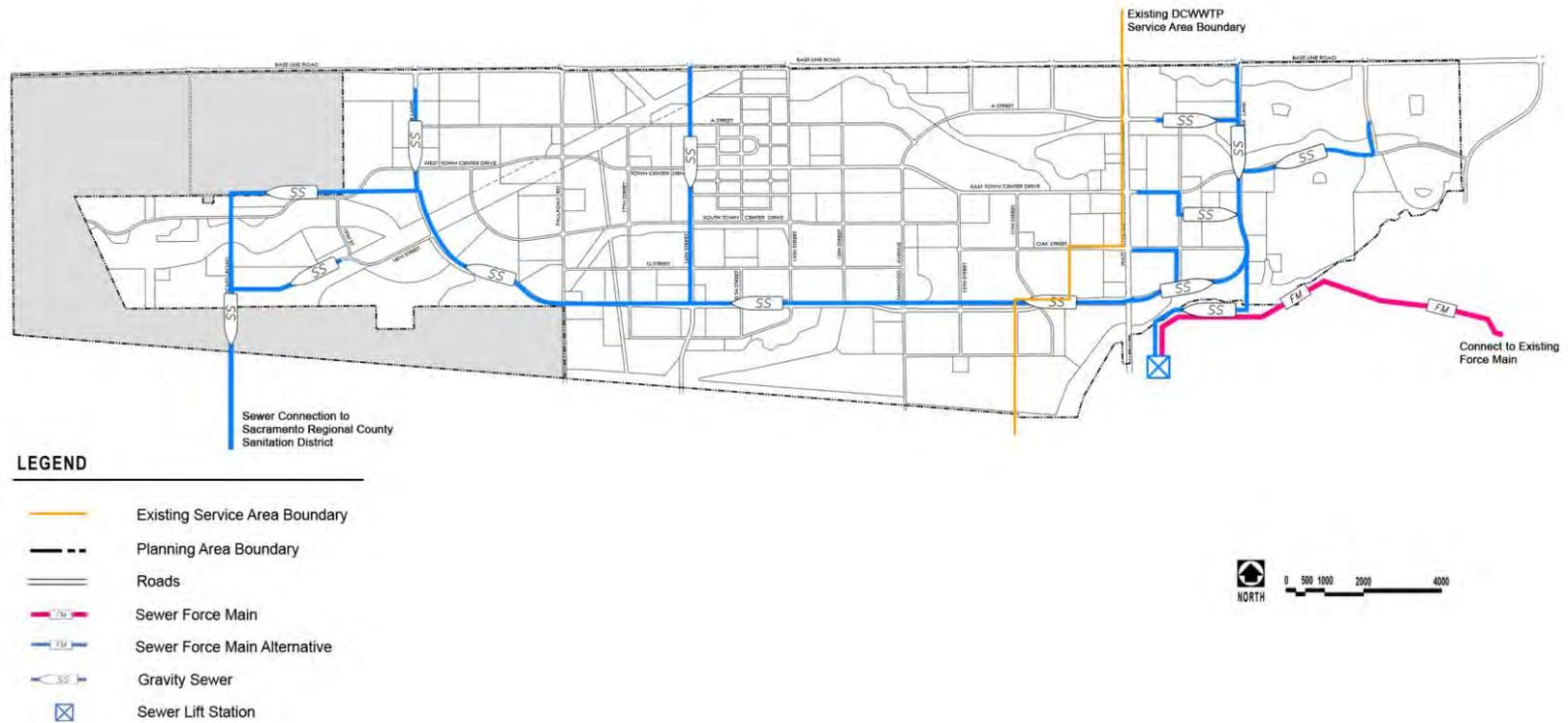
Figure 8.6 - On-Site Wastewater Collection System



Source: MacKay & Somps 2014

PUBLIC UTILITIES AND SERVICES

Figure 8.7 - Alternative On-Site Wastewater Collection System



Source: MacKay & Soms 2014

### 8.2.3 DRAINAGE AND FLOOD CONTROL

The following section summarizes more detailed drainage information included in the *Master Project Drainage Study* (Civil Solutions 2006). The Placer County Flood Control and Water District and Department of Public Works establish the criteria for the construction and maintenance of drainage facilities. Requirements for the construction of drainage facilities are found in the *Placer County Land Development Manual* and *Storm Water Management Manual*.

**Goal 8.7** Use and preserve existing drainage ways as much as possible and design flood control facilities to preserve significant wetlands and avoidance areas where sensitive features exist.

**Policy 8.4 Service Standards**

*All Plan Area improvements shall be designed and constructed in accordance with standards listed in the Placer County Land Development Manual and Storm Water Management Manual.*

**Policy 8.5 Drainage Standards for Individual Projects**

*Individual projects shall provide appropriate short and long-term best management practices and source controls consistent with the land use.*

#### Existing Conditions

The hydrologic characteristics in the Plan Area are largely affected by the seasonal rainfall, with the exception of Dry Creek. Dry Creek flows year round because of the size of its watershed, the discharge of ditch water from PCWA into creek tributaries, and the discharge of effluent water from the Roseville Sewer Treatment Plant. The Plan Area is divided into 3 major drainage sheds: Curry Creek, Dry Creek, and Steelhead Creek (formerly known as the Natomas East Main Drainage Canal [NEMDC]).

#### Dry Creek Watershed

The southeastern portion of the Plan Area drains directly to the Dry Creek floodplain. Drainage is carried through a number of small intermittent shallow swales and roadside ditches to Dry Creek without collecting at any one location.

#### Curry Creek Watershed

The northeast part of the Plan Area drains to Curry Creek, which is tributary to the NEMDC, which in turn is north of the Plan Area. The Curry Creek watershed is an intermittent drainage, with flows occurring only after rainfall in the area. Only about 240 acres of the Curry Creek watershed lie within the Plan Area.

#### Steelhead Creek Watershed

The remainder of the Plan Area drains through tributaries of Steelhead Creek, formerly known as NEMDC. The upper NEMDC watersheds occupy the majority of the Plan Area. Seven independent watersheds within the Plan Area drain from east to west and terminate at Steelhead Creek. Three watersheds drain into Sutter County, west of the Plan Area, and one watershed drains north across Base Line Road.

There are no distinct channels in any of the 7 Steelhead Creek watersheds; drainage is carried west through the Plan Area via many shallow swales and roadside ditches. Water flows in these intermittent swales only after rainfall in the area.

#### Proposed Drainage improvements

The Placer Vineyards drainage system is designed to discharge into channels and detention facilities, providing detention and retention of increased runoff volumes. Flood control within the Plan Area will consist of newly constructed channel systems and parallel flood control channels where avoidance areas are to be maintained. These facilities would generally follow or be placed along natural drainage courses. Other types of facilities include excavated lake areas, constructed wetland areas, and water quality basins and channels. Refer to the *Master Project Drainage Study* for more detailed information and the location of drainage improvements needed to serve Plan Area build-out.

#### Drainage Design Criteria

- ◆ Open space corridors have been created in the Plan Area to convey stormwater flows. All development will occur outside of these corridors so as to provide 100year flood protection to all residences.
- ◆ Piped drainage collection facilities of up to 96 inches in diameter will be used before open channels are chosen to convey urban stormwater runoff.
- ◆ Manage urban runoff through use of stormwater conveyance, detention, and water treatment facilities. Planned channel improvements should include components to mitigate adverse impacts on wetlands.
- ◆ When possible, increase the depth of existing drainage courses to accommodate storm drainage collection.
- ◆ Vegetation proposed within the low-flow open channel(s) shall provide treatment of urban stormwater runoff.

## PUBLIC UTILITIES AND SERVICES

### Beaver Dam Management

Beaver dams create problems when they create drainage backwater conditions. These impacts may be mitigated by the County when they interfere with utility operations, cause property damage, negatively affect certain natural resources (such as preserve areas, riparian and woodland vegetation), or present other health and safety issues.

#### **Policy 8.6 Beaver Dam Management**

*When found to create a potential health and safety hazard, beaver dams shall be breached or removed according to procedures determined by the County, including proper disposal of vegetation and woody materials removed from beaver dams.*

### 8.2.4 DISPOSAL OF SOLID WASTE

Solid waste generated by existing residents of the Plan Area is collected and disposed of by Recology Auburn Placer. After collection, solid waste is transported to the Western Placer Waste Management Authority's Materials Recovery Facility where recyclables are recovered from the waste stream. Residual solid waste is disposed at the adjacent Western Regional Sanitary Landfill, which is anticipated to serve the needs of Placer County through the year 2058.

### 8.2.5 ELECTRICITY AND NATURAL GAS

The Plan Area is located within the service area of two electrical utility companies, Sacramento Municipal Utility District (SMUD) and Pacific Gas and Electric Company (PG&E). The boundaries of each service territory are shown in Figure 8.8. Gas service will be provided to the Plan Area by PG&E.

#### Existing Electrical Service Lines and Substations

Three power line easement corridors cross the Plan Area. These are owned by PG&E, SMUD, and the Western Area Power Administration (WAPA). SMUD and PG&E own existing facilities and maintain numerous 12-kilovolt (kV) power lines in the Plan Area. These service lines are generally located along roadways and provide services to existing residences in the area.

The SMUD Black Eagle Crystal Ridge Substation is located in close proximity to the Plan Area, off of Black Eagle Drive and Watt Avenue. It is fed by a 69-kV line that extends west along PFE Road from the power line easement between Cook Riolo Road and Walerga Road. The substation is currently approaching capacity and will need to be increased in capacity and site area to be able to serve the Plan Area.

PG&E maintains two substations that serve the Plan Area. The Catlett Substation, located on Field Road just east of Natomas Road in Sutter County, feeds the circuit on Pleasant Grove Road. The Pleasant Grove Substation on Industrial Boulevard feeds the Fiddymont Road circuit. Both have the capacity to carry additional service loads.

#### New Electrical Service Lines

When the demand exceeds the load capacity, new 12-kV or 21-kV lines will need to be extended from existing or new substations. A location for a new electrical substation is set aside in the Placer Vineyards land use plan.

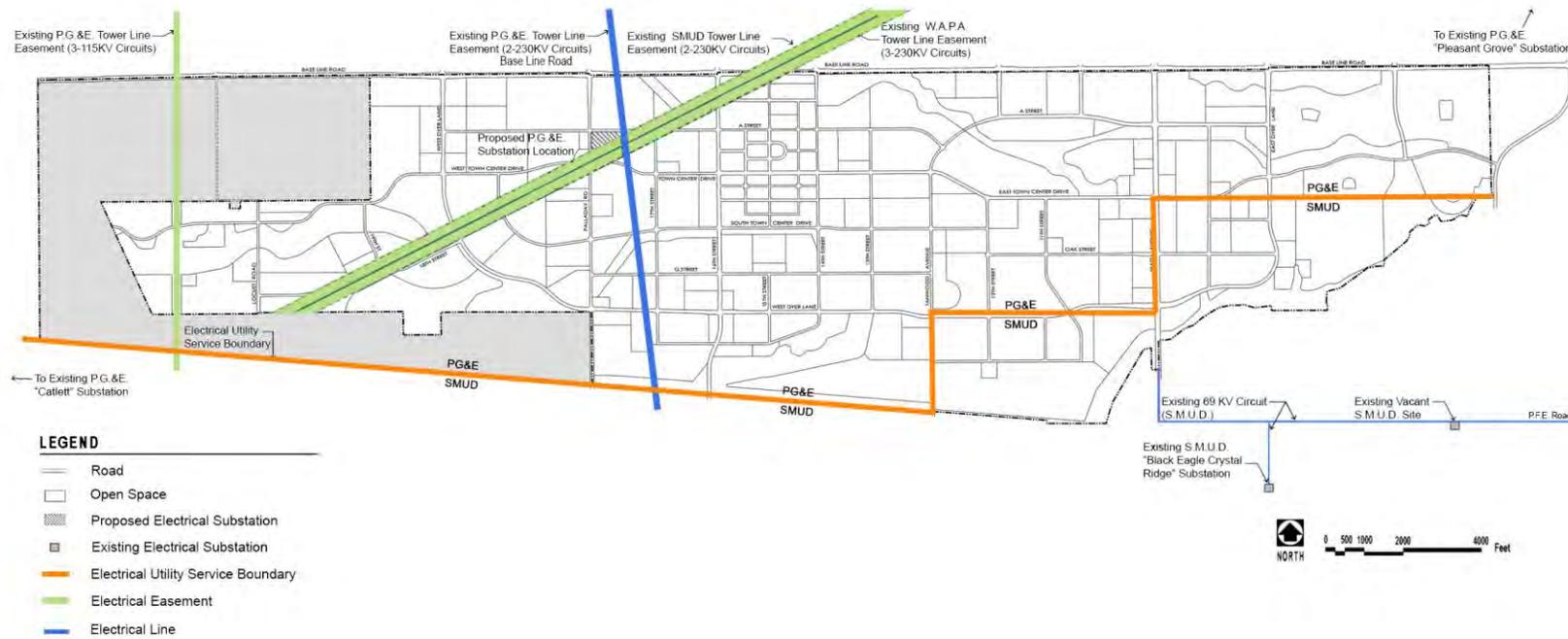
#### Existing Gas Service Lines

Several natural gas lines run north toward the Plan Area and stop short of the Sacramento County and Placer County line. A 12-inch high-pressure transmission main is located east of the intersection of Cook Riolo Road and Base Line Road, two miles east of the Plan Area.

#### New Gas Service Lines

Service will be obtained by constructing off-site gas transmission facilities to serve the Plan Area. Extension of gas mains to the individual project sites will be required. Connection to the 12-inch high-pressure transmission main at Cook Riolo Road and Base Line Road will require the construction of a pressure regulation station at the point of connection. Initial service will be provided by extending a distribution main along Base Line Road and a transmission main along PFE Road. The Base Line Road main will function as a backbone main serving the entire Plan Area at build-out. Smaller distribution mains will be stubbed off this backbone main and looped through the internal street system.

Figure 8.8 - Electrical Distribution System



Source: Quad Knopf/MacKay & Soms 2005

## PUBLIC UTILITIES AND SERVICES

### 8.2.6 TELEPHONE/COMMUNICATIONS

The Plan Area is currently served by Consolidated Communications and AT&T.

#### Existing Telephone Facilities

Both AT&T and Consolidated Communications own and maintain pole lines in the Plan Area, however, none of the existing facilities have available reserve capacity. Therefore, these facilities will need to be upgraded and placed underground.

#### New Telephone Facilities

Fiber optic facilities owned by Consolidated Communications, approximately 1/2 mile east of the Plan Area, on Crowder Lane and Base Line Road will be extended into the Plan Area as development occurs. AT&T also plans to extend its facilities into the Plan Area as development occurs. Within the Plan Area, all telephone services will be constructed underground along road corridors.

### 8.2.7 CABLE TELEVISION SERVICE

No cable television service is currently available in the area and no cable television provider has a franchise for the area. Comcast has fiber optic infrastructure in the vicinity and, were it to enter into a franchise agreement with the County, it could provide service to the Plan Area. If that were to occur, cable would be extended west from the Roseville city limits along Base Line Road. Cable television will be phased with the construction of major roads. All cable facilities will be installed underground for the Plan Area. If that were to occur, cable would be extended west from the Roseville city limits along Base Line Road. Cable television will be phased with the construction of major roads. All cable facilities will be installed underground

### 8.3 COMMUNITY SERVICE FACILITIES

This section addresses community service facilities to support the residential population of Placer Vineyards, including schools, public safety facilities, and County service facilities. Parks and open space facilities are covered in Chapter VII. Details related to the funding and timing of community service facilities are included in the *Urban Services Plan*.

**Goal 8.8** Locate neighborhood facilities (neighborhood parks and elementary schools) generally central to each neighborhood so that such facilities are within 1/2 mile or less of a majority of neighborhood residents.

#### 8.31 EDUCATION FACILITIES

The Plan Area falls within 3 school districts, as shown in Figure 8.9. Center Unified School District (CUSD), providing elementary and high school services, is located in the east portion of the Plan Area. The Elverta Joint Elementary School District and Grant Joint Union High School District are located in the west portion of the Plan Area and share the same district boundary line. The existing school district boundaries were set before the development of this Specific Plan and thus, fall in the middle of proposed neighborhoods in the Specific Plan.

#### Student Generation and School requirements

Demand for school facilities, sizing, location, and generation rates have been based on information provided by CUSD and state standards. Table 8-2 summarizes the anticipated numbers of students in the Plan Area and the number of schools needed based on the land use plan proposed for the Plan Area.

#### School Facility Sizes and Locations

In the Plan Area, standard school sizes are generally 12 acres for elementary schools, 22.5 acres for middle schools, and 50 acres for the high school. Schools have been collocated with park sites to encourage the joint use of facilities. School/park sites serve as the focal point of each neighborhood, creating a gathering place within a half-mile distance of most surrounding residences. Most schools are located near open space corridors for safe passage routes to schools. See Figures 8.10 and 8.11 for conceptual school layouts.

**Table 8-2: School Enrollment Summary**

Unit type	Dwelling Units (du)	Elementary School (K 6) Student Generation Rate <sup>2</sup>	Elementary School Population	Middle School (7 8) Student Generation Rate <sup>2</sup>	Middle School Population	High School (9 12) Student Generation Rate <sup>2</sup>	High School Population
Low-Density <sup>1</sup>	2,588	0.383	991	0.126	326	0.228	590
Medium-Density	6,474	0.383	2,480	0.126	816	0.228	1,476
High-Density	3,092	0.151	467	0.058	179	0.127	393
Commercial/Mixed-Use	636	0.151	96	0.058	37	0.127	81
Special Planning Area	411	0.383	157	0.126	52	0.228	94
<b>Total</b>	<b>13,201</b>		<b>4,191</b>		<b>1,410</b>		<b>2,633</b>
Total School Population			<b>8,234</b>				

Notes:

1. Excludes age-restricted units.
2. Student generation rates provided by Center Unified School District, October 2005

PUBLIC UTILITIES AND SERVICES

Figure 8.9 - School District Boundaries

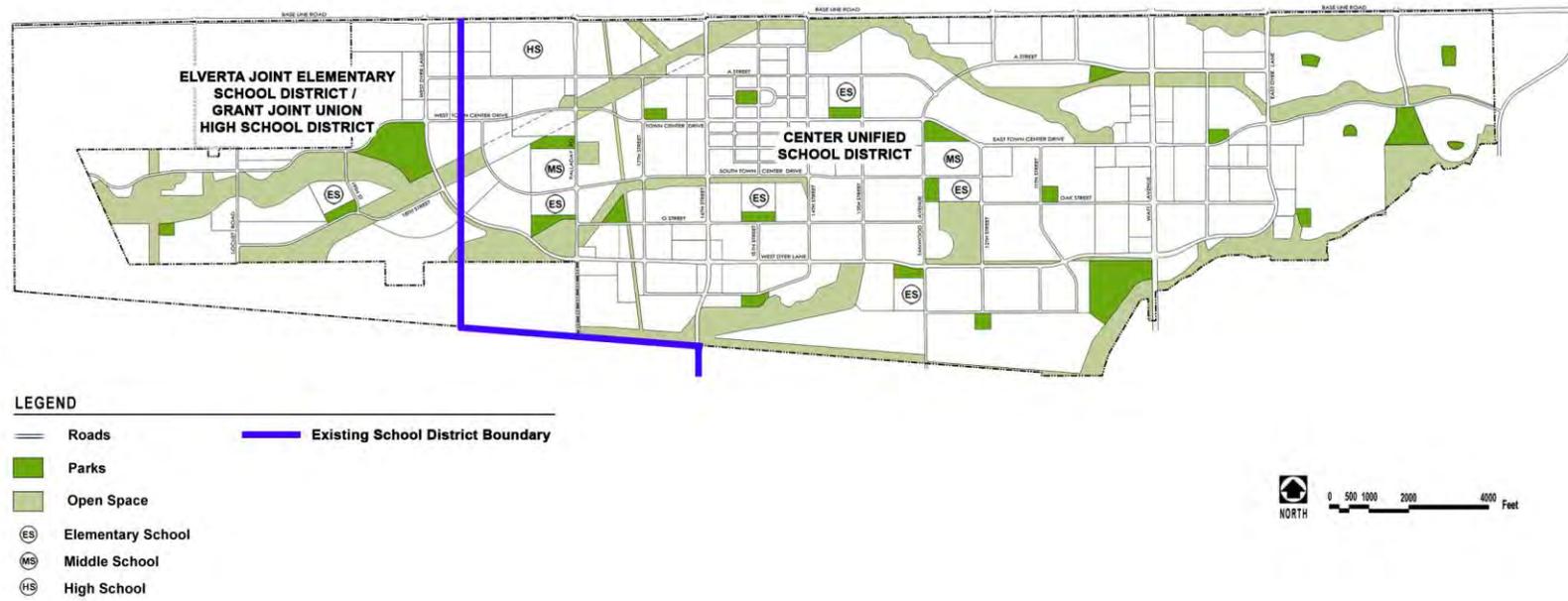


Figure 8.10 - Conceptual Elementary and Middle School Site Designs

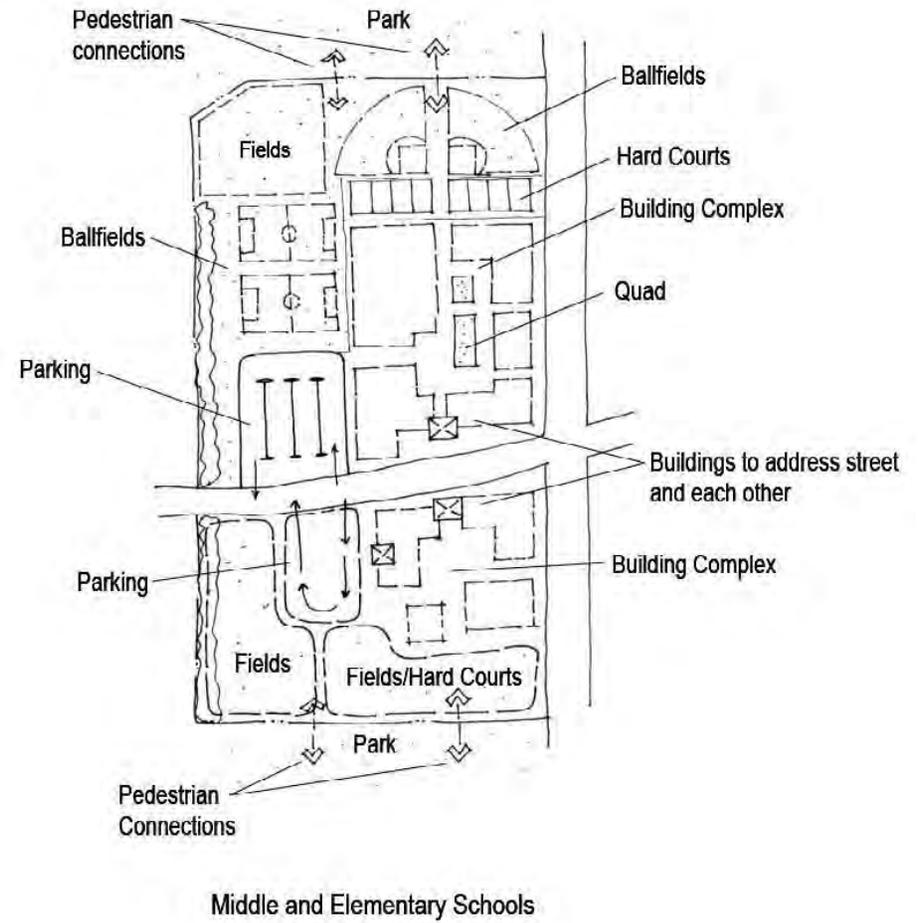
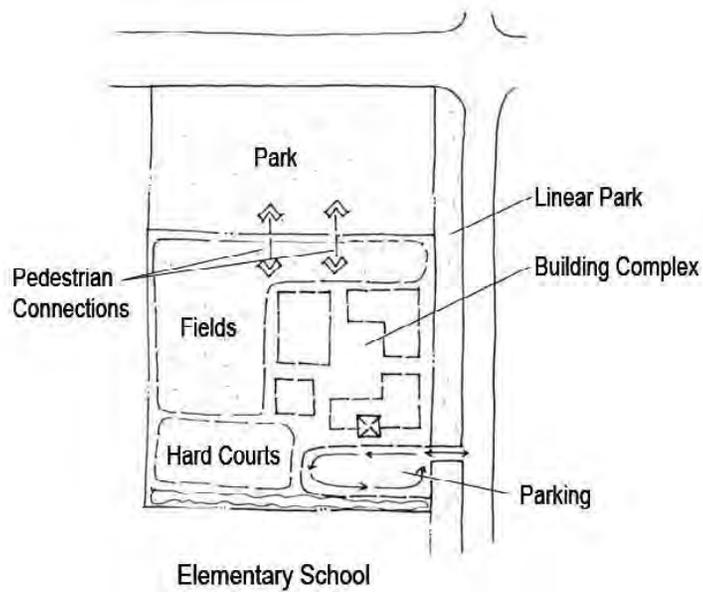
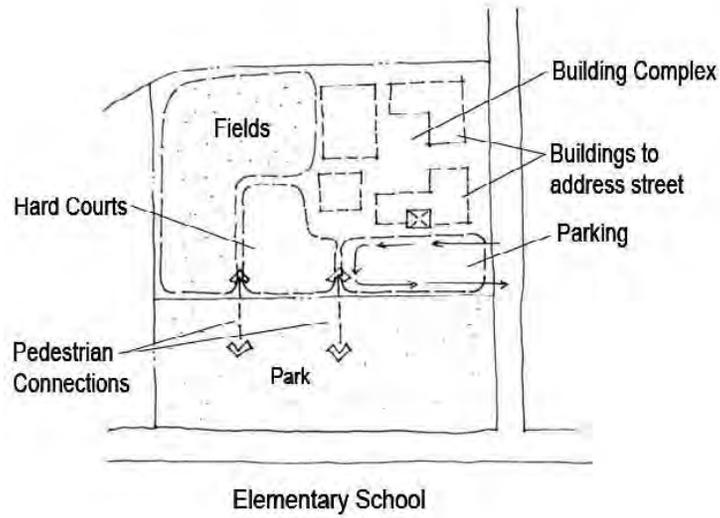
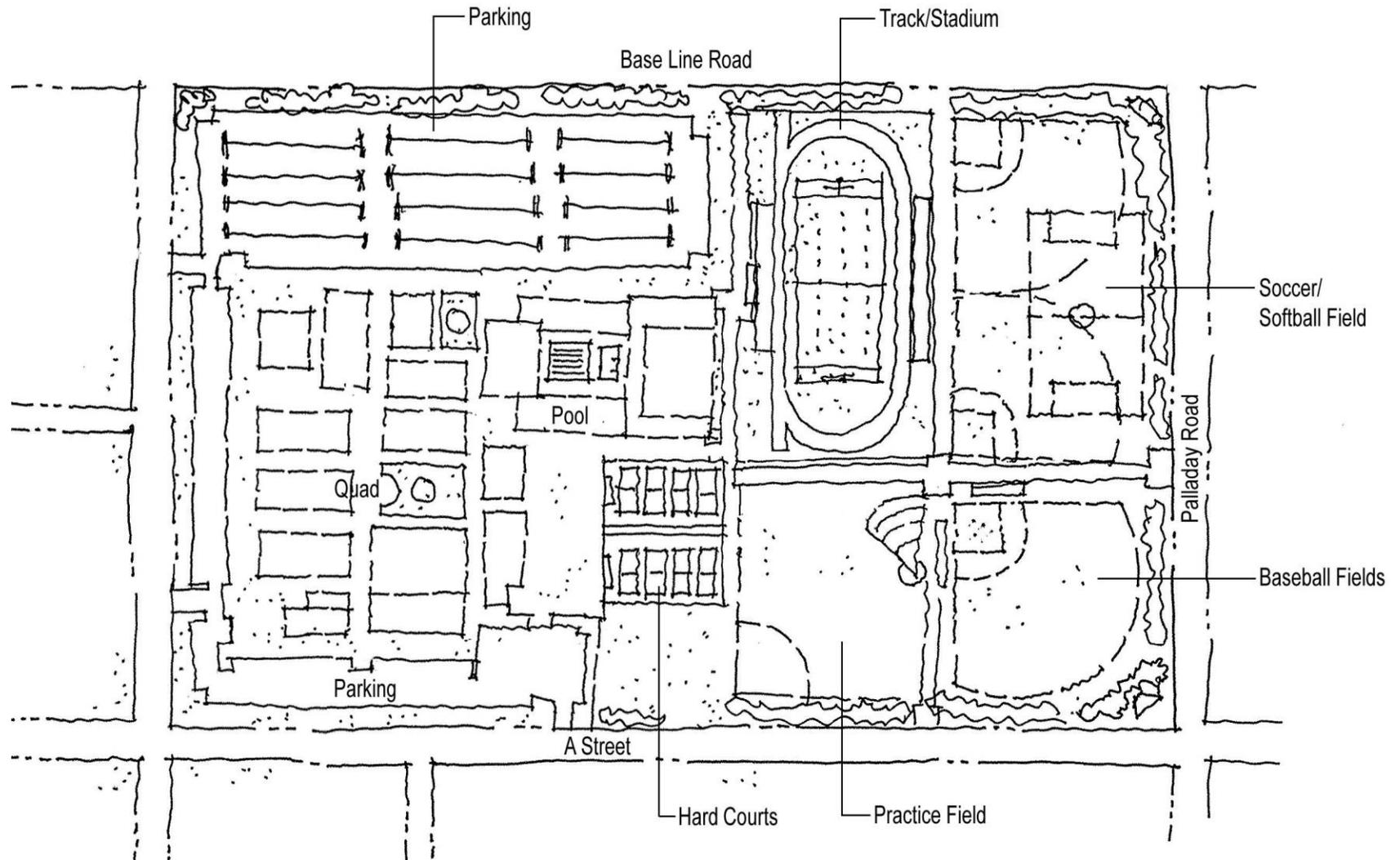


Figure 8.11 - Conceptual High School Site Design



### 8.3.2 COMMUNITY FACILITIES

#### Planned County Facilities

Placer County uses a County Capital Facilities Fee (CFF) to mitigate the impacts of new development on County facilities. Collection of this fee will provide funding for improvements to an expansion of the County's finance and administration facilities, justice system, health and human services facilities, and public works facilities needed to maintain appropriate levels of service in the Plan Area.

Several satellite County facilities need to be located within the Plan Area to ensure that adequate general services are provided. These facilities and their respective locations in the Plan Area include:

- ♦ New government administration service offices and County Sheriff's Department substation (designated "Gov" in the Land Use Plan), to be located in the town center;
- ♦ A corporation/maintenance yard (designated "CY" in the Land Use Plan), planned and located on Town Center Drive a short distance from the town center, adjacent to open space buffers with some separation from the surrounding residential community; and

#### Planned Library Facilities

Libraries for the Plan Area are currently administered by the Auburn Placer County Library Department. Current residents of the Plan Area are served by a bookmobile that visits the area an average of twice a month. The closest existing library facility is the City of Roseville main library, approximately 3.5 miles away, located at 225 Taylor Street, off Walerga Road. The Specific Plan provides a site for the proposed library in the Town Center (designated "L" on Figure 3.1). Placer Vineyards will pay the library portion of the County Public Facilities Impact Fee as its fair share of the costs for the construction of the library facility.

#### Cemetery

The cemetery proposed for the Plan Area shall be owned and operated by the Roseville Public Cemetery District or an independent public district. The cemetery shall be located near the corner of West Town Center Drive and Palladay Road, with portions of the site located below the power line easement corridor.

### 8.3.3 PUBLIC SAFETY FACILITIES

#### Existing Fire Protection

Fire protection is currently provided by the Placer County Fire District (PCFD), with the exception of a 1 square mile area in the southwestern portion of the Plan Area, which is served by the Sacramento Metropolitan Fire District. Primary response to emergency calls from the Plan Area is currently handled by the PCFD Dry Creek Fire Station east of the Plan Area on Cook Riolo Road.

#### Planned Fire Protection

Analysis of the necessary stations, equipment, staffing, and response time has been performed by PCFD staff. 2 new PCFD stations and an administrative center are anticipated to serve the Plan Area. The fire administrative center is to be collocated with other County administrative offices in the town center. The planned locations for the two fire stations are shown on the land use diagram (Figure 3.1). Refer to the Placer Vineyards Public Facilities Financing Plan for the timing and specific details related to the construction of fire station facilities.

#### Existing Law Enforcement Services

Existing law enforcement services are provided by Placer County Sheriff's Department and traffic enforcement is provided by the California Highway Patrol. The nearest sheriff's substation is located in Loomis at the intersection of Horseshoe Bar Road and Interstate 80.

#### Planned Law Enforcement Services

Law enforcement for the Plan Area will be provided by the Placer County Sheriff's Department. Analysis of the necessary stations, equipment, staffing, and response times has been performed by Placer County Sheriff's Department staff. A sheriff's substation site, as shown in the land use diagram (Figure 3.1), is proposed for collocation with other County administrative offices within the town center.

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**CHAPTER IX: IMPLEMENTATION**



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## 9.1 OVERVIEW

### Context

This chapter outlines the methods by which the Specific Plan will be implemented and includes a discussion of administrative procedures, the timing of development, and financing. California Government Code Section 65451 and *Placer County Zoning Ordinance* Section 17.58.200 require that specific plans include a program of implementation measures, including regulations, programs, public works projects, and financing measures needed to carry out the proposed land use, infrastructure, development standards, and criteria outlined in the Specific Plan.

Placer Vineyards is projected to be built-out over 20 to 30 years. Thus, the implementation policies are intended to ensure that implementation will be comprehensive, coordinated, and responsive to changing circumstances and market conditions. The objective of this section is to describe how infrastructure and public facilities will be constructed in a timely manner, concurrent with the provision of housing and other land uses.

The remainder of this chapter is divided into three sections: “Administrative Procedures,” “Timing of Development Infrastructure and Public Services” and “Specific Plan Financing Strategy.”

Section 9.2, “Administrative Procedures,” identifies the procedural steps in implementing the Specific Plan and discusses the subsequent approvals necessary to begin construction of individual projects and subdivisions, as well as modifications and amendments to the Specific Plan. Section 9.3, “Timing of Development Infrastructure and Public Services,” describes the process and sequence of infrastructure and facilities to serve the Plan Area over time. Section 9.4, “Specific Plan Financing Strategy,” describes the financing methods that will allow development to proceed in an orderly and fiscally responsible manner.

Refer also to the applicable chapters of the Specific Plan for the following information:

- ◆ Chapter I, “Introduction,” for the framework of the Specific Plan and its relationship to other documents and regulatory approvals;
- ◆ Chapter III, “Land Use,” for land use regulations; Chapter IV, “Environmental Resources,” for the protection of site resources;
- ◆ Chapter IV, “Environmental Resources,” for the protection of site resources
- ◆ Chapter V, “Transportation and Circulation,” for design standards for roadways, trails, and transit systems;

- ◆ Chapter VII, “Parks and Open Space,” for a discussion of the parks and open space system; and
- ◆ Chapter VIII, “Public Utilities and Services,” for an overview of the backbone infrastructure systems and public services required to serve the Plan Area



The following documents have been prepared in support of this Specific Plan and contain more detailed information on environmental conditions, infrastructure, and financing mechanisms:

- ◆ Certified (Resolution No. 2007-229) Placer Vineyards Specific Plan Final Environmental Impact Report and Mitigation Monitoring and Reporting Program (Quad Knopf-October 2006).
- ◆ Addendum (Resolution No. 2012-038) to the Environmental Impact Report to modify the Mitigation Monitoring and Reporting Program and corresponding text revisions to the Certified Environmental Impact Report (February, 2012)
- ◆ Amendment (Resolution No. 2012-039) to the adopted Placer Vineyards Specific Plan (February 2012).
- ◆ Modifications (Resolution No. 2012-211) to the 2007 Placer Vineyards Specific Plan Mitigation Monitoring and Reporting Program and corresponding text revisions to the Certified Environment Impact Report (September 2012).
- ◆ Placer Vineyards Wetland Delineation (ECORP-Various Dates)
- ◆ Corps of Engineers 404 Permit Application (ECORP-May 2006)
- ◆ Placer Vineyards Master Drainage Plan (Civil Solutions-August 2006)
- ◆ Placer Vineyards Sewer Master Plan (MacKay & Soms-March 2006)
- ◆ Placer Vineyards Water Master Plan (Brown and Caldwell-September 2006)
- ◆ Placer Vineyards Recycled Water Master Plan (Brown and Caldwell-August 2006)
- ◆ Placer Vineyards Public Facilities Financing Plan (EPS-July 2007) and updates to the 2007 plan.
- ◆ Placer Vineyards Urban Services Plan (EPS-July 2007) and updates to the 2007 plan.

## IMPLEMENTATION

### 9.2 ADMINISTRATION PROCEDURES

#### 9.2.1 SPECIFIC PLAN APPROVALS

The following actions occurred concurrently with the adoption of this Specific Plan.

- ◆ **Placer Vineyards Specific Plan and Development Standards:** The County Board of Supervisors adopted the Placer Vineyards Specific Plan by resolution on July 16, 2007 and adopted the Specific Plan Land Use and Development Standards attached to this Specific Plan as Appendix A, by ordinance on the same date (see Section 9.2.3 that follows);
- ◆ **Final Environmental Impact Report (FEIR):** The County Board of Supervisors certified the FEIR addressing this Specific Plan and any necessary amendments to the *Placer County General Plan* and *Dry Creek/West Placer Community Plan*; on July 16, 2007
- ◆ **Zoning:** The County Board of Supervisors approved the Specific Plan zoning designations by ordinance on July 16, 2007 (see Section 9.2.2 that follows); and
- ◆ **Public Facilities Financing Plan and Urban Services Plan.** The Board of Supervisors accepted the Public Facilities Financing Plan and Urban Services Plan on July 16, 2007. The PFFP identifies the estimated costs of public facilities and describes the mechanisms for funding these facilities; the Urban Services Plan identifies the level of public services expected within Placer Vineyards and describes the funding methods which might be used.
- ◆ **Development Agreements:** Property owners who have been participating in the planning and preparation of the Specific Plan (the “Participating Developers”) entered into a Development Agreement with the County in 2007. The Development Agreement set forth property owners’ obligations related to the construction and financing of infrastructure, County facilities and public services, including financial contributions for public infrastructure and facilities maintenance, provision of urban services for the Plan Area, and other obligations that may be imposed by the County as a condition of development. The Development Agreement vests the property with the right to proceed to development subject to the limitations and obligations of the Development Agreement and the Specific Plan.

The following actions occurred after approval of the original Specific Plan and prior to the approval of the first Development Phase and Phasing Plan:

- ◆ **Specific Plan:** On February 14, 2012, the Board of Supervisors adopted an amendment to the Specific Plan and on December 9, 2014 the Board adopted this Amended Specific Plan.
- ◆ **Environmental Impact Report:** On February 14, 2012, the Board of Supervisors adopted an Addendum to the Certified Environmental Impact Report and on September 11, 2012, the Board approved text revisions to the Certified Environmental Impact Report and modifications to the Mitigation Monitoring and Reporting Program. On December 9, 2014 the Board adopted an additional Addendum to the Certified Environmental Impact Report.
- ◆ **Development Agreement:** The original Development Agreement approved in 2007 was superseded by the Amended and Restated Development Agreement dated February 14, 2012 and the First Amendment to Amended and Restated Development Agreement, dated September 11, 2012 (collectively the “First Restated Development Agreement”). On December 9, 2014 the Second Amended and Restated Development Agreement was approved by the Board of Supervisors. The Second Amended and Restated Development Agreement replaces and supersedes all previous Development Agreements.
- ◆ **Landscape Master Plan:** A Landscape Master Plan was submitted to the County for review and conceptual prior to submission of the first Development Phase and Phasing Plan application. The Landscape Master Plan addresses the design of the streetscape, landscape corridors adjacent to streets, landscaped buffer areas, other open space areas, community entries, street lights, and other image features that help establish the landscape and streetscape character of the community. The Board of Supervisors shall approve the Landscape Master Plan prior to the approval of the first Development Phase and Phasing Plan.
- ◆ **Sewer Master Plan:** The Development Group prepared and obtained approval from the County of a Sewer Master Plan for providing sewer service to the developed properties within the Specific Plan area. The Sewer Master Plan includes information on wastewater generation rates, peaking factors, location, placement and sizing of gravity pipelines, force mains, lift stations, and other necessary infrastructure.

- ♦ **Drainage Master Plan:** The Development Group prepared and obtained approval from the County of a Drainage Master Plan updating the work previously undertaken in conjunction within the EIR. The Drainage Master Plan identifies each of the drainage sheds with the Plan Area and the area-wide drainage facilities (the “Permanent Drainage Facilities”) required to serve each of the drainage sheds. Subject to other agency approvals, including but not limited to the Regional Water Quality Control Board, the U.S. Army Corps of Engineers and the California Department of Fish and Game, the Drainage Master Plan identifies the size and location of all Permanent Drainage Facilities proposed for each of the drainage sheds with the Plan Area;
- ♦ **Transit Master Plan:** The Development Group prepared and obtained approval from the County of a Transit Master Plan for public transit service to the Specific Plan area. The Transit Master Plan includes detail on routes, service times, fares programs (including a method to determine fair share costs for inter-community and inter-regional routes connecting the Specific Plan area to other areas within and outside Placer County), vehicle requirements, service triggers establishing the timing for expansion of service to reach ultimate service levels, staffing requirements, administrative costs, capital requirements, and other related information necessary to provide a complete transit service;
- ♦ **Public Facilities Financing Plan and Urban Services Plan:** Both the Public Facilities Financing Plan and the Urban Services Plan, originally accepted in 2007, have been updated and accepted by the Board of Supervisors concurrent with the approval of this Specific Plan Amendment.
- ♦ **Establishment of Urban Services Financing Mechanisms.** The Urban Services Plan will be used, in conjunction with the Development Agreement, to decide financing urban services through a Community Facilities District (CFD) or County Service Area (CSA). Cost estimates may be updated, final project taxes and assessments defined, and any necessary CFD or CSA formed; and
- ♦ **Implementation Policies and Procedures Manual.** This manual was approved by the Board of Supervisors in 2013 and provides the County with a comprehensive approach for processing approvals and issuing permits for development within the Plan Area, including developing forms and checklists, processing approvals consistent with the procedures set forth in the Development Agreement and in the Specific Plan, and obtaining Certificates of Good Standing and any required Specific Plan shortfall payments as and when required. The County may periodically update the Implementation Policies and Procedures Manual

## 9.2.2 SPECIFIC PLAN AREA ZONING

The County approved by ordinance “Specific Plan District” (SPL) zoning for the Specific Plan (participating properties only) in 2007. The SPL district, in combination with the Specific Plan, functions as the zoning text and map for the urban portion of the Placer Vineyards Specific Plan Area. Plan Area participating properties are designated SPL-PVSP on the official County of Placer zoning maps.

The approximately 505 acre portion of the Plan Area, owned by the property owners who did not participate (or have elected to withdraw from such participation) in the preparation of the Specific Plan and have not entered into Development Agreements with the County retain their existing rural zoning classifications until the Non-Participating Property Owners elect to develop their properties for urban uses. Refer to Section 9.2.6.C for the procedures regulating the properties of the portions of the Plan Area owned by Non-Participating Property Owners.

The remaining approximately 979 acres of the Plan Area constitutes the Special Planning Area (SPA) portion of the Specific Plan. Refer to Section 9.2.6D for the procedures regulating the SPA area.

## 9.2.3 DEVELOPMENT STANDARDS

The Land Use and Development Standards (Development Standards) were adopted by ordinance as Appendix A to the Specific Plan in 2007. The Amended Appendix A was approved by the Board on December 9, 2014. The Development Standards supplement the *Placer County Zoning Ordinance* and will serve as the zoning regulations governing development, improvements, and construction within the Specific Plan Area. Where a standard is not provided in this document, the standards contained in the *Placer County Zoning Ordinance*, *Placer County Code*, and/or *Land Development Manual* shall apply. The standards in this document supersede, replace, and shall take precedence over conflicting County standards governing the Plan Area.

Refer to Appendix A for the details regarding the Land Use and Development Standards, requesting changes or amendments to permitted uses or development standards, and the enforcement of these development standards.

## IMPLEMENTATION

### 9.2.4 DEVELOPMENT PHASE

A Development Phase (DP) shall be defined as a refinement of the PVSP to provide a detailed proposal to develop a defined sub area of the PVSP. A DP shall identify the proposed phase and the infrastructure identified in the PVSP and PVSP Development Agreement necessary to support the proposed DP including providing all necessary access and utility service, implementation of the PVSP master plans for infrastructure, and additional backbone infrastructure, including secondary roads identified in the PVSP and Development Agreement as required by Placer County. It is anticipated that a DP may include, but is not limited to a geographic area, one or multiple properties, or portions of properties within the PVSP and therefore multiple property owners.

One or more Participating Developer(s), or the Development Group acting on behalf thereof, may submit an application for approval of a Development Phase within a designated portion of land owned by said Participating Developer(s) within the Plan Area. The Development Phase application shall include a Phasing Plan describing the portion of the Backbone Infrastructure together with any other interim or permanent public improvements proposed to be installed to serve the Development Phase (the Phased Improvements) in conjunction with the Backbone Infrastructure previously installed or being installed by other development or by the County through the infrastructure fee program.

The DP application shall describe the area within the Plan Area proposed for development including the total number of residential units and/or square footage of non-residential development proposed to be developed within such Development Phase and/or to be allocated within portions of the Development Phase.

Developer shall submit concurrently with the Development Phase application either an application for a large lot tentative map, which shall include a lotting study or lotting plan for the proposed parcels to indicate how development would affect and be integrated with adjacent developments; or a small lot tentative map for residential parcel(s) within the proposed Development Phase, and/or a subdivision or parcel map and preliminary site plan for development of any nonresidential parcel(s) which shall include a proposed maximum square footage for such development.

A DP shall be processed in accordance with the requirement of the Subsequent Entitlement Process in the Specific Plan and the Implementation Policies and Procedures Manual. Processing of Large Lot Tentative Maps or Small Lot

Tentative Maps for residential parcels and/or preliminary site plans for nonresidential parcels shall generally process concurrently with the processing of the DP. Developer's application for approval of a Small Lot Tentative Map and/or non-residential development entitlements shall not be required to be approved concurrently with the approval of the DP and Phasing Plan in order for the County to approve the DP.

The Phasing Plan required for each DP shall include a list of the Phased Improvements proposed to be constructed by each Developer and/or Development Group to serve the DP and shall include the timing for the commencement and completing of each Phased Improvement in relation to the timing of development of the entitlement proposed to be granted for each Developer whose Property is included with the DP.

Where more than one Participating Developer is involved in a proposed DP, the Phasing Plan may include an allocation to the respective properties of the Phased Improvements required to support development thereof. The County may not require the Participating Developers to construct any additional permanent public improvements or facilities as part of the Phased Improvements that are not otherwise described in the Specific Plan, the Mitigation Monitoring and Reporting Program or the Development Agreement, but may require improvements as part of the Phasing Plan or Phased Improvement as a temporary alternative to any such identified public improvement and/or facilities if required by the Development Group, or as determined by the County to be necessary through the Subsequent Conformity review process in order to adequately serve the Property or reduce impacts outside of the Development Phase whether within or outside of the Plan Area pending installation of the permanent public improvement.

Each application for a Development Phase shall also include, as part of the Phasing Plan, technical studies, as required by the County, to demonstrate that the Phased Improvements to be installed along with the development of the Developer Phase will be adequate to serve the needs of the Development Phase. The County may require the Developer and/or Development Group to construct improvements as part of the Phasing Plan in order to mitigate the impacts associated with such development.

Each application for approval of a Development Phase shall also be accompanied by financing information, acceptable to the County, describing and/or confirming the plan for financing the construction and completion of the remaining Backbone Infrastructure to be constructed after the Phased Improvements that are associated with the proposed DP. This financial information will be used by the County to confirm the ongoing validity of the

Finance Plan to provide a reasonably feasible, economic plan whereby the remaining development planned for the Specific Plan can be anticipated to support the costs to construct and complete the remaining Backbone Improvements.

Each application for approval of a DP and Phasing Plan shall be subject to the review and approval by the County Planning Commission with the right to appeal in accordance with the Implementation Policies and Procedures Manual. Approval of a DP and Phasing plan shall also be subject CEQA compliance to the extent deemed necessary by the County in accordance with the Subsequent Review Process required under the SP. Approval of the DP shall be subject to the determination by the County that the DP presents a logical and reasonable pattern for development within the Plan Area, that the Phasing Plan will adequately mitigate the impacts of development and that it can be served by the Phase Improvements consistent with the SP, the MMRP and related development entitlements and approvals.. Once approved, minor modifications to the Phasing Plan may be approved by the Planning Director. Refer to the Development Agreement for additional details on Development Phase and Phasing Plan.

### 9.2.5 SUBSEQUENT ENTITLEMENT PROCESS

Development within the Plan Area is subject to approval of subsequent entitlements by the County. Examples of such entitlements include Development Phase, tentative maps, Specific Plan amendments, Conditional Use Permits, Minor Use Permits, Tree Permits, and Design/ Site Review applications. Individual project applications will be reviewed to determine consistency with the Specific Plan and other regulatory documents and guidelines.

Application and processing requirements shall be in accordance with the Placer County Zoning Ordinance and other regulations, unless otherwise modified by this Specific Plan. All subsequent development projects, public improvements, and other activities shall be consistent with this Specific Plan and its appendices, the Specific Plan Development Agreement, and applicable County policies, requirements, and standards. In acting to approve a subsequent project or permit, the County may impose reasonable and necessary conditions to ensure that the project is in compliance with the Specific Plan and all applicable plans, ordinances, and regulations.

#### Application Requirements

The individual property owner(s) or Development Group may submit applications for a Development Phase and small lot tentative maps and other

entitlements as necessary. Large lot and small lot tentative maps may also be processed concurrently, but will be approved sequentially. Applications shall be made in writing on forms provided by the County Planning Department and shall be accompanied by required application fees and such data and information as may be prescribed for that purpose.

All of the Participating Developers have formed a development group (the “Placer Vineyards Development Group”) to finance the costs of preparing and finalizing the Master Plans and establishing the financing mechanisms described in Section 9.2.1 above and to bear the costs to design and install the backbone infrastructure and parks described in the Development Agreement. The Development Agreement requires that, as a condition for scheduling a hearing to approve a Development Phase, a small lot final subdivision map for recordation for single family development, or to obtain a building permit for multifamily development, or to obtain signed improvement plans for non-residential development, the applicant shall be required to deliver to the County a Good Standing Certificate from the Development Group certifying that the applicant is a member in good standing with the Development Group. The Development Agreement also requires an applicant to thereafter remain a member in good standing with the Development Group to obtain building permits for its development.

#### Application Processing

Applications will be analyzed by County staff members for consistency with the Specific Plan. Plan consistency must be demonstrated in several ways:

- ◆ Consistency with policy language (goals, policies, objectives, etc.);
- ◆ Consistency with land use designations, roadways, and bike paths;
- ◆ Consistency with development standards and design guidelines;
- ◆ Consistency with figures and tables; and
- ◆ Consistency with an applicable, approved Development Phase; and
- ◆ Consistency with the property Development Agreement, large-lot tentative map, small-lot tentative map, and other relevant implementation documents.

Consistency is also required with the Specific Plan Mitigation Monitoring and Reporting Program, infrastructure and County facilities plans, finance and public service plans, and other implementing documents of the Specific Plan.

## IMPLEMENTATION

### Implementation Policies and Procedures Manual

To assist the County in implementing and performing its various administrative tasks as contemplated by the entitlement process and Development Agreement, an Implementation Policies and Procedures Manual (IPPM) was prepared by the Participating Developers in association with the County Executive Officer and was approved by the County Board of Supervisors in 2013. The manual provides for a comprehensive approach for processing approvals and issuing of permits for development within the Plan Area, including but not limited to Development Phase processing, developing forms and checklists to assist the County staff in tracking and accounting for credits, and reimbursements, processing approvals consistent with the procedures set forth in the Development Agreement and the Specific Plan, and confirming issuance of Good Standing Certificates from the Development Group and payment of any required PVSP Shortfall Payments. The County may periodically update the IPPM.

#### 9.2.6 ENVIRONMENTAL REVIEW

All applications for a development entitlement that are submitted after approval of the Specific Plan shall be reviewed for conformity with the Specific Plan and for compliance with the California Environmental Quality Act (CEQA), Public Resources Code Section 21000 et seq. An environmental impact report (EIR) was certified concurrent with the approval of the original Specific Plan in 2007 and the certified EIR, and its adopted addenda, shall serve as the base environmental document for subsequent entitlement approvals within the Plan Area. The determination of whether the requested subsequent development entitlement is consistent with the Specific Plan and whether the Specific Plan EIR considered the project-specific effects will be made through the Subsequent Conformity Review process as described in the next section.

#### Subsequent Conformity Review Process

In conjunction with submitting any required County application for approval of a subsequent development entitlement within the Plan Area, the applicant for each proposed project shall complete a Subsequent Conformity Review questionnaire. The purpose of the questionnaire is to enable the County to determine whether the proposed project is consistent with the Specific Plan, to examine whether there are project-specific effects that are particular to the project or its site that were not considered in the Specific Plan EIR, and/or whether an event as described in Section 15162 of the State CEQA Guidelines has occurred. The County may require additional information to make such a determination, including, but not limited to, the following:

- A. Preliminary Grading Plan (including off-site improvements);
- B. Preliminary Geotechnical Report;
- C. Preliminary Drainage Report;
- D. Preliminary Water Quality Best Management Practices (BMP) Plan;
- E. Traffic Circulation Plan;
- F. Traffic Study;
- G. Tentative Map (Section 16.12.040 of the Placer County Code);
- H. Acoustical Analysis (and associated Traffic and Circulation Studies);
- I. Hazards/Past Use Studies (Phase I Environmental Site Assessments and Phase II limited soils investigation, and/or Preliminary Endangerment Assessment with California Department of Toxic Substances Control, as determined by County Environmental Health Services);
- J. Mosquito Control Design Features (for waterways, underground water detention structures, water facilities, etc.);
- K. Water Quality Related Studies/Details (BMPs, Preliminary Grading Plan, Preliminary Drainage Plan);
- L. Utility Will-Serve Requirement Letters (water, sewer, solid waste, reclaimed water, etc.);
- M. Senate Bill (SB) 221 Water Supply Assessment Information;
- N. Hazardous Materials Usage Information;
- O. Water Supply Well Information (as applicable);
- P. Biological and Cultural Resources Study; and
- Q. Public Safety Assessment.

Based on the information provided, the County will determine whether the proposed development entitlement is consistent with the Specific Plan and related documents, whether additional environmental review is required, and if so, the scope of such additional review. Based on the content of future submittals and the process described above, the County will determine whether performance standards are still achievable given the nature of the submittal, current conditions, and/or changed circumstances. In the event that the performance standards do not appear to be achievable, the County may perform subsequent environmental review pursuant to Sections 15162, 15163, or 15164 of the State CEQA Guidelines to identify additional or alternative mitigation measures.

## Surveys for Native American Cultural Sites

As a part of the Subsequent Conformity Review process, or prior to any other activity that may result in ground disturbance, the applicant must begin a process assessing whether or not the site contains any Native American Cultural Places. In order to allow the applicant, the County, and any potentially affected Native American tribe effective input on potential impacts to such a site, this process should be started during the Subsequent Conformity Review Process. The process and procedures must be completed prior to the issuance of a grading permit or any other activity that results in ground disturbance.

### **Policy 9.1 Native American Cultural Places.**

*All properties in the Plan Area shall be inspected by a qualified archaeologist or other cultural resource specialist familiar with Native American Cultural Places. The applicant shall retain such an archaeologist or specialist to determine whether some or all of the affected property qualifies as a Native American Cultural Place and if such a site is identified on the property complete the process described below.*

1. *For purposes of this policy, a Native American Cultural Place is any of the following:*
  - ◆ *A Native American sanctified cemetery, place of worship, religious or ceremonial site, or sacred shrine;*
  - ◆ *A Native American historic, cultural, or sacred site that is listed or may be eligible for listing in the California Register of Historic Resources; or*
  - ◆ *A site that includes Native American historic or prehistoric ruins, a Native American burial ground, or Native American inscriptions or rock art.*
2. *If the archaeologist determines that some or all of the affected property qualifies as a Native American Cultural Place, he or she shall recommend to the County potentially feasible mitigation measures that would preserve the integrity of the site or minimize impacts to it, including any or a combination of the following:*
  - ◆ *Avoidance, preservation, and/or enhancement of all or a portion of the Native American Cultural Place as open space or habitat, with a conservation easement dedicated to the most interested and appropriate tribal organization, if such an organization is willing to accept and maintain such an easement, or alternatively, a cultural resource organization that holds conservation easements;*

- ◆ *An agreement with any such tribal or cultural resource organization to maintain the confidentiality of the location of the site so as to minimize the danger of vandalism to the site or other damage to its integrity; or*
  - ◆ *Other measures, short of full or partial avoidance or preservation, intended to minimize impacts to the Native American Cultural Place consistent with Specific Plan policies, land use assumptions, and the proposed design and footprint of the development project for which the requested grading permit has been approved.*
3. *After receiving such recommendations, the County Planning Director shall assess the feasibility of the recommendations and impose the most protective mitigation feasible in light of Specific Plan policies, land use assumptions, and the proposed design and footprint of the development project for which the requested grading permit has been proposed. In reaching his or her conclusions with respect to these recommendations, the Planning Director shall consult with both the project applicant and the most interested and appropriate tribal organization.*
  4. *The process described in this policy must be completed prior to the issuance of a grading permit or any other activity that results in ground disturbance.*

## Mitigation Monitoring

CEQA requires all state and local agencies to establish reporting and monitoring programs for projects approved by a public agency whenever the approval involves adoption of either a “mitigated negative declaration” or specified environmental findings related to EIR’s. The Mitigation Monitoring and Reporting Program is intended to satisfy the requirements of CEQA as they relate to the FEIR for the Placer Vineyards Specific Plan. This monitoring program is to be used by County staff and the project developers to ensure compliance with adopted mitigation measures during project implementation. Monitoring and documentation of the implementation of mitigation measures will be coordinated by County staff according to Article 18.24 of the Placer County Environmental Review Ordinance. The Mitigation Monitoring and Reporting Program is provided, under separate cover, as Appendix D to this Specific Plan.

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### 9.2.7 INTERPRETATIONS, AMENDMENTS, AND ENTITLEMENTS FOR NON-PARTICIPATING PROPERTY OWNERS AND THE SPA

Implementation of the Specific Plan is anticipated to occur over several years. During the long-term build-out of the Plan Area, property owners will face dynamic and evolving market conditions. Situations may arise where amendments to the adopted Specific Plan are necessary because of changing circumstances. Additionally, because of unforeseen circumstances, some design guidelines or development standards may not be feasible on a parcel-by-parcel basis. An alternative design solution that meets the goals and objectives of the Specific Plan may also be developed.

#### A. Interpretations

Interpretations are judgments that apply the vision, goals, and intent of the Specific Plan to specific issues and situations related to the land use decisions and development. Interpretations are generally limited to details where the requirements and guidelines of this Specific Plan may appear to provide alternative guidance, differ from each other, or from other adopted County policies or requirements from other agencies. Interpretations may be needed when the County is considering discretionary development applications (such as a subdivision map), or a ministerial application (such as a building permit). Interpretations for the Specific Plan shall be made as described in Section 17.02.050 of the *Placer County Zoning Ordinance*. In making interpretations, the Planning Director will consult with any other affected County department or other agency as needed.

#### B. Administrative Modifications and Amendments

Amendments to the adopted Specific Plan shall be categorized by the County Planning Director as either an Administrative Modification or an Amendment.

Administrative Modifications do not have a significant impact on the character of the Plan Area. Administrative Modifications are consistent with the spirit and intent of the vision, goals, and policies of the Specific Plan.

Amendments to the Specific Plan are changes to the plan elements, including differences in land use development types assigned to specific parcels; changes to capacity requirements; changes to the intensity or density of land uses on specific parcels (including public facilities); density transfers; changes in the Plan Area boundaries; or changes in policies. Amendments usually involve issues of consistency with the original vision and intent of the Specific Plan or with the *Placer County General Plan*.

Proposed amendments to the Specific Plan can include, but are not limited to, changing land use designations, design criteria, detailed road alignments, road classifications, or Specific Plan policies. Typically, amendments to the Specific Plan will be requested by property owners. There may also be circumstances, however, where the County may wish to request an amendment. The procedures described below shall be followed to amend the adopted Specific Plan.

#### Administrative Modification and Amendment Requests

All requests to amend the Specific Plan shall be made using the appropriate application forms, required documentation, and applicable fees as established by the County Planning Department. Any or all of the following information may be required:

1. A detailed justification statement that explains in detail why an amendment to the Specific Plan is warranted, and any exhibits deemed necessary by the Planning Director;
2. A statement of consistency with *Placer County General Plan* policies and Specific Plan land use designations;
3. A statement of consistency with the Specific Plan;
4. Analysis as required by CEQA; and
5. Identification of any required modifications of the public infrastructure improvements that are not currently mandated by the Specific Plan (description, location, timing, funding source, method, etc).

#### Administrative Modifications

Administrative Modifications do not have a significant impact on the character of the Plan Area. Administrative Modifications are consistent with the spirit and intent of the vision, goals, and policies of the Specific Plan. Administrative Modifications shall conform to the following criteria:

- ◆ The Planning Director determines that the proposed adjustments to the design guidelines are offset by the merits of the proposed design and do not significantly change the anticipated physical characteristics, goals, and intent of the Specific Plan;
- ◆ Proposed changes to the alignment of arterial and local streets, if adopted, would not substantially alter the land use or circulation concepts set forth in the Specific Plan;

- ◆ Proposed changes to land use diagram shapes or to the alignment of collector and secondary streets maintain the general land use pattern and/or provide an improved circulation system consistent with the intent and direction of the vision, goals, and policies of the Specific Plan;
- ◆ The proposed change is not expected to increase environmental impacts beyond the levels identified in the EIR;
- ◆ The proposed change will not result in an increase in the total maximum number of units proposed in the Specific Plan and will comply with the criteria for modification of the land use diagram described in Policy 9-2; and
- ◆ The proposed change will not significantly reduce the number of acres designated for high density residential.

An Administrative Modification may be reviewed and acted upon by the County Planning Director. No Planning Commission or Board of Supervisors review is required, unless the Administrative Modification is appealed.

**Policy 9.2 Modification of the Land Use Diagram.**

*Figure 3.1, “Land Use Diagram,” is intended to provide the structural framework for the land use program within Placer Vineyards. Land use boundaries may be modified or reconfigured on site plans and designs through the Administrative Modification process, in conjunction with the Subsequent Conformity Review and Design Review process, without an amendment to the Specific Plan, so long as the modifications comply with the following criteria:*

- ◆ *The Planning Director has determined that the proposed residential project is consistent with the approved Specific Plan and with land use patterns and assumption analyzed in the Specific Plan EIR;*
- ◆ *The density of development does not exceed the permitted range of the land use designation and the allowable number of units is not exceeded by each property within the Plan Area;*

*The areas planned for parks, open space, County facilities and other such public amenities described on the Land Use Diagram is not reduced; and no small lot final map has been recorded.*

**Amendments**

An Amendment is any change proposed to the Specific Plan that could significantly increase environmental impacts or other changes determined to be significant by the Planning Director. An Amendment is the appropriate procedure where changes to the Specific Plan are proposed that meet one or more of the following criteria:

- ◆ A new category of land use not specifically discussed in the Specific Plan is introduced;
- ◆ Significant changes to the distribution of land uses beyond those allowed under an Administrative Modification or other changes affecting land use are proposed and may substantially affect the Specific Plan;
- ◆ Proposed changes to the design guidelines and/or development standards that if adopted, would substantially change the physical character of the Plan Area as envisioned by the Specific Plan and the Planning Director; or
- ◆ Proposed changes to the development sequencing that would significantly increase or alter Plan Area boundaries or units allocated.

Amendments require approval by the County Planning Commission and Board of Supervisors. They shall comply with the procedures outlined in Section 17.60.090 of the Placer County Zoning Ordinance.

**C. Entitlements for Non-Participating Property Owners in the Plan Area**

Participating Developers as described above have entered into a Development Agreement with the County. The County will encourage those landowners, exclusive of those in the SPA, not participating in the development of the urban area of the Specific Plan (the “Non-Participating Property Owners”) to enter into a similar Development Agreement relating to their respective properties.

The Placer Vineyards Specific Plan and its EIR place substantial requirements on the development of the properties within the project area. These requirements include financing, construction and maintenance of public improvements and facilities, design standards, and mitigation of environmental impacts. It is appropriate that each property within the Plan Area to be developed for urban uses, as planned in this Specific Plan, bear its fair share of the costs to prepare and implement this Specific Plan. When processing an application to rezone a Non-Participating Property Owner’s property to

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SPL-PVSP zoning described in Section 9.2.2 above and/or to enter into an agreement to bear its fair share of such costs, which agreement may be in the form of a development agreement, the following issues will be considered as part of such rezoning and/or fair share agreement:

- ◆ Augmenting the County’s standard development regulations in response to the particular characteristics of each individual project;
- ◆ Defining the precise financial responsibilities of the Non-Participating Property Owner, including its obligation to participate in and/or annex into the financing mechanisms described in Section 9.2.1;
- ◆ Determining the non-participating property owners’ fair share of the costs advanced by the Participating Developers to plan and implement this Specific Plan, including the costs to prepare the Specific Plan and related Master Plan documents and to form the financing mechanisms described in Section 9.2.1 above;
- ◆ Determining the property’s fair share for lands dedicated by the Participating Developers for parks and public facilities and for costs advanced by the Participating Developers to acquire additional rights of way and easements, obtain permits, and provide environmental mitigation required to develop the Plan Area;
- ◆ Ensure timely provision of adequate public facilities for each project
- ◆ Streamlining the development approval process by coordinating various discretionary approvals;
- ◆ Providing the terms for reimbursement to the Participating Developers for the property’s fair share of costs advanced by the Participating Developers and for reimbursement to the Non-Participating Property Owner, if and when the Non-Participating Property Owner advances funding for planning, financing, land dedications, permitting, environmental mitigation, designing and constructing specific facilities required for the development of the Specific Plan Area that benefit the properties of both the Non-Participating Property Owner and the Participating Developers;
- ◆ Provide additional terms for both the County and the Non-Participating Property Owner regarding entitlements to the developer in return for commitments for public improvements

### ***Policy 9.3 Property 5C Land Use Exchange.***

*A middle school site and adjacent 6 acre park was originally planned to be included within the northwest corner of Property 5C. (owned by a Non-Participating Property Owner), but was subsequently moved west to Properties 7 and 10 as shown on the Land Use Diagram. Also, as a part of this land use exchange, the religious site planned on Property 7 and the religious site planned on Property 10 were relocated to Property 5C and combined into 1 larger religious site. If a complete application for entitlements for Property 5C is submitted to the County within 1 year of final approval of the Specific Plan, the middle school site on Property 7 and the adjacent park site on Property 10, as shown on the Land Use Diagram, shall be relocated to Property 5C and the religious site shall be relocated back to Properties 7 and 10 as part of the County’s approval of entitlements for Property 5C. Any such relocation shall be treated as an Administrative Modification, not as an amendment, to the Specific Plan, and shall not require the consent of the owners of Properties 7 and 10.*

*However, with respect to any application for entitlements for Property 5C submitted more than 1 year after approval of the Specific Plan, any proposed relocation of the middle school site and adjacent park site from Properties 7 and 10 to Property 5C, or relocation of the religious site(s) from Property 5C to Properties 7 and 10 shall be treated as an amendment to the Specific Plan, and shall require the consent of the owners of Properties 7 and 10 and an amendment of the Development Agreement. Any such relocation of the middle school and adjacent park sites from Properties 7 and 10 to Property 5C shall not change the number of residential units permitted for such properties.*

## D. Entitlements in the Special Planning area

This Specific Plan does not revise the existing zoning for the SPA properties. Within this area, existing County zoning and administrative processes govern. Entitlements requested in the SPA shall be governed by the following:

1. Approximately 150 units have been permitted and exist in the SPA.
2. Approximately 63 additional units may be developed within the SPA consistent with current zoning.
3. An additional 198 units (for a total of 411 units) may be developed within the SPA under the Placer Vineyards Specific Plan and the Specific Plan EIR on a first-come, first-served basis. Property owners utilizing any of the 198 additional units will be required to connect to Plan Area infrastructure systems. Property owners who choose to subdivide their properties beyond that allowed under current zoning in the SPA will be required to submit an Amendment application and perform additional environmental review.
4. Any property owner who proposes to develop beyond the total 411 units allowed in the SPA shall be required to submit applications for appropriate amendments to the Placer County General Plan, Specific Plan, and Zoning Ordinance and shall be subject to additional environmental review.

Plan Area public infrastructure systems (e.g., water, wastewater, drainage) shall be sized to accommodate the SPA (411 total units). Property owners in the SPA may hook up to public infrastructure systems but shall be responsible for any fees for the extension of services to their property, any hook-up fees, and any Plan Area or Special District fees that apply to the Plan Area. Developers of properties west of Dyer Lane which abut the eastern boundary of the SPA Area shall be required to stub water and sewer mains of a size adequate to serve the SPA area to their western project boundary and shall provide any necessary easements to accommodate this infrastructure. The specifics as to the number, location, and timing of such extensions shall be established at such time as subdivision tentative maps are approved for these properties.

## 9.2.8 TRANSFER OF DENSITY

### Density Transfer for Housing Units

To further the intent of providing development flexibility, units assigned to specific properties may be transferred between development land use parcels provided that all the following criteria are met:

- ◆ The sending and receiving parcels are within the Placer Vineyards Specific Plan Area and the total maximum number of approved units for the entire Plan Area equals 14,132;
- ◆ The density transfer does not increase or decrease the number of units allocated to any one receiving parcel (or combination of receiving parcels) by more than 20%;
- ◆ The sending and receiving parcels have an existing land use designation of Low Density Residential (LDR), Medium Density Residential (MDR), or High Density Residential (HDR) and the density adjustments would not result in densities above or below those for the existing land use designation;
- ◆ The adjustment is made between parcels before a small lot tentative map is approved by Placer County for either the receiving or sending parcel;
- ◆ Commercial/Mixed-Use (C/MU) units may not be transferred;
- ◆ Units may be transferred into, but not out of, Commercial/Mixed-Use (C/MU) designated property;
- ◆ Units may not be transferred to the Special Planning Area;
- ◆ The transfer of units would not result in increased impacts beyond those identified in the Placer Vineyards EIR;
- ◆ The adjustments in density and units would not significantly affect planned infrastructure, roadways, schools, other public facilities, or Plan Area financing districts;
- ◆ The transfer of units would not result in a reduction in the total number of HDR units of the sending plus the receiving property (For example, if the total of the sending parcel's HDR units plus the receiving parcel's HDR units equals 100 before the transfer, the total must also equal 100 after the transfer); and
- ◆ The County Planning department shall track sending and receiving parcels.

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The transfer may occur between different legal owners. To request a density adjustment, the owner or owners of both the sending and receiving parcels shall submit to the County Planning Director all information needed to determine compliance with the above unit-transfer criteria. This submittal shall include information identifying the affected parcels and designating the number of units being transferred, an Administrative Modification application with filing fee, and any other necessary documentation requested by the Planning Director. The applicant shall also provide a revised Specific Plan table reflecting the adjusted unit counts and densities, as well as any necessary maps. The revised table will allow the Planning Department to track unit allocations.

Density transfers that fulfill the above criteria and are consistent with the intent of the Specific Plan and EIR will be processed as an Administrative Modification (see 9.2.6B, above). If a request for density adjustments does not comply with the above criteria, the applicant must request an amendment to the Specific Plan.

If a density transfer is requested after a small lot tentative map has been approved for either the sending or receiving parcel a revised tentative map for both parcels shall be submitted to the Planning Commission for review and approval as revised tentative maps. This request shall follow the County's normal process for such approval including payment of normal processing fees.

### 9.2.9 DESIGN/SITE REVIEW

The purpose of the County's Design/Site Review process is to ensure that the design of buildings constructed in the Plan area is of high quality and to prevent new construction from adversely affecting the desirability of the immediate and nearby areas for residents and businesses. All development within the Plan Area will be subject to Design/Site Review in accordance with the procedures outlined in Section 17.52.070 of the *Placer County Zoning Ordinance*. Through the Design/Site Review process, applications are approved, conditionally approved, or denied, based on consistency with the design standards and guidelines established for each district and the Plan Area.

## 9.3 TIMING OF DEVELOPMENT INFRASTRUCTURE AND PUBLIC SERVICES

The Placer Vineyards Plan Area is anticipated to be built-out over 20 to 30 years and may evolve in a variety of ways depending on several factors. These factors include shifts in market demand for various housing types and non-residential uses and changes in the development goals or financial capabilities of property owners.

The sequence of development for Placer Vineyards is to invest and construct Backbone Infrastructure and public facilities in phases that will allow all the major project developments in the Plan Area to proceed in a logical fashion. Thereafter, additional infrastructure and public facilities are to be available when developments are approved and constructed. Chapter VII, "Public Utilities and Services" of the Specific Plan describes the infrastructure and public service facilities required to serve the build-out of the Plan Area. The Placer Vineyards Public Financing Plan defines the construction of the Backbone Infrastructure system and installation of County service facilities to support the development of all major projects within the Plan Area. As development occurs, infrastructure and services will be extended and expanded, as required, to serve new development. The Public Facilities Financing Plan, Urban Services Plan and the Development Agreement provide the detailed analysis regarding the timing and costs for planned infrastructure and public facilities and the ongoing provision of public services to support development of the Plan Area as such development proceeds.

As described in the Development Agreement and the Implementation Policies and Procedures Manual, as each Development Phase is processed for approval by the County, the specific detailed components and timing of the improvements and facilities required to serve the Development Phase shall be determined by the County, consistent with the Development Master Plans and the Development Agreement to confirm both the adequacy to serve a Development Phase as well as the manner in which such improvements and facilities will be coordinated with subsequent Development Phases to serve the entire Specific Plan.

A Development Phase shall include a Phasing Plan describing the portions of the Backbone Infrastructure together with any other interim or permanent public improvements proposed, or as determined by the County, to be installed to serve such Development Phase (the Phased Improvements). The Phasing Plan shall include the timing for the commencement and completion of each Phased Improvement. In addition, the Phasing Plan shall include proposed roadway improvement costs and an analysis of Development Phase Dwelling Unit Equivalents to determine adequate fair share percentages. As part of its review and approval of a Phasing Plan, the County may require the update and/or amendment of the Development Master Plans.

Prior to approval of improvement plans for any Phased Improvement to be constructed as part of the first Development Phase approved by the County, the Developer shall fund a study to be undertaken by the County to identify and review the feasibility of alternatives to retaining Locust Road as a through roadway between Baseline Road and West Town Center Drive (the “Locust Road Circulation Study”).

In addition to Backbone Infrastructure, development of a property shall, as determined by the County, be subject to the completion of Secondary Road Improvements. Developer shall also be obligated to design and construct all other road improvements within or adjacent to the property including but not limited to curb, gutter, sidewalks, utilities, landscaping, streetlights, pavement, aggregate base and aggregate sub-base, underground water, sewer and drainage improvements wholly within the Property and to the centerline of the road rights-of-way adjacent to the Property or as needed to serve the property.

Sidewalks, trails and landscaping to be installed adjacent to single-family subdivisions with the Plan Area shall be installed concurrently with the subdivision improvements for each single-family residential lot subdivision.

### 9.3.1 INFRASTRUCTURE AND PUBLIC FACILITIES SYSTEMS

The Financing Plan and the Development Agreement define more precisely the following backbone infrastructure and public facilities to support development of this Specific Plan.

#### Backbone Infrastructure

This term describes certain, essential public service-based items that are underground or on the surface. These items include roads, water, sewer, drainage, recycled water, erosion control, and dry utilities. Backbone infrastructure is sized to serve numerous individual development projects in the Specific Plan and, in some cases, to serve the broader region’s development areas.

As described previously, development of the property, or a portion of the property, will require the submittal and approval of a Development Phase and Phasing Plan that will describe the portions of the backbone infrastructure proposed to be installed to serve a particular Development Phase in conjunction with the Backbone Infrastructure previously installed or being installed by other developments or by the County through the infrastructure fee program. The Plan Area Backbone Infrastructure will consist of the following improvements:

#### Roadway Improvements

The backbone roadway improvements will consist of the widening and reconstruction of a number of existing roads within and adjacent to the Plan Area as well as the construction of new roads.

#### *Base Line Road*

- ◆ Widen/reconstruct existing road to six-lane thoroughfare, including applicable intersection widening with curb and gutter on both sides of the street and median landscaping between Pleasant Grove Road (East) to Walerga Road. Construct sidewalk on south side of street between Newton Street and Walerga Road.
- ◆ Modify and/or Construct traffic signals at the following intersections:
  - Walerga Road
  - 9<sup>th</sup> Street
  - East Dyer Lane
  - Watt Avenue
  - 11th Street
  - 12th Street
  - 14th Street
  - 16th Street

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- Palladay Road
- West Dyer Lane
- Locust Road
- Pleasant Grove Road (West)
- Pleasant Grove Road (East)
- Natomas Road

### ***Watt Avenue***

- ◆ Widen/reconstruct existing road to a six-lane thoroughfare section, including applicable intersection widening with curb and gutter on both sides of street and landscaped median from Placer County/Sacramento County line to Base Line Road. Remove existing bridge and construct new bridge over Dry Creek. Construct sidewalk/trail on west side of street between West Dyer and Base Line Road only.
- ◆ Widen road to a six-lane thoroughfare section, including applicable intersection widening with curb, gutter and sidewalk on both sides of the street from Tolman Lane in Sacramento County to the Placer County/Sacramento County line.
- ◆ Construct traffic signals at the following intersections:
  - ‘A’ Street
  - East Town Center Drive
  - Oak Street Intersection
  - West Dyer Lane
  - PFE Road

### ***West Dyer Lane***

- ◆ Construct four-lane arterial road section including applicable intersection widening with curb, gutter and sidewalk on both sides of the street.
- ◆ Construct traffic signals at the following intersections:
  - ‘A’ Street
  - West Town Center Drive
  - 18th Street
  - West Town Center Drive
  - 1<sup>8th</sup> Street
  - Palladay Road
  - 16th Street
  - Tanwood Avenue
  - 11<sup>th</sup> Street

### ***East Dyer Lane***

- ◆ Construct four-lane arterial road section with curb and gutter on both sides of street, sidewalk on west side of the street only and median landscaping.
- ◆ Construct traffic signals at intersections of ‘A’ Street and East Town Center Drive

### ***16th Street***

Construct four-lane arterial road section, including curb and gutter on both sides of the street, median landscaping, and sidewalk on the east side of the street only from West Dyer Lane to Base Line Road. Construct the same section from West Dyer Lane to the Placer County/Sacramento County line with sidewalk on the west side of the street only. Construct a traffic signal at the intersection of ‘A’ Street.

### ***18<sup>th</sup> Street***

- ◆ Construct a two-lane collector street with curb, gutter and sidewalk on both sides of the street from Locust Road to West Dyer Lane.

### ***Palladay Road***

- ◆ Construct a four-lane arterial road section with curb, gutter and sidewalk on both sides of the street, and median landscaping from West Dyer Lane to the Placer County/Sacramento County line.
- ◆ Construct two 12-foot lanes with 2-foot AC shoulders – Fire Station Access – Palladay Road / ‘A’ Street
- ◆ Construct a traffic signal at the intersection of ‘A’ Street (fire station access).

### ***Locust Road***

- ◆ Construct a four-lane arterial road section with curb, gutter and sidewalk on both sides of the street and median landscaping from 18<sup>th</sup> Street to the Placer County/Sacramento County line.

### ***14<sup>th</sup> Street***

- ◆ Construct a traffic signal at the intersection of ‘A’ Street.

### ***12<sup>th</sup> Street***

- ◆ Construct a traffic signal at the intersection of ‘A’ Street.

### ***Walerga Road***

- ◆ Construct a traffic signal at the intersection of West Town Center Drive.

**Major On-Site Improvements**

- ◆ Construct Advance Traffic Signal Operation System.

**Secondary Road Improvements**

In addition to constructing backbone infrastructure, if and when required by each Phasing Plan for a Development Phase, and as determined by the County, the completion of other roadway improvements not included within the list of Backbone Infrastructure may be required. Secondary road improvements that may be required include Palladay Road, Town Center Drive, Locust Road, 14<sup>th</sup> Street and/or Tanwood Avenue.

**Wastewater Improvements**

There are two alternative methods for collecting and treating wastewater flows from the Plan Area. The preferred alternative is to collect wastewater flows and deliver them for treatment to the Dry Creek Wastewater Treatment Plant (DCWWTP) in the City of Roseville. In this alternative, the backbone wastewater improvements will consist of the following:

- ◆ The portion of the Plan Area west of Watt Avenue will be served by a gravity sewer pipe trunk system which will collect and deliver wastewater flows from east to west to a lift station to be located near the westerly edge of the Plan Area. Wastewater will be pumped from this lift station through a sewer force main to the DCWWTP. Within the Plan Area the force main will be installed in roadway corridors. From the Watt Avenue/PFE Road intersection the force main will travel easterly along an off-site route to the DCWWTP.

The second wastewater alternative is to collect the flows and deliver them to a trunk sewer owned and maintained by the Sacramento Regional Sanitation District (SRCSD) for transport to the SRCSD treatment facility. If the secondary wastewater alternative is implemented, the backbone wastewater improvements will consist of the following:

- ◆ A gravity sewer pipe trunk system will collect and deliver Plan Area wastewater flows to an SRCSD sewer trunk pipeline in Elkhorn Boulevard. The on-site gravity pipe system installed under this scenario would generally be the same as the system installed for the preferred alternative; however, instead of terminating at a lift station near the westerly edge of the Plan Area, the on-site gravity trunk pipeline would continue southerly into, and through Sacramento County, eventually connecting into the SRCSD Upper Northwest Interceptor in Elkhorn Boulevard. The Upper Northwest Interceptor would then carry the Plan Area wastewater flows to the SRCSD treatment plants.

**Water Improvements**

The backbone water improvements consist of a pipe network within the backbone infrastructure and five water storage tanks spaced throughout the Plan Area. The storage tanks will connect to the pipe network and be located on sites adjacent to the backbone roads.

**Recycled Water Improvements**

Recycled water, generated from wastewater effluent treated at the DCWWTP, will be used for landscape irrigation purposes throughout the Plan Area. The Backbone recycled water infrastructure will consist of an off-site pipe connection to an existing recycled water supply pipeline, installation of an on-site pipe network located primarily adjacent to backbone roadways, and construction of a recycled water storage tank connected to the on-site pipe network.

**Drainage Improvements**

The Placer Vineyards drainage system is designed to discharge into channels and detention facilities, providing detention and retention of increases in runoff volume. Flood control with the Plan Area will consist of newly constructed channel systems and parallel flood control channels. Storm drain collection improvements will consist of a gravity trunk storm drain collection system including drop inlets, manholes, cross culverts, inlet structures, outlet structures, water quality facilities and appurtenances.

**Dry Utility Improvements**

Another component of the Backbone Infrastructure consists of “dry” utility improvements to provide natural gas, electric, telephone, cable television, and street lighting service to the properties in the Plan Area. Dry utility improvements will be installed in public utility easements adjacent to the Backbone roadways within the Plan Area. The improvements will include the relocation and conversion of existing overhead facilities, where required, and the installation of electric and telephone conduits, substructures, cabling, transformers and switches, natural gas pipes, a streetlight conduit system and streetlights, and a broadband conduit system within public utility easements adjacent to Backbone roads.

**Miscellaneous Improvements****All Areas of New Construction**

- ◆ Erosion control features including straw wattles, gravel bag inlet protection and hydroseeding.

**Open Space Corridors**

- ◆ Class I bike paths and multi-purpose trails.

## IMPLEMENTATION

### Public Facilities

Public Facilities are the amenities to the Specific Plan (park facilities and libraries) or facilities that house employees providing services to the area (sheriff, fire, public administration). The County, the Park District and/or applicable school districts shall be solely responsible for the planning, design, construction and equipping of the following public facilities.

Public facilities include the following:

- ◆ Schools;
- ◆ Public buildings, including the Government Center, Libraries, Fire Stations, Sheriff Substation, and their corresponding equipment;
- ◆ Corporation Yard; and
- ◆ Community Parks.

### 9.3.2 URBAN SERVICES

Urban Services, described in more detail in the Urban Services Plan, are the new and/or enhanced services to be provided by the County and/or the Park District within the Plan Area, and include:

- ◆ Sheriff services;
- ◆ Fire protection and suppression services, including ambulance and paramedic services;
- ◆ Recreation program services;
- ◆ Maintenance of parks, landscaping, and open space, including all off-site open space and habitat preservation lands;
- ◆ Maintenance of storm drainage systems within any County maintained right-of way;
- ◆ Maintenance of roads, traffic signals, and streetlights;
- ◆ Any other service provided by the County to the property that may be allowed by law to be funded through a Community Facilities District;
- ◆ Public transit services

### 9.3.3 GENERAL TIMING/DEVELOPMENT POLICIES

#### ***Policy 9.4 Backbone Infrastructure***

*Development and the required infrastructure and public facilities shall be timed to be available for new development in the Plan Area as the development proceeds. Construction of the Backbone Infrastructure system consistent with provisions in the Development Agreement will provide access to the infrastructure and services*

*required to develop a DP. Any property owner in the Plan Area may develop, provided that they build and fund the necessary infrastructure and public facilities as set forth in the Development Agreement, the Public Facilities Financing Plan, and the Urban Services Plan.*

#### ***Policy 9.5 Landscape Master Plan***

*A substantially complete Landscape Master Plan was submitted to the County for review and approval in concept prior to the submittal of the first Development Phase and Phasing Plan. The Board of Supervisors shall approve the Landscape Master Plan prior to the approval of the first Development Phase and Phasing Plan.*

#### ***Policy 9.6 Approval of Urban Services Financing Mechanisms.***

*The Urban Services Plan, as described in Sections 9.3.2 and 9.4.3 of this Specific Plan, shall be used to decide financing through a Community Facilities District (Services CFD) or County Service Area (CSA). In preparing and establishing the Urban Services Financing Mechanisms, cost estimates developed during the preparation of this Specific Plan and the Urban Services Plan shall be updated and final project taxes for the Services CFD and assessments for the CSA shall be defined. Any necessary Services CFD or CSA shall be formed as specified in the Development Agreement.*

## 9.4 SPECIFIC PLAN FINANCING STRATEGY

### 9.4.1 OVERVIEW OF FINANCING STRATEGY

The major infrastructure required for development to proceed in the Plan Area will be funded primarily with private financing. Municipal debt financing mechanisms for infrastructure may be considered only as capacity exists after services financing is established. Fees include County, Special District, New Development Mitigation fees, Project Development Fees and PVSP Fees. Placer County and special districts serving the Plan Area have developed impact fee programs to fund a portion of the road, sewer, water, sheriff's, and park facilities.

The PVSP fee program, imposed through the Development Agreement, will be used to fund the Backbone Infrastructure costs and other public facilities serving the Plan Area. Also, new Plan Area fees will be imposed through the Development Agreement to fund public facilities serving the entire Southwest Placer County area.

Facilities will be constructed to serve new development, consistent with the Public Facilities Financing Plan and Development Agreement during the development and subdivision process. Developers may receive either fee credits or reimbursements for advancing eligible projects based on the reimbursement policies described in the Development Agreements.

School facilities are planned to be funded through school mitigation fees and other funding sources, including the State School Building Program and local general obligation bonds.

It is expected that costs will change over time; therefore, the Development Agreements defines methods for the adjustment of the amount of funding to reflect current costs at the time of construction.

The Public Facilities Financing Plan describes the financing strategy to fund major Backbone Infrastructure and Public Facilities needed to serve new development in the Placer Vineyard Specific Plan. The Public Facilities Financing Plan strategy includes:

- ◆ Specifying the major public facilities to be constructed, equipped and furnished by the County through the facilities fee program in association with the development of the Plan Area including corresponding costs based on available engineering data, existing County department data, and other estimates;
- ◆ Identifying funding sources to pay for the Backbone Infrastructure and public facilities, including any existing and potential future fee programs

or financing districts;

- ◆ Providing information regarding the development timing of Backbone Infrastructure and Public Facilities improvements; and
- ◆ Establishing the policy framework for financing the required Backbone Infrastructure and Public Facilities improvements.

The Public Facilities Financing Plan and the Urban Services Plan address the fact that some of the property owners (Participating Developers) will participate in development at the outset and that these owners will be contractually bound through a Development Agreement with the County as well as private agreements between developers to work cooperatively in constructing the Backbone Infrastructure required to serve all projects.

There are property owners that have not been participating (Non-Participating Developers) in the process and may not have Development Agreements. Non-Participating Developer properties are included in the Specific Plan, but are not expected to move forward with development at the present time. If and when Non-Participating Developers move forward, their projects will be subject to the Specific Plan policies, including the Public Facilities Financing Plan and the Urban Services Plan.

### 9.4.2 GOALS OF THE PUBLIC FACILITIES FINANCING PLAN

The goals of the Public Facilities Financing Plan are as follows:

- ◆ Require developers to fund and construct Backbone Infrastructure and Facilities;
- ◆ Implement new fee programs and, if needed, modify existing fee programs to ultimately provide shared funding mechanisms for all development projects through fee credits and reimbursements;
- ◆ Consider appropriate use of municipal debt-financing mechanisms to reimburse developers for construction of facilities, recognizing however that municipal debt financing for infrastructure and public facilities is questionable given public service funding needs and priority status; and
- ◆ Build in flexibility to allow response to market conditions.

The Public Facilities Financing Plan and the Development Agreement propose the following fee program formation and update process which will be used to ultimately provide equal participation in financing for all development projects:

- ◆ Use existing County and Special District fee programs, including Existing Development Mitigation Fees and Mitigation Monitoring Program Fees to the extent possible;

## IMPLEMENTATION

- ◆ New Development Mitigation Fees. The County, consistent with the terms of the Development Agreement and the Public Facilities Financing Plan, may establish new development mitigation fees consistent with the Mitigation Fee Act to finance the Plan Area's fair share of other pending regional facilities.
- ◆ As more fully described in the Development Agreement, Project Development Fees will be paid by the Developer.
- ◆ PVSP Fees. The County, consistent with the Development Agreement, shall adopt, impose and implement a PVSP Fee Program for capital facilities, neighborhood parks, community parks and recreation, trails and backbone infrastructure not funded by existing development mitigation fees and;
- ◆ Participate in other new regional fee programs (i.e., planned regional road fee programs).

### 9.4.3 GOALS OF THE URBAN SERVICES PLAN

The goals of the Urban Services Plan serve to provide a solid framework for the delivery and finance of services within the Plan Area. Financing of required services recognizes existing policies of the County and urban service levels as a priority for funding and maintaining services over public financing of project and development related infrastructure and facilities. The goals of the Urban Services Plans are as follows:

- ◆ Establish a high level of urban service standards for the Plan Area commensurate with surrounding jurisdictions;
- ◆ Identify funding sources to pay for urban service costs that minimize financial risk to the County and can be sustained as development occurs at build-out;
- ◆ Ensure services are funded and available when needed; and
- ◆ Consistent with General Plan policy, finance services by the development of the project without adversely affecting existing County funds.

### 9.4.4 GENERAL FINANCING POLICIES

#### ***Policy 9.7 General Public Facilities Financing Policy.***

*The following policies shall be followed in implementing the Public Facilities Financing Plan for the Plan Area:*

- 1. The full costs of both on-site and off-site public infrastructure and public facilities required to support the Plan Area will be funded first and foremost from private financing and revenues generated by development within the Plan Area. Some regionally serving public facilities may be funded by a larger fee program that includes areas both within and outside of the Plan Area.*
- 2. Development projects will be required to provide up-front funding for the costs of installing and expanding the Backbone Infrastructure and Public Facilities as and when necessary to adequately serve and support their projects, consistent with the Public Facilities Financing Plan and the Development Agreement. Developers will be subject to fee credits or future reimbursements, consistent with the provisions of the Development Agreement. The costs for Backbone Infrastructure and Public Facilities will be allocated as much as possible based on a project's fair share of required improvements and as described in the Public Facilities Financing Plan and the Development Agreement.*
- 3. Plan Area fees will be imposed by the Development Agreement, for those Backbone Improvements and Public Facilities that are not funded by existing fee programs. A fair-share cost allocation of the Plan Area fee for required backbone improvements and public facilities will be established for each land use consistent with the Public Facilities Financing Plan.*
- 4. The Public Facilities Financing Plan establishes methods to time the availability of infrastructure funding to the need for said infrastructure.*
- 5. The use of public financing to fund urban services shall take priority over the use of such financing to fund infrastructure improvements in the Plan Area.*
- 6. When public financing is used, the total annual tax and/or assessment rates for developed land shall not exceed fiscally prudent levels and will be consistent with the Rules and Procedures of the Placer County Bond Screening Committee.*

7. Before properties can be developed, two Community Facilities Districts shall be formed for the purposes of funding park maintenance and County services as more fully described in the Development Agreement. Parcel maps that are found by the applicable hearing body to be for bonafide agricultural uses and are consistent with their current agricultural zoning are exempt from this policy.

**Policy 9.8 General Urban Services Financing Policies.**

The following policies shall be followed in implementing the Urban Services Plan for the Plan Area:

1. Services will be funded and provided to residents, businesses, and employees of the Plan Area at a higher urban level commensurate with similar urban communities, and above the existing levels provided by the County with the unincorporated area and cities.
2. Ensure timing for funding of urban and county-wide services is coordinated so that services are available when needed as the population and employment grows as possible based on a project's fair share of required improvements and as described in the Public Facilities Financing Plan and the Development Agreement.
3. A funding strategy shall be developed to ensure that the County's General Fund is not negatively impacted by the cost of providing urban and county-wide services in a sustainable and reliable manner.
4. Use of public financing to fund services shall take priority over the use of such financing for infrastructure and public facility improvements in the Plan Area.
5. When public financing is used, the total annual tax and/or assessment rates for developed land shall not exceed fiscally prudent levels and will be consistent with the rules and procedures of the Placer County Bond Screening Committee.
6. Before properties can be developed, two Community Facilities Districts shall be formed for the purposes of funding park maintenance and County services as more fully described in the Development Agreement. Parcel maps that are found by the applicable hearing body to be for bonafide agricultural uses and are consistent with their current agricultural zoning are exempt from this policy.

**Policy 9.9 Other Financing Mechanisms.**

As noted, other financing mechanisms, such as creation of private districts or associations, may be used to fund maintenance of certain facilities in the Plan Area. Any such alternative or supplemental financing mechanisms shall comply with the policies described in Policy 9.8 above.

**Policy 9.10 Infrastructure Not Included in Financing Plan.**

The construction of in-tract subdivision infrastructure shall not be included in the Public Facilities Financing Plan; however the maintenance of such public infrastructure is included in the Urban Services Plan.

## IMPLEMENTATION

### 9.4.5 FINANCING PUBLIC IMPROVEMENTS

As described below, the construction of backbone infrastructure and other public improvements, public facilities and services designed to serve the Plan Area will be funded by a variety of mechanisms: county-wide impact fees; school district impact fees, Placer Vineyards Specific Plan fees, the use of CFD's and/or a CSA, developer financing, and other potential methods.

#### County Impact Fees (Existing Development Mitigation Fees)

Placer County has adopted a set of development impact fees to finance capital improvements. Future updates to the Placer County fees may include certain improvements within the Plan Area.

#### School District Impact Fees

The various school districts have established fees, in accordance with state regulations, to be used to construct school facilities. School impact fees are collected by the County before the issuance of a building permit and are forwarded to the applicable school districts.

#### Placer Vineyard Specific Plan New Development Mitigation Fees

As more fully described in the Development Agreement and the Public Facilities Financing Plan, New Development Mitigation Fees will pay for capital facilities not covered by Existing Development Mitigation Fees.

#### Project Development Fees

As more fully described in the Development Agreement, Placer Vineyards developers agree to pay certain project development fees.

#### PVSP Fees

As more fully described in the Development Agreement, PVSP Fees are established to create a fair share mechanism whereby the costs of the infrastructure and public facilities necessary for development of the Plan Area are allocated to and fairly shared by the benefitted land uses within the Plan Area. PVSP Fees are intended to fund the construction of required infrastructure and facilities serving the Plan Area that are not included for financing within the Existing or New Development Mitigation Fees and to fund the County costs of monitoring and implementing the PVSP program.

#### Community Facilities Districts

The 1982 Mello-Roos Community Facilities Act enables cities and other entities to establish a community facilities district to fund various facilities and services. Under this act, an annual special maximum tax may be levied on land within the boundaries of the community facilities district. The proceeds from a bond sale by the community facilities district can be used for direct funding of improvements, to acquire facilities constructed by the developer, and/or to reimburse developers for advance funding of improvements. The annual maximum special tax can be used toward bond debt service or to build infrastructure as needed. The proceeds of the Mello-Roos special tax can be used for direct funding of facilities and/or to pay off bonds. The proceeds of the Mello-Roos special tax for services can be used to fund such services in perpetuity.

##### Infrastructure CFD

As more fully described in the Development Agreement and consistent with the Rules and Procedures of the County's Bond Screening Committee, the County, upon request of a Developer, shall form one or more Infrastructure Community Facilities Districts for the purpose of financing the Developer's share of the costs of construction and acquisition of the public infrastructure and facilities within the Plan Area.

##### Services CFD

As more fully described in the Development Agreement and the Urban Services Plan, two Services Community Facility Districts shall be formed: a Park Services CFD and a County Services CFD. The Park Services CFD will be used to fund park maintenance and services and the County Services CFD will be used to provide funding for new and/or enhanced services provided by the County.

#### County Service Area 28

As more fully described in the Development Agreement, the Plan Area property shall be annexed to County Service Area 28, Zone 173, or a newly formed Zone of Benefit, for the purposes of funding sewer maintenance services.

**Placer Vineyards Specific Plan  
Placer County, California**

**Appendix A:  
Land Use and Development Standards  
(Provided Under Separate Cover)**

*Revised November 2014  
Adopted July 2007*



**Placer Vineyards Specific Plan  
Placer County, California**

**Appendix B:  
Recommended Plant List**

*Revised November 2014  
Adopted July 2007*



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## APPENDIX B: RECOMMENDED PLANT LIST

The list of plants below are recommended for use in Placer Vineyards within the design of its open space areas, landscape buffer corridors, streetscapes, gateways and parks. Plants similar to those listed in the table may also be substituted at the discretion of the County.

OPEN SPACE		
Botanical Name	Common Name	Distribution Percentage
<b>Upland-Savanna</b>		
<b>TREES</b>		
Aesculus californica	California Buckeye	15%
Quercus douglasii	Blue Oak	15%
Quercus lobata	Valley Oak	40%
Quercus wislizenii	Interior Live Oak	15%
Umbellularia californica	California Laurel	15%
		100%
<b>SHRUBS</b>		
Arctostaphylos sp	Manzanita	15%
Artemisia californica	California Sagebrush	10%
Ceanothus gloriosus	Point Reyes Creeper	30%
Ceanothus sp.	California Lilac	10%
Heteromeles arbutifolia	Toyon	20%
Rhamnus ilicifolia	Hollyleaf Redberry	15%
		100%
<b>GROUNDCOVER</b>		
Bromus carinatus	California Brome	15%
Hordeum brachyantherum	Meadow Barley	15%
Muhlenbergia rigens	Deergrass	40%
Nassella pulchra	Purple Needlegrass	15%
Lupinus polyphyllus	Blue Lupine	15%
		100%

RECOMMENDED PLANT LIST

OPEN SPACE		
Botanical Name	Common Name	Distribution Percentage
<b>Riparian Woodland (2- to 5-year event creek flow)</b>		
<b>TREES</b>		
Acer negundo	Boxelder	5%
Alnus rhombifolia	White Alder	5%
Fraxinus latifolia	Oregon Ash	10%
Populus fremontii	Fremont Cottonwood	25%
Quercus lobata	Valley Oak	5%
Salix gooddingii	Goodding's Willow	15%
Salix lasiandra	Red Willow	15%
Salix hinsiana	Sandbar Willow	20%
		100%
<b>SHRUBS</b>		
Ceanothus gloriosus	Point Reyes creeper	20%
Cephalanthus occidentalis	Buttonbush	40%
Rosa californica	California Wild Rose	20%
Rubus ursinus	California Blackberry	20%
<b>VINES</b>		
		100%
Vitis californica	California Wild Grape	30%
Aristolochia californica	California Dutchman's Pipe	70%
		100%
<b>GROUNDCOVER</b>		
Artemisia californica	California Sagebrush	30%
Carex barbarae	Barbara Sedge	40%
Leymus triticoides	Creeping Wildrye	15%
Nassella pulchra	Purple Needlegrass	15%
		100%

OPEN SPACE		
Botanical Name	Common Name	Distribution Percentage
Riparian-Wetland (<2-year event creek flow)		
<b>TREES</b>		
<i>Alnus rhombifolia</i>	White Alder	30%
<i>Populus fremontii</i>	Fremont Cottonwood	30%
<i>Salix exigua</i>	Sandbar Willow	40%
		100%
<b>GROUNDCOVER</b>		
<i>Carex praegracilis</i>	Clustered Field Sedge	10%
<i>Equisetum hyemale</i>	Horsetail	10%
<i>Juncus effusus</i> var. <i>pacificus</i>	Pacific Rush	30%
<i>Juncus xiphioides</i>	Iris-leaved Rush	30%
<i>Scirpus acutus</i>	Tule	20%
		100%

RECOMMENDED PLANT LIST

LANDSCAPE BUFFERS		
Botanical Name	Common Name	Distribution Percentage
<b>TREES</b>		
Aesculus californica	California Buckeye	15%
Cercis occidentalis	Western Redbud	10%
Platanus racemosa	Western Sycamore	20%
Cedrus deodar	Deodar Cedar	10%
Quercus douglasii	Blue Oak	10%
Quercus lobata	Valley Oak	35%
		100%
<b>SHRUBS</b>		
Arctostaphylos densiflora 'Howard McMinn'	McMinn's Manzanita	10%
Artemisia californica	California Sagebrush	15%
Ceanothus sp.	California Lilac	15%
Cephalanthus occidentalis	Buttonbush	15%
Cistus purpurea	Orchid Rockrose	10%
Cistus salvifolius	Sageleaf Rockrose	5%
Fremontodendron californicum	Common Flannel Bush	15%
Heteromeles arbutifolia	Toyon	15%
		100%
<b>GROUNDCOVER</b>		
Arctostaphylos 'Emerald Carpet'	Emerald Carpet Manzanita	15%
Ceanothus gloriosus	Point Reyes Creeper	20%
Carpenteria californica	Bush Anemone	5%
Iris douglasiana	Douglas Iris	5%
Mimulus aurantiacus	Monkey Flower	10%
Muhlenbergia rigens	Deergrass	25%
Penstemon spp.	Penstemon	5%
Vitis californica	California Wild Grape	10%
Zauschneria californica	California Fuchsia	5%
		100%

STREETSCAPE		
Botanical Name	Common Name	Distribution Percentage
<b>TREES</b>		
Calocedrus decurrens	Incense Cedar	Large Screen Evergreen
Cercis occidentalis	Western Redbud	Flowering Accent Deciduous
Eucalyptus polyanthemus	Silver Dollar Gum	Large Canopy Evergreen
Pinus canariensis	Canary Island Pine	Large Canopy Evergreen
Cedrus deodar	Deodar Cedar	Large Canopy Evergreen
Platanus acerifolia	London Plane Tree	Large Canopy Deciduous
Pyrus calleryana	Flowering Pear	Accent Deciduous
Quercus douglasii	Blue Oak	Large Canopy Deciduous
Quercus pilustris	Pin Oak	Large Canopy Deciduous
Quercus rubra	Red Oak	Large Canopy Deciduous
Quercus suber	Cork Oak	Large Canopy Evergreen
Lithocarpus densiflora	Tanbark Oak	Large Canopy Evergreen
Tilia cordata	Little Leaf Linden	Large Canopy Deciduous
Zelkova serrata	Sawleaf Zelkova	Large Canopy Evergreen
		100%
<b>SHRUBS</b>		
Arctostaphylos spp.	Manzanita	
Artemisia californica	California Sagebrush	
Ceanothus spp.	California Lilac	
Cistus spp.	Rockrose	
Elaeagnus pungens	Silverberry	
Escallonia 'Fradesii'	Escallonia	
Fremontodendron californicum	Common Flannel Bush	
Juniperus spp.	Junipers dwarf varieties	
Rhaphiolepis indica	'Springtime' or 'Dancer' Indian Hawthorn	
Xylosma congestum	Xylosma congestum	
Ribes aureum	Golden Currant	

RECOMMENDED PLANT LIST

Streetscape		
Botanical Name	Common Name	Distribution Percentage
<b>VINES</b>		
Clematis arandii	Evergreen Clematis	Train to Walls/Fences
Clytostoma callistegioides	Violet Trumpet Vine	Train to Walls/Fences
Hardenbergia violacea	Hardenbergia	Train to Walls/Fences
Lonicera japonica 'Halliana'	Hall's Honeysuckle	Train to Walls/Fences
Vitis californica	California Wild Grape	Train to Walls/Fences
<b>GROUNDCOVER</b>		
Jasminum	Jasmine	
Festuca arundinacea	Tall Fescue	
Hypericum calycinum	St. John's Wort	
Ceanothus gloriosus	'Point Reyes' Ceanothus	
Santolina chamaecyparissus	Lavender Cotton	
Verbena peruviana	Verbena	

GATEWAYS AND ENTRIES		
Botanical Name	Common Name	Distribution Percentage
<b>TREES</b>		
Acer rubra	'October Glory' Maple	Accent Deciduous
Betula alba	White Birch	Accent Deciduous
Cinnamomum camphora 'Monum'	Camphor Tree	Large Canopy Evergreen
Cornus nuttallii	Pacific Dogwood	Accent Deciduous
Fraxinus uhdei	Evergreen Ash	Large Canopy Evergreen
Malus floribunda	Japanese Crabapple	Orchard Accent Deciduous
Prunus cerasifera	Flowering Cherry	Orchard Accent Deciduous
Pyrus calleryana	Flowering Pear	Orchard Accent Deciduous
Quercus rubra	Red Oak	Large Canopy Deciduous
Rhus lancea	African Sumac	Accent Evergreen
Sequoia sempervirens	Redwood	Large Screen Evergreen
Zelkova		
<b>SHRUBS</b>		
Arbutus unedo	Strawberry Tree/ Shrub	
Juniperus spp.	Junipers dwarf varieties	
Pennisetum setaceum 'Rubrum'	Purple Fountain Grass	
Rhaphiolepis indica	'Springtime' or 'Dancer' Indian Hawthorn	
<b>VINES</b>		
Clematis armandii	Evergreen Clematis	
Vitis californica	California Wild Grape	

RECOMMENDED PLANT LIST

GATEWAYS AND ENTRIES		
Botanical Name	Common Name	Distribution Percentage
<b>GROUNDCOVER</b>		
Eschscholzia californica	California poppy	
Festuca arundinacea	Tall Fescue	Turf - limited accent areas
Lantana camara	Wild Lantana	
Lupinus polyphyllus	Blue Lupine	
Penstemon spp.	Penstemon	
Portulaca grandiflora	Rose Moss	
Salvia gregatae	Lavender Sage	
Zauschneria californica	California Fuchsia	

GATEWAYS AND ENTRIES		
Botanical Name	Common Name	Distribution Percentage
<b>TREES</b>		
Acer rubra	'October Glory' Maple	Accent Deciduous
Calocedrus decurrens	Incense Cedar	Large Screen Evergreen
Jacaranda mimosifolia	Jacaranda	Flowering Accent Deciduous
Maytenus boaria	Mayten Tree	Accent Evergreen
Sequoiadendron giganteum	Giant Sequoia	Large Screen Evergreen
Pistacia chinensis	Chinese Pistache	Large Canopy Deciduous
Quercus lobata	Valley Oak	Large Canopy Deciduous
Quercus suber	Cork Oak	Large Canopy Evergreen
Tilia cordata	Little Leaf Linden	Large Canopy Deciduous
Umbellularia californica	California Bay	Large Canopy Evergreen
<b>SHRUBS</b>		
Arctostaphylos spp.	Manzanita	
Ceanothus spp.	California Lilac	
Cistus spp.	Rockrose	
Arbustus unedo	Strawberry Tree/ Shrub	
Xylosma congestum	Xylosma	
<b>GROUND COVER</b>		
Arctostaphylos densiflora	Vine Hill Manzanita	
Ceanothus gloriosus	Point Reyes Creeper	
Eschscholzia californica	California poppy	
Dietes vegeta	Fortnight Lily	
Festuca arundinacea	Tall Fescue	Turf Areas
Lupinus polyphyllus	Blue Lupine	
Mahonia repens	Creeping Mahonia	

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**Placer Vineyards Specific Plan  
Placer County, California**

**Appendix C:  
Traffic Intersection Designs**

*Revised November 2014  
Adopted July 2007*



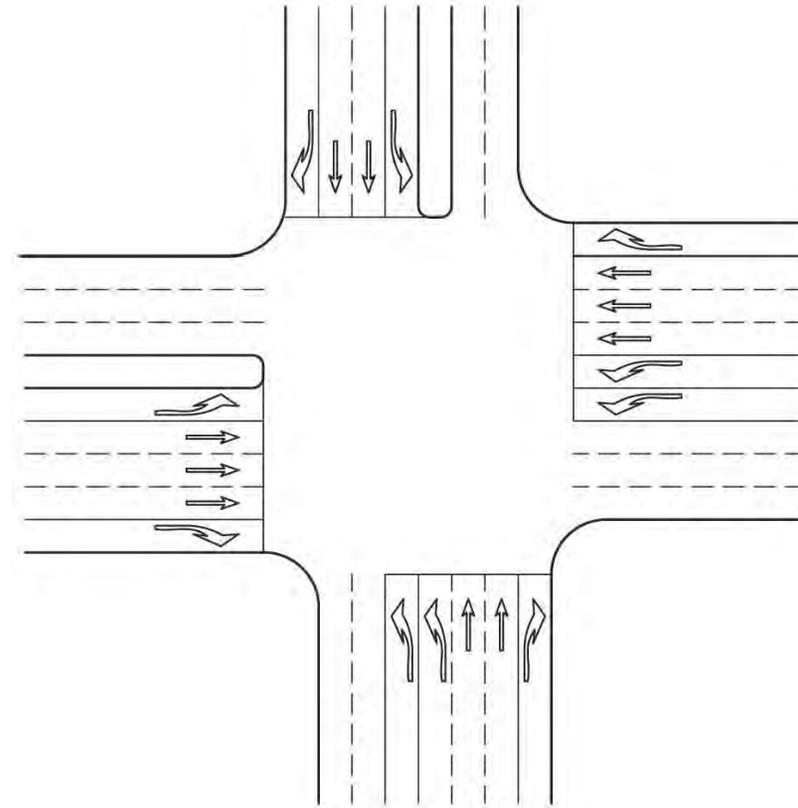
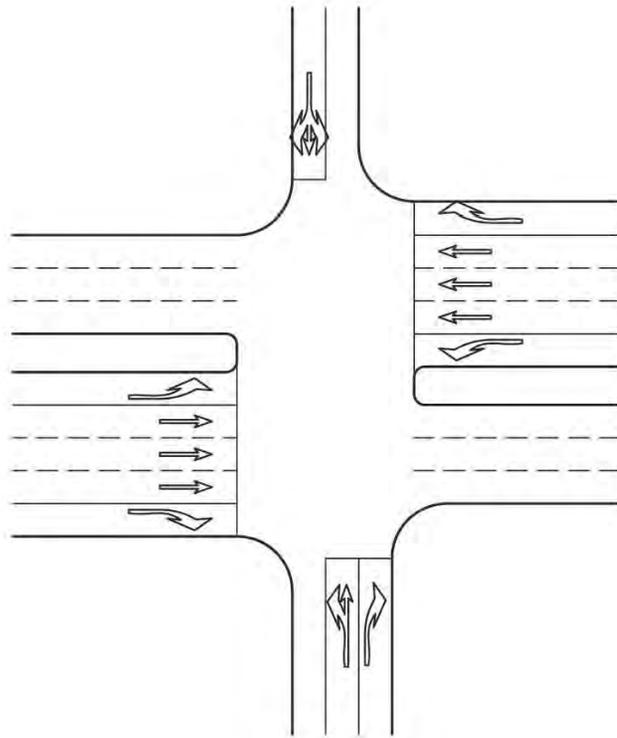
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## APPENDIX C: TRAFFIC INTERSECTION DESIGN

### INTRODUCTION

The traffic intersection designs that follow in this section show the ultimate intersection design configurations for the 2025 cumulative plus project mitigated transportation network within and adjacent to the boundaries of the Placer Vineyards Specific Plan Area.

TRAFFIC INTERSECTION DESIGNS

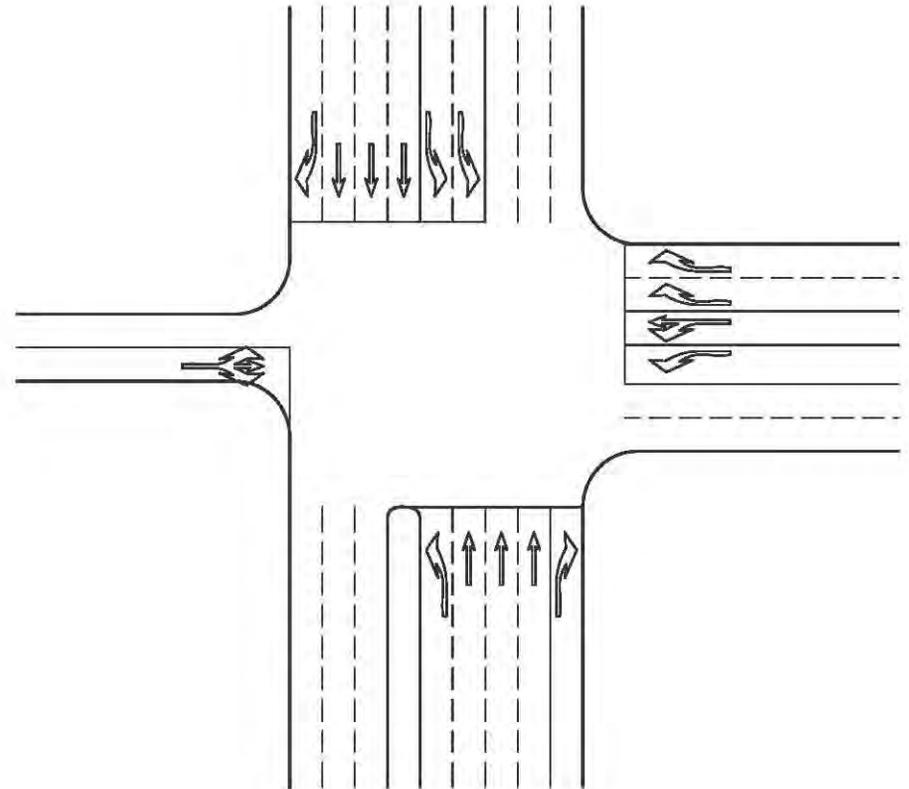
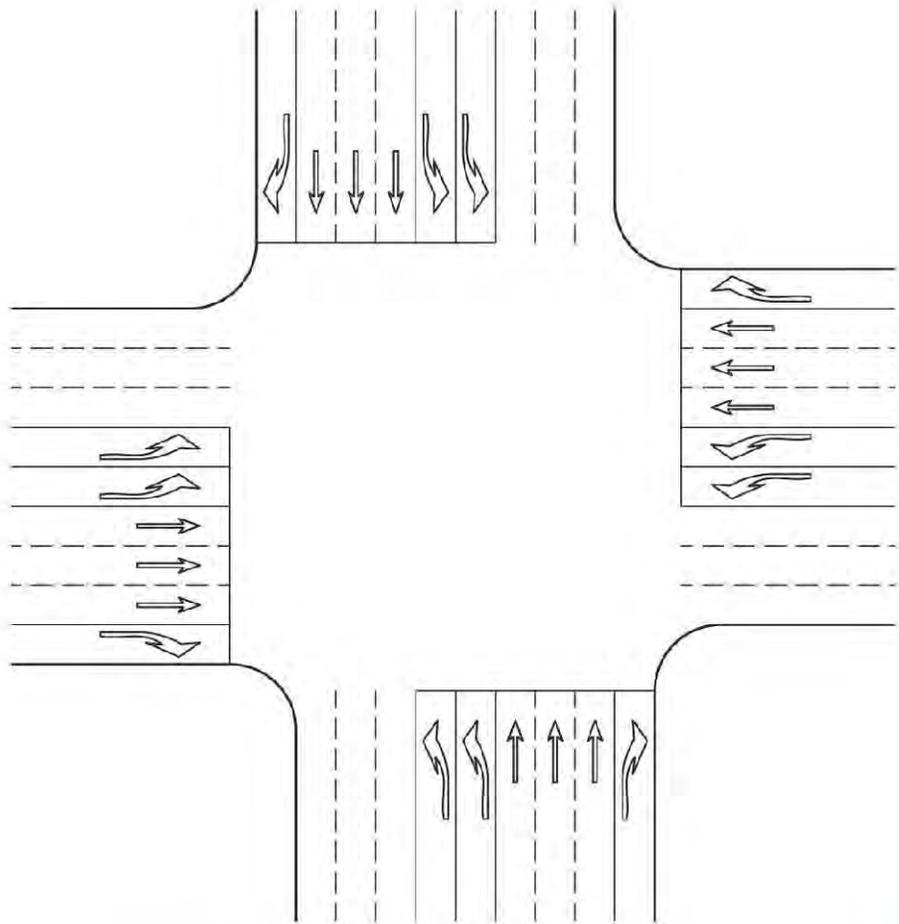


Intersection - TYPE A	
North-South Roadway	East-West Roadway
1 Locust Road	Base Line Road

Intersection - TYPE B	
North-South Roadway	East-West Roadway
2 Brewer Road	Base Line Road

Source: McKay & Somps, 2006

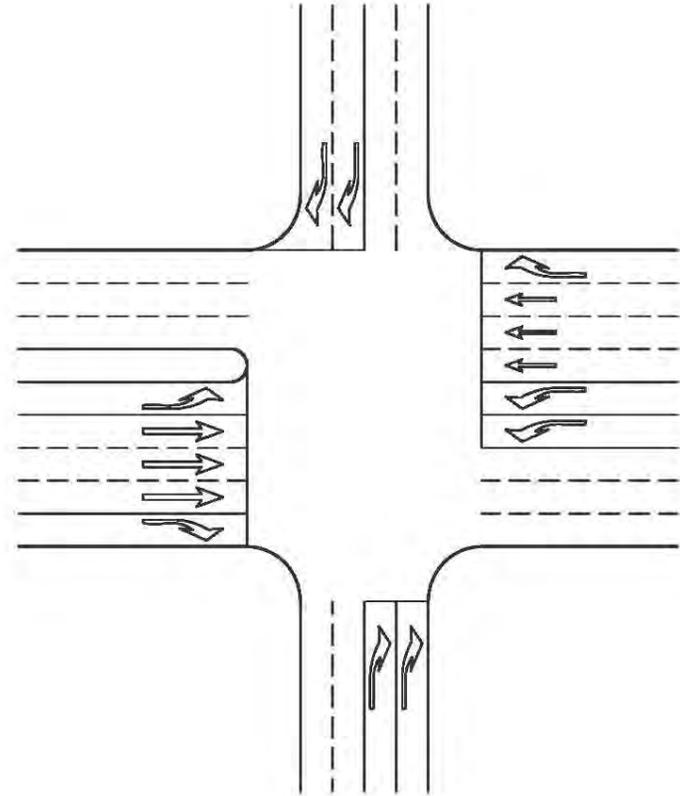
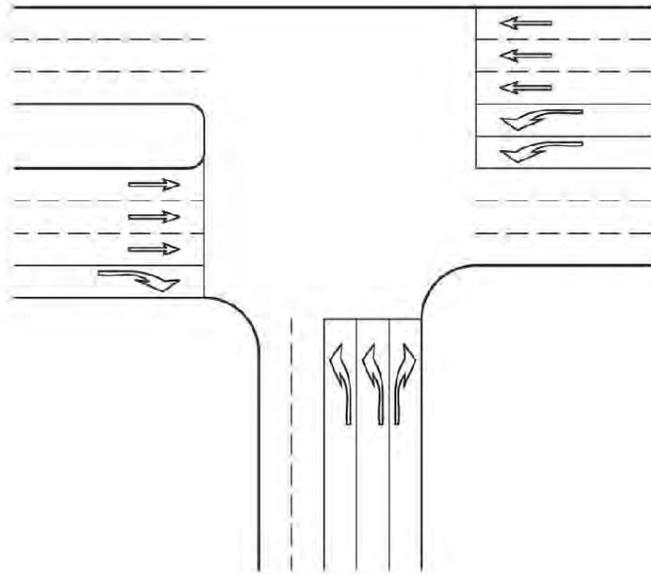
TRAFFIC INTERSECTION DESIGNS



Intersection - TYPE C	
North-South Roadway	East-West Roadway
3 Watt Avenue	Base Line Road
13 East Dyer Lane	Base Line Road
4 Fiddymment Road	Base Line Road
29 Watt Avenue	East/West Dyer

Intersection - TYPE D	
North-South Roadway	East-West Roadway
5 Watt Avenue	PFE Road

Source: McKay & Somps, 2006

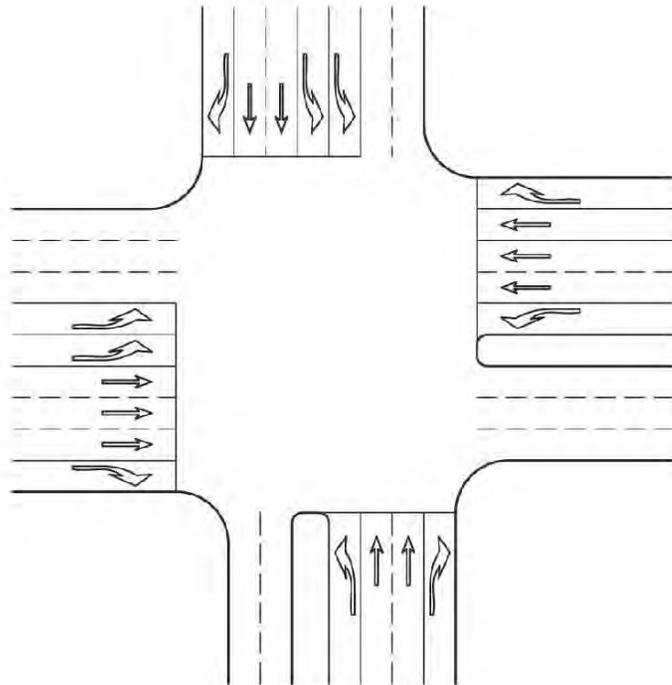


Intersection - TYPE E	
North-South Roadway	East-West Roadway
8 Palladay Road	Base Line Road
9 16th Street	Base Line Road
10 14th Street	Base Line Road
11 12th Street	Base Line Road

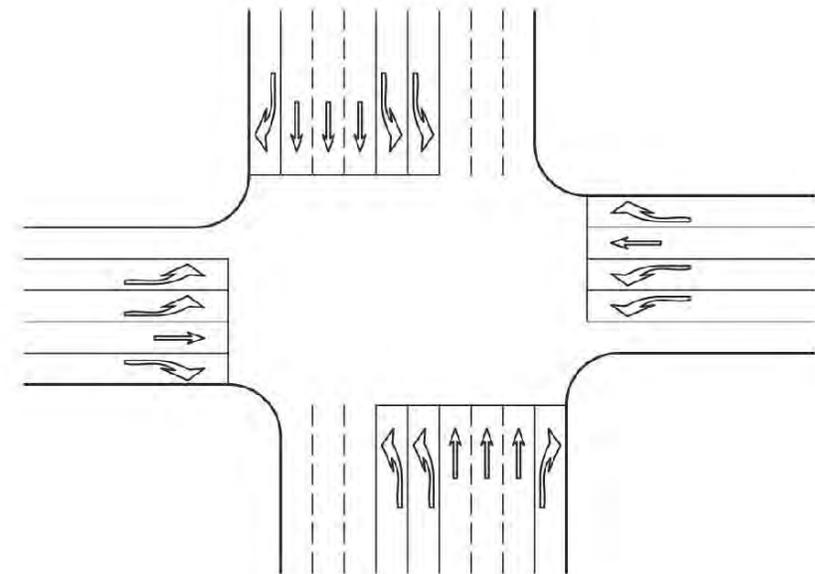
Intersection - TYPE F	
North-South Roadway	East-West Roadway
12 11th Street	Base Line Road

Source: McKay & Soms, 2006

TRAFFIC INTERSECTION DESIGNS



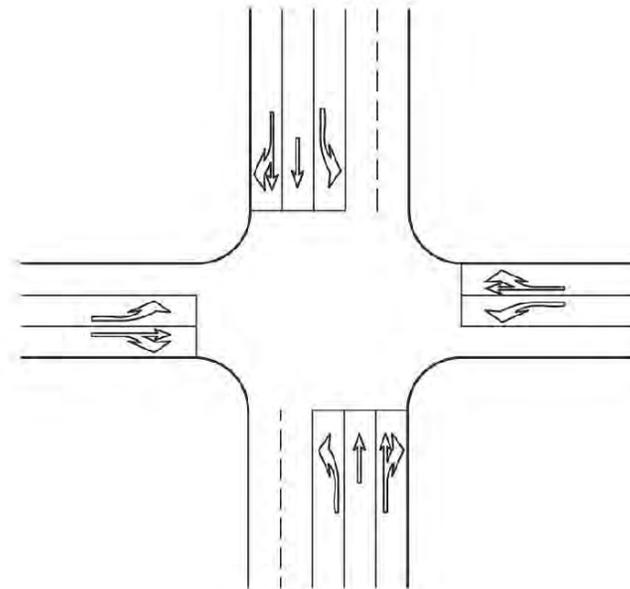
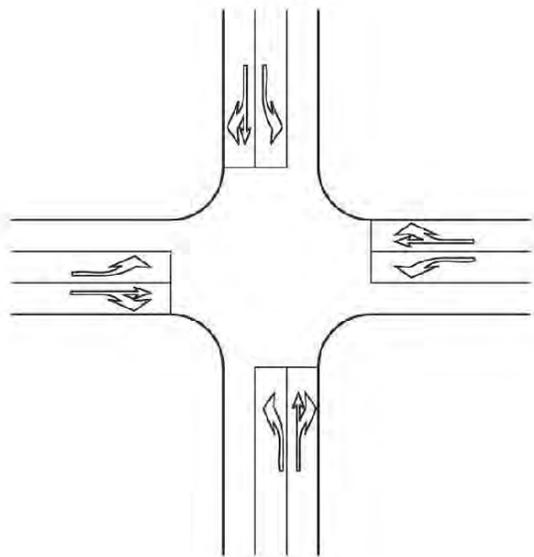
Intersection - TYPE G	
North-South Roadway	East-West Roadway
14 9th Street	Base Line Road



Intersection - TYPE H	
North-South Roadway	East-West Roadway
15 Watt Avenue	A Street

Source: McKay & Soms, 2006

TRAFFIC INTERSECTION DESIGNS

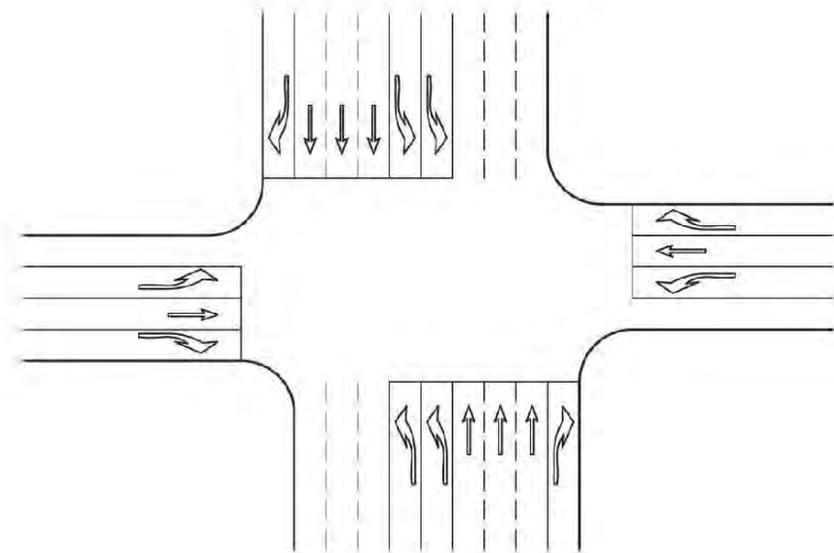
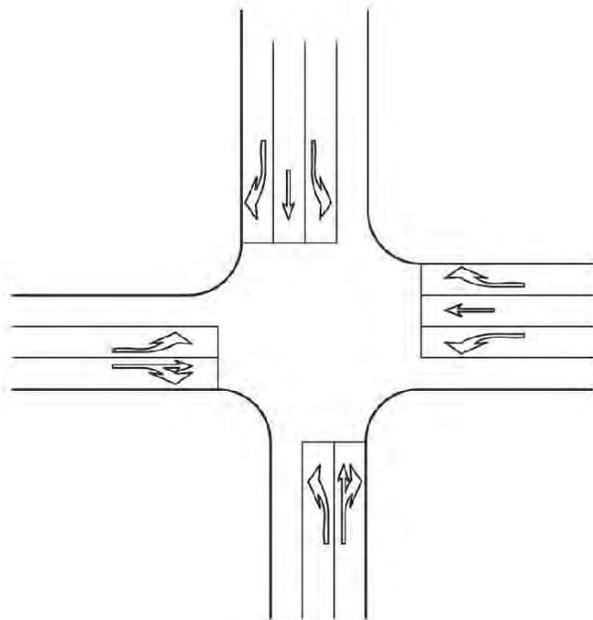


Intersection - TYPE I	
North-South Roadway	East-West Roadway
17 Palladay Road	A Street
19 14th Street	A Street

Intersection - TYPE J	
North-South Roadway	East-West Roadway
23 East Dyer Lane	Town Center Drive
18 16th Street	A Street
21 West Dyer Lane	W. Town Center Drive
28 West Dyer Lane	Tanwood Avenue
West Dyer Lane	A Street

Source: McKay & Soms, 2006

TRAFFIC INTERSECTION DESIGNS

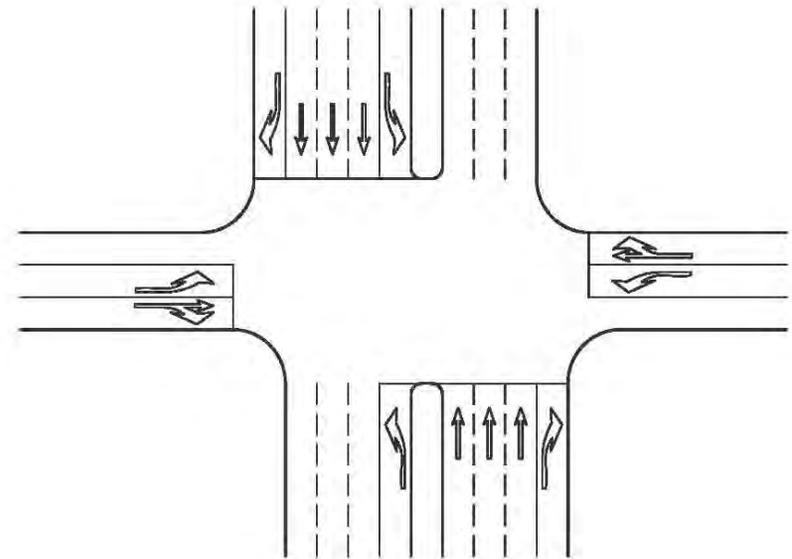
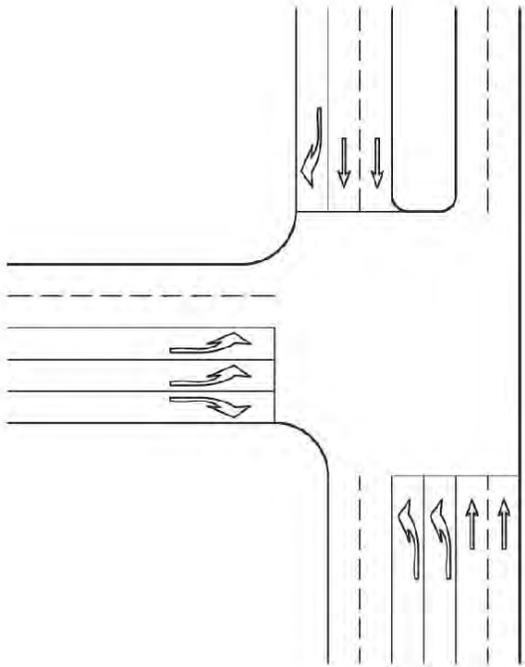


Intersection - TYPE K	
North-South Roadway	East-West Roadway
20 12th Street	A Street

Intersection - TYPE L	
North-South Roadway	East-West Roadway
22 Watt Avenue	Town Center Drive

Source: McKay & Somps, 2006

TRAFFIC INTERSECTION DESIGNS

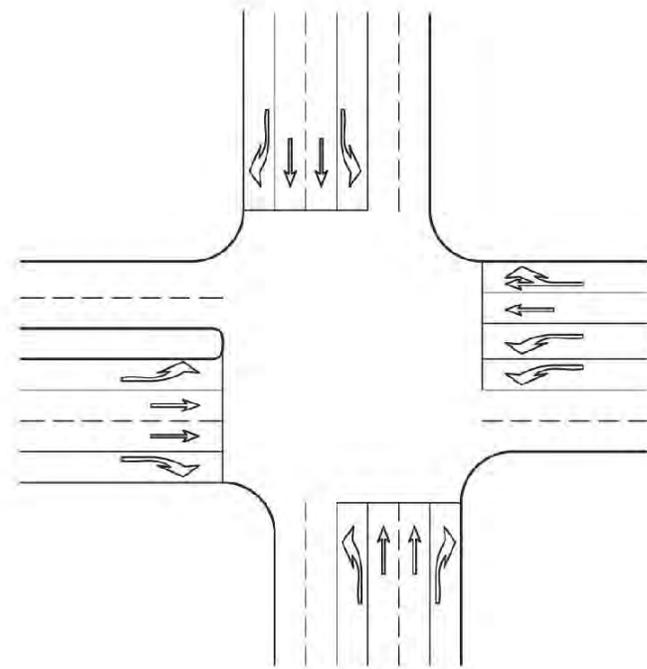
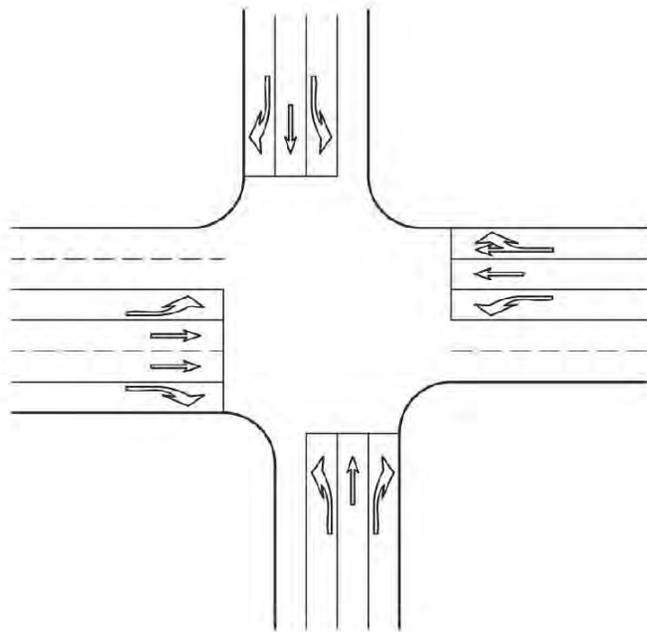


Intersection - TYPE M	
North-South Roadway	East-West Roadway
24 Walerga Road	Town Center Drive

Intersection - TYPE N	
North-South Roadway	East-West Roadway
25 Watt Avenue	Oak Street

Source: McKay & Somps, 2006

TRAFFIC INTERSECTION DESIGNS



Intersection - TYPE O	
North-South Roadway	East-West Roadway
26 18th Street	West Dyer Lane

Intersection - TYPE P	
North-South Roadway	East-West Roadway
27 16th Street	West Dyer Lane

Source: McKay & Somps, 2006

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**Placer Vineyards Specific Plan  
Placer County, California**

**Appendix D:  
Mitigation Monitoring and  
Reporting Program  
(Provided Under Separate Cover)**

*Revised November 2014  
Adopted July 2007*

